



**CITY OF MOLALLA  
PLANNING COMMISSION  
AGENDA**

Civic Center | 315 Kennel Ave  
Wednesday, July 1, 2026 | 6:30 PM

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*NOTICE: Planning Commission will hold this meeting in-person and through Live-Streaming on the City's YouTube channel, City of Molalla | Oregon. Written comments may be delivered to City Hall or emailed to [recorder@cityofmolalla.com](mailto:recorder@cityofmolalla.com). Submissions must be received by 12:00 p.m. the day of the meeting.*

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*[This institution is an equal opportunity employer.](#)*

- 1. CALL TO ORDER & FLAG SALUTE**
- 2. ROLL CALL**
- 3. PRESENTATIONS, PROCLAMATIONS & CEREMONIES**
- 4. CONSENT AGENDA**
  - a. [Planning Commission Meeting Minutes - June 3, 2026](#)
- 5. PUBLIC COMMENT & WRITTEN COMMUNICATIONS**

Citizens are allowed up to 3 minutes to present information relevant to the city but not listed as an item on the agenda. Prior to speaking, citizens shall complete a comment form and deliver it to the support staff. The Planning Commission does not generally engage in dialog with those making comments but may refer the issue to the Community Development Director. Complaints shall first be addressed at the department level prior to addressing the Planning Commission.

- 6. PUBLIC HEARINGS**
  - a. [New Storage Facility for Molalla Rural Fire District \(SDR03-2026 & VAR02-2026\)](#)
- 7. GENERAL BUSINESS**
  - a. [Zone Change from Light Industrial \(M-1\) to Public/Semi-Public \(PSP\) for Taxlot 52E07A 00711](#)
  - b. [Planning Commission Start-Time](#)
- 8. STAFF COMMUNICATION**
- 9. COMMISSION COMMUNICATION**
- 10. ADJOURN**

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*Agenda posted on the City Website at <http://www.cityofmolalla.com/meetings>. This meeting location is wheelchair accessible. Disabled individuals requiring other assistance must make their request known 48 hours preceding the meeting by contacting the Community Development Office at 503-759-0243.*



## CITY OF MOLALLA

### Staff Report

**Agenda Category: CONSENT AGENDA**

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**Agenda Date: Wednesday, July 1, 2026**

**Submitted by: Christie Teets, City Recorder**

**Approved by: Dan Huff, City Manager**

**SUBJECT: Planning Commission Meeting Minutes - June 3, 2026**

**ATTACHMENTS:**

[Planning Commission Meeting Minutes- June 3, 2026.pdf](#)



City of Molalla  
Planning Commission - Regular Meeting Minutes  
June 3, 2026  
Molalla Civic Center | 315 Kennel Ave. | Molalla, OR

**CALL TO ORDER**

The Molalla Planning Commission Meeting of June 3, 2026, was called to order by Chairman Doug Eaglebear at 6:30 pm.

**COMMISSIONER ATTENDANCE**

Present: Commissioner Chair Doug Eaglebear, Commissioner Clint Ancell, Commissioner Connie Sharp, Commissioner Martin Ornelas, Commissioner David Potts, Commissioner Brady Rickey.

**STAFF IN ATTENDANCE**

Present: Assistant City Manager, Mac Corthell; Associate Planner, Claressa Davis; Engineering Section Manager, Sam Miller, and City Recorder, Christie Teets.

**PRESENTATIONS, PROCLAMATIONS & CEREMONIES**

None.

**CONSENT AGENDA**

A. Planning Commission Meeting Minutes – May 6, 2026

**ACTION:**

Commissioner Ancell moved to approve the Consent Agenda; Commissioner Potts seconded. **Motion passed 6-0.**

AYES: Sharp, Ancell, Potts, Rickey, Ornelas, Eaglebear.

NAYS: None.

ABSTENTIONS: None.

**PUBLIC COMMENT**

None.

**PUBLIC HEARINGS:**

*Commissioner Chair Eaglebear opened Public Hearing at 6:38pm for consideration of a consolidated quasi-judicial land use application. The applications, SDR01-2026, CUP01-2026, ADJ01-2026, and ADJ02-2026 are for a Site Design Review, Conditional Use Permit, and two Adjustments for a new Self-Storage and Truck Rental Use at Lot 3 and 4 of the Industrial Subdivision, addressed as 545 OR-211*

Associate Planner Davis provided the staff report, with a full outline of the intended use of the applicant. Ms. Davis stated that the application was considered light industrial. *(Staff report conditions are attached to these Meeting Minutes.)*

The special sauce of this application is pressure reducing valves. Ms. Davis provided further details regarding the requirements. She relayed that the applicant had been amenable to each condition.

Kyle Campian, President of U-Haul, greeted Commissioners and thanked them for their time. He shared his excitement regarding the opportunity to create a facility and serve the citizens of Molalla. *(For full account of Planning Associate Davis staff report review YouTube meeting recording minutes 18:12 – 33:51)*

*Chair Eaglebear closed the Public Hearing at 7:00pm.*

**ACTION:**

Commissioner Chair Eaglebear moved to approve SDR01-2026, CUP01-2026, ADJ01-2026, and ADJ02-2026, subject to the conditions of approval as set forth in the staff report, and to authorize the Chair to sign a final decision approving application SDR01-2026, CUP01-2026, ADJ01-2026, and ADJ02-2026 and incorporating the findings from the staff report; Commissioner Ancell seconded. **Motion passed 6-0.**

AYES: Sharp, Ancell, Potts, Rickey, Ornelas, Eaglebear.

NAYS: None.

ABSTENTIONS: None.

**GENERAL BUSINESS:**

None.

## STAFF COMMUNICATION

- **Assistant City Manager Corthell:** provided updates on several major capital projects. He reported that the Wastewater Treatment Plant project is approximately 60 percent complete and highlighted recent project modifications that are expected to reduce overall costs through construction sequencing efficiencies, biosolids management changes, and potential grant funding opportunities. He also reported that the Water Intake project was nearing 30 percent design completion and shared that the City has secured approximately federal funding to support the project.
- **Associate Planner Davis:** no report.
- **Engineering Section Manager Miller:** no report.
- **City Recorder Teets:** shared that applications are being accepted for the vacant Planning Commission position. She also reported the filing period for the November 2026 City Council election was open. Three council positions would be on the ballot.

## COMMISSION COMMUNICATION

- **Commissioner Rickey:** no report.
- **Commissioner Potts:** no report.
- **Commissioner Ancell:** no report.
- **Commissioner Sharp:** reminded community members to register early for the upcoming Giant Street Parade. Noted parade participation may be capped due to anticipated high interest associated with the nation's 250th anniversary celebration.
- **Chairman Eaglebear:** update on construction of the new school, noting that building framing, roof installation, and site infrastructure work are progressing steadily. She reported that utility and roadway improvements along Lynn Lane are underway and observed that the school project, along with nearby industrial development, is expected to significantly transform the area upon completion.

## ADJOURNMENT

Chair Eaglebear adjourned the meeting at 7:19pm.

For the complete video account of the Planning Commission Meeting, please visit the  
City of Molalla official YouTube page  
[City of Molalla | Oregon – Molalla Planning Commission | June 3, 2026](#)

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Doug Eaglebear, Planning Commission Chair

Prepared By:

Attest:

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Crystal Robles, Deputy City Recorder

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Christie Teets, City Recorder



## CITY OF MOLALLA

### Staff Report

**Agenda Category: PUBLIC HEARINGS**

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**Agenda Date: Wednesday, July 1, 2026**

**Submitted by: Dan Zinder, Planning Manager**

**Approved by: Dan Huff, City Manager**

**SUBJECT:** New Storage Facility for Molalla Rural Fire District (SDR03-2026 & VAR02-2026)

**RECOMMENDATION/RECOMMENDED MOTION:**

Conduct Public Hearing.

**BACKGROUND:**

See attached Staff Report.

**ATTACHMENTS:**

[SDR03 & VAR02- 2026 - 320 N Molalla - Staff Report and Exhibits.pdf](#)



CITY OF MOLALLA STAFF REPORT

Review for SDR03-2026 & VAR02-2026  
New Storage Building for the Molalla Fire District

**Date:** June 19, 2026, for the July 1, 2026, Planning Commission Meeting  
**File Number:** SDR03-2026 & VAR02-2026  
**Proposal:** New Storage Building and Stormwater Pond for the Molalla Fire District  
**Addresses:** 320 N Molalla, 414 N Molalla, & 317 Kennel Avenue  
**Taxlot:** 52E09CB08400, 52E09CB08500, 52E09CB08600, 52E09CB09300, 52E09CB09500, 52E09CB09600  
**Applicant/ Owner:** Molalla Fire District



**Applicable Standards:** Molalla Municipal Code, Title 17, Development Code

**Division II, Zoning Regulations**

*Section 17-2.2.030 Allowed Uses*

*Section 17-2.2.040 Lot and Development Standards*

**Division III**

*Section 17-3.2.040 Non-Residential Buildings*

*Section 17-3.2.050 Civic Space and Pedestrian Amenities*

*Chapter 17-3.3 Access and Circulation*

*Chapter 17-3.4 Landscaping, Fences and Walls, Outdoor Lighting,*

*Chapter 17-3.5 Parking and Loading*

*Chapter 17-3.6 Public Facilities*

**Division IV Applicable Review Procedures and Approval Criteria**

*Section 17-4.2.050 Approval Criteria (Site Design Review)*

*Section 17-4.7.040 Variances*

**TABLE OF CONTENTS:**

- I. Executive Summary
- II. Recommendations
- III. Conditions of Approval

**EXHIBITS:**

- EXHIBIT A: Findings of Fact for SDR03-2026 & VAR02-2026
- EXHIBIT B: Application Package SDR03-2026 & VAR02-2026
- EXHIBIT C: Molalla Public Works Comments

**I. EXECUTIVE SUMMARY**

**Proposal:**

The Molalla Fire District seeks approval for a Site Design Review and Variance for a new single-story accessory storage building and associated landscaping, stormwater pond, and site improvements. The building will serve as added storage for the existing emergency response facilities at Molalla Fire Station 82.

The applicant proposes to add the new pre-engineered building to Taxlot 8600, which is addressed at 414 N Molalla Ave and zoned PSP. The “Emergency Services” use category is outright permitted in the PSP zone. The applicant proposes to add a stormwater pond to unaddressed Taxlot 9300, which is zoned R-3. The “Utility Structures and Facilities, City Planned Projects; i.e., utilities identified by an adopted City master plan or development review approval” use category is outright permitted in the R-3 zone, and the stormwater pond would fall under the development review approval section. Site Design Review is the appropriate application for this proposal, and the applicant has applied for a Variance to address articulation requirements. No zone change is proposed or required for this application.

**Site Description:**

The site includes 6 lots (8400, 8500, 8600, 9300, 9500, and 9600) over 2.3 acres and is associated with addresses 320 N Molalla Avenue, 414 N Molalla, and 317 Kennel Avenue. The site includes the Fire Station, parking, vehicular circulation, open space, a residence that is used for training, and several storage sheds. The site has two existing approved accesses from N Molalla Avenue, and one existing access from Kennel Avenue. The primary Fire Station building is located in the southwest corner of the site on tax lot 8400 and 8500. To the west of the primary building, on tax lot 8600 there is an existing residential structure (to remain) and residential garage structure which is proposed to be demolished. The tax lot 9300 is vacant with grass cover. On tax lot 9500 is a small storage structure. Tax lot 9600 has another building to remain.

**Surrounding Zoning and Land Uses:**

Abutting uses include housing to the north and west, Molalla Civic Center and Leonard Long Park to the south, and commercial uses to the east across N Molalla Avenue. These adjacent properties are zoned General Commercial (C-1), Medium-High Density Residential (R-3), and Public/Semi-Public (PSP).

**Public Agency Notice & Responses:**

Staff circulated notice of the project to the City’s Public Works Division, Portland General Electric, and the Molalla Fire District on 5/13/2026. The City has included responses from Public Works as Exhibits C and comments are integrated into the proposed findings and conditions of this decision. The Molalla Fire District choose not to submit comments due to their conflict of interest, but has incorporated their feedback into the submitted applicant narrative.

**Public Notice & Comments:**

Per MMC 17-4.1.040, notice was published in the Herald-Independent on June 3, 2026. Notice of the public hearing was sent to all property owners within 300 feet of the subject properties on June 5, 2026. Notice was sent to interested parties on June 11, 2026. Signage containing public notice information was posted on the property on 6/15/2026. As of 6/19/2026, staff had received no written public comment on the application. If public comment is submitted between June 19, 2026, and the hearing it will be provided to the Planning Commission at the hearing and added to the record.

**II. Recommendation**

Based on the application materials and findings demonstrating present or conditioned compliance with the applicable standards, staff recommends approval of SDR03-2026 and VAR02-2026 subject to the Conditions of Approval that follow this recommendation. This approval is based on the Applicant’s written narrative, site plans, and supplemental application materials. Any modifications to the approved plans other than those required by the conditions of this decision will require a new land use application and approval.

**III. Conditions of Approval**

**1. Subsequent Required Approvals and Improvements:**

- a. The applicant shall be required to receive all required Building Permits and Occupancy from Clackamas County Building Department.
- b. No building permit may be issued until all required public facility improvements are in place and approved by the City Engineer, or otherwise bonded, in conformance with the

provisions of the Molalla Municipal Code (MMC) and the City of Molalla Standard Specifications for Public Works (MSSPW).

- c. Applicant shall submit a performance bond, irrevocable stand-by letter of credit, or a cash deposit that meets the requirements in MSSPW section 1.15.9 for public improvements, in a form and substance satisfactory to the City.
- d. Applicant shall conform to all emergency access, building, and fire code standards, as applicable at the time of building permit submittal.
- e. Separate engineering drawings reflecting the installation of public utilities will be required. Civil plans must be accepted prior to building permit authorization by the City.
  - i. All public facility improvements shall be completed and accepted by the City of Molalla prior to issuance of final occupancy.
  - ii. Applicant shall be required to dedicate a 10-foot-wide public utility easement fronting the public right-of-way if one does not exist. Applicant shall provide proof of existing dedication.
  - iii. The applicant shall record electrical and storm easements for crossover use. These easements shall be recorded prior to any occupancy.
  - iv. Applicant will be required to submit design and construction requirements for stormwater and surface water management at the time of Public Works Permit for the onsite private storm system for detention and water quality prior to discharge into a public facility. Design shall be in accordance with Section 3 of the Molalla Standard Specifications for Public Works Construction and Stormwater Master Plan. On-site private storm system shall comply with state plumbing code requirements. The detention and flow control facilities shall be reviewed, permitted, and inspected by Public Works. The onsite storm conveyance system shall be reviewed and inspected by Clackamas County Building under a plumbing permit. in Accordance with MMC 13.13 Surface Water Management.
  - v. Should Fire Department regulations require additional fire flow that results in looping the water line through the site, then the applicant's engineer shall coordinate with Public Works for the extension of the public waterline. Public waterline shall be sized appropriately to meet fire flows, and meet City of Molalla Public Works Standards, and all public water lines and hydrants shall be within a public utility easement approved by the Public Works Department.

- vi. Utility connections cannot be made until required permitting has been issued and all SDC & Connection Fees have been paid.
- vii. No construction of, or connection to, any existing or proposed public utility/improvements will be permitted until all plans are approved by Staff, all fees have been paid, bonding completed, all necessary permits, right-of-way and easements have been obtained and approved by staff.
- viii. Applicant shall pay all associated System Development Charges relating to the development. The Applicant shall also submit necessary information to calculate System Development Charges at the time of Building Permit submittal.
- ix. No work will be performed, nor materials stored, nor encroachment made on or within right-of-way, Public easement, or Public utility easement until all requirements have been met and permit has been issued.
- x. Plans submitted for review shall meet the requirements described in Section 1 of the Molalla Standard Specifications for Public Works Construction (MMC 17-3.6.080).
- xi. All public utility/improvement plans submitted for review shall be based upon a 22"x 34" format and shall be prepared in accordance with the City of Molalla Public Work's Standards (MMC 17-3.6.080).

Note: All conditions noted in this section are required prior to occupancy. Failure to comply with conditions of approval may result in code compliance action and revoking of this Conditional Use Permit Approval.

## 2. Ongoing Conditions:

- a. As an ongoing condition of approval, no visual obstruction (e.g., sign, structure, solid fence, or shrub vegetation) greater than 2.5 feet in height shall be placed in "vision clearance areas" at street intersections.
- b. As an ongoing Condition of Approval, all landscaping shall be maintained in good condition, or otherwise replaced by the property owner.
- c. As an ongoing Condition of Approval, fences and walls shall be maintained in good condition, or otherwise replaced by the property owner.

**Exhibit A:**

*City Staff's Findings of Fact for SDR03-2026 &  
VAR02-2026*

# Division IV Findings

## 17-4.1.040 Type III Procedure

Type III decisions are made by the Planning Commission after a public hearing, with an opportunity for appeal to the City Council.

**Findings:** This application seeks approval for Site Design Review (Type III) for a new 4,800 SF accessory storage building for Molalla Fire Station 82 and associated landscaping, utility connections, and storm pond. The applicant also requests Variance approval for alternate articulation methods as part of this application. Staff have found this application to comply or able to comply with all Type III procedural requirements. This criterion is met.

## Chapter 17-4.2 Site Design Review

### 17-4.2.030 Review Procedure

Site Design Review shall be conducted using the Type II procedure in Section 17-4.1.030, except that proposals exceeding any one of the thresholds below shall be reviewed using the Type III procedure in Section 17-4.1.040:

- A. The proposed use's estimated vehicle trip generation exceeds 100 average daily trips, based on the latest edition of the Institute of Transportation Engineers (ITE) Manual;
- B. The use exceeds 5,000 square feet of gross leasable floor area; or the project involves more than one acre total site area;
- C. The proposal involves a Conditional Use (new or expanded);
- D. The proposal involves a variance under Chapter 17-4.7;
- E. The proposal involves expansion of a nonconforming use; or
- F. The Planning Official determines that, due to the nature of the proposal, a public hearing is the most effective way to solicit public input in reviewing the application.

**Findings:** This consolidated application includes a Variance and involves more than one acre of total site area, as such, it qualifies as a Type III application. The standards and procedure of a Type III application have been followed. This criterion is met.

### 17-4.2.040 Application Submission Requirements

All of the following information is required for Site Design Review application submittal, except where the Planning Official and the City Engineer determines that some information is not pertinent and therefore is not required.

**Findings:** The applicant has submitted sufficient information for the Site Design Review and Variance for Staff to adequately evaluate this application package. This criterion is met.

#### **17-4.2.050 Approval Criteria**

An application for Site Design Review shall be approved if the proposal meets all of the following criteria. The Planning Official, in approving the application, may impose reasonable conditions of approval, consistent with the applicable criteria.

- A. The application is complete, in accordance with Section 17-4.2.040;

**Finding:** As discussed in the responses to Section 17-4.2.040, this application includes the required submittal materials. This criterion is satisfied.

## Division II Findings

- B. The application complies with all of the applicable provisions of the underlying Zoning District (Division II), including, but not limited to, building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards;

**Findings:** This application complies with all applicable provisions of the underlying Zoning District (Division II). The Applicant has applied for a Variance to address articulation standards. See responses to Division II requirements below:

### 17-2.2.030 Allowed Uses

**Findings:** The Applicant is proposing to add a pre-engineered storage building to Taxlot 8600, and a storm detention facility to Taxlot 9300. Taxlot 8600 is zoned Public and Semi Public (PSP) and Taxlot 9300 is zoned Medium-High Density Residential (R-3). Per MMC Table 17-2.2.030, “Emergency Services, includes Police, Fire, Ambulance” is outright allowed in the PSP zone and “Utility Structures and Facilities, City Planned Projects; i.e., utilities identified by an adopted City master plan or development review approval” is outright permitted in the R-3 zone. The applicant has applied for a Site Design Review in accordance with this standard. This criterion is met.

### 17-2.2.040 Lot and Development Standards

Standard	PSP Standard	Proposed
Minimum Lot Area	None	Not Applicable, meets
Minimum Lot Width and Depth	None	Not Applicable, meets
Maximum Building Height	55 feet	24’6.25”, meets
Lot Coverage	Up to 100%	Meets
Front, Interior Side, Rear Setback	0’	Front: 110’ Interior Side: 10.5’ Rear: 10.5’, meets
Build-to-Line	None	Not Applicable, meets

**Findings:** As shown in the table, the proposed project meets all the lot and development standards. This criterion is met.

## Division III Findings

- C. The proposal complies with all of the Development and Design Standards of Division III, as applicable, including, but not limited to:
1. Chapter 17-3.3 Access and Circulation,
  2. Chapter 17-3.4 Landscaping, Fences and Walls, Outdoor Lighting,
  3. Chapter 17-3.5 Parking and Loading,
  4. Chapter 17-3.6 Public Facilities, and

**Findings:** This staff report addresses the relevant standards of Division III. With the Variance, all standards are met, or can be met, subject to the Conditions of Approval. As this application includes a new non-residential building, applicable standards also include 17-3.2 Building Orientation and Design, specifically Section 040 Non-Residential Buildings. Please see responses to Division III standards below:

### 17-3.2 Building Orientation and Design

#### 17-3.2.040 Non-Residential Buildings

- A. Purpose and Applicability. The following requirements apply to non-residential development, including individual buildings and developments with multiple buildings such as shopping centers, office complexes, mixed-use developments, and institutional campuses. The standards are intended to create and maintain a built environment that is conducive to pedestrian accessibility, reducing dependency on the automobile for short trips, while providing civic space for employees and customers, supporting natural surveillance of public spaces, and creating human-scale design. The standards require buildings placed close to streets, with storefront windows (where applicable), with large building walls divided into smaller planes, and with architectural detailing.

**Findings:** This application is for a non-residential building and associated development; thus, it is subject to the Non-Residential Building Orientation and Design standards.

- B. Building Orientation. The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.
1. Buildings subject to this section shall conform to the applicable build-to line standard in Table 17-2.2.040.E, ...

**Findings:** This application is subject to Site Design Review; thus, it is subject to Building Orientation standards. As shown in Table 17-2.2.040.E, the PSP Zone does not have a Build-to-Line. This criterion is met.

3. Except as provided in subsections C.5 and 6, all buildings shall have at least one primary entrance (i.e., tenant entrance, lobby entrance, breezeway entrance, or courtyard entrance) facing an abutting street (i.e., within 45 degrees of the street property line); or if the building entrance must be turned more than 45 degrees from the street (i.e., front door is on a side or rear elevation) due to the configuration of the site or similar constraints, a pedestrian walkway must connect the primary entrance to the sidewalk in conformance with Section 17-3.3.040.

**Findings:** The proposed storage building will not be accessed by the public, so it does not have a public entrance. However, the proposed building does have an entrance facing the street to which it is oriented, N Molalla Avenue. This criterion is met.

4. Off-street parking, trash storage facilities, and ground-level utilities (e.g., utility vaults), and similar obstructions shall not be placed between building entrances and the street(s) to which they are oriented. To the extent practicable, such facilities shall be oriented internally to the block and accessed by alleys or driveways.

**Findings:** As shown on the submitted site plans, the site has existing off-street parking to serve the public, visitors, and staff. Some of the parking spaces are located between the main fire station and Molalla Avenue, but this is pre-existing nonconforming development. The creation of the new storage building will not increase the nonconformity of the site. No new parking is proposed as part of the current project. Similarly, the project does not propose new trash storage facilities or ground level utilities. These facilities will not be located between the new building's entrance and the streets to which the new building is oriented. This criterion is met.

4. Off-street parking shall be oriented internally to the site to the extent practicable, and shall meet the Access and Circulation requirements of Chapter 17-3.3, the Landscape and Screening requirements of Chapter 17-3.4, and the Parking and Loading requirements of Chapter 17-3.5.

**Findings:** As shown on submitted site plans, the site has existing off-street parking to serve the public, visitors, and staff. Some of the parking spaces are located between the main fire station and Molalla Avenue, but this is pre-existing nonconforming development. No new parking is proposed as part of the current project, and the proposed project does not increase the nonconformity of the site. This criterion is met.

5. Where a development contains multiple buildings and there is insufficient street frontage to meet the above building orientation standards for all buildings on the subject site, a building's primary entrance may orient to plaza, courtyard, or similar pedestrian space containing pedestrian amenities and meeting the requirements under Section 17-3.2.050, subject to Site Design Review approval. When oriented this way, the primary entrance(s), plaza, or courtyard shall be

connected to the street by a pedestrian walkway conforming to Section 17-3.3.040.

**Findings:** As shown on the submitted site plans, the proposed building has an entrance facing N Molalla Avenue. This criterion is met.

- C. Large-Format Developments. Plans for new developments, or any phase thereof, with a total floor plate area (ground floor area of all buildings) greater than 35,000 square feet, shall meet all of the following standards [...]

**Findings:** The proposed building will contain approximately 4,800 SF, below the 35,000 SF trigger for large-format developments. These standards do not apply.

- D. Primary Entrances and Windows. The following standards, as generally illustrated in Figures 17-3.2-8 and 17.3.2-9, apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.

**Findings:** This application is subject to Site Design Review; thus, these standards apply.

1. All Elevations of Building. Architectural designs shall address all elevations of a building. Building forms, detailing, materials, textures, and color shall contribute to a unified design with architectural integrity. Materials used on the front façade must turn the building corners and include at least a portion of the side elevations, consistent with the overall composition and design integrity of the building.

**Findings:** As depicted the submitted site plans, the proposed building uses detailing, materials, textures, and color on all four (4) elevations. The same materials and color themes are used on all facades, maintaining consistent composition and design. This criterion is met.

2. Pedestrian Entrances. Ground level entrances oriented to a street shall be at least partly transparent for natural surveillance and to encourage an inviting and successful business environment. This standard may be met by providing a door with a window or windows, a transom window above the door, or sidelights beside the door. Where ATMs or other kiosks are proposed on any street-facing elevation, they shall be visible from the street for security and have a canopy, awning, or other weather protection shelter.

**Findings:** As shown on the submitted site plans, a pedestrian entrance is proposed on the east elevation facing N Molalla Avenue. The submitted building elevations show a transom window above the door. No ATMs or kiosks are proposed. This criterion is met.

3. Corner Entrances. Buildings on corner lots are [...]

**Findings:** The site is not a corner lot. This standard does not apply.

4. Street Level Entrances. All primary building entrances shall open to the sidewalk and shall conform to Americans with Disabilities Act (ADA) requirements, as applicable. Primary entrances above or below grade may be allowed where ADA accessibility is provided.

**Findings:** As shown on the submitted site plans, the proposed building entrances conform to ADA requirements and is at grade. This standard is met.

5. Windows—General. Except as approved for parking structures or accessory structures, the front/street-facing elevations of buildings shall provide display windows, windowed doors, and where applicable, transom windows to express a storefront character.

**Findings:** The proposed building is an accessory structure designed for storage, however, as shown on the submitted elevations, the street-facing front (east) façade expresses a storefront character by providing a door with a transom window and two (2) windows at the mezzanine level. The design also includes smooth fiber cement insets surrounded by painted trim to resemble windows. The applicant proposes to paint these with community murals. The faux windows are proposed to meet the window exception of Section 17-3.2.040.D.12 below. Any murals would need mural approval through the City, which this approval would not provide. This criterion is met.

6. Storefront Windows. Storefront windows shall consist of framed picture or bay windows, which may be recessed. Framing shall consist of trim detailing such as piers or pilasters (sides), lintels or hoods (tops), and kick plates or bulkheads (base)—or similar detailing—consistent with a storefront character. The ground floor, street-facing elevation(s) of all buildings shall comprise at least 60 percent transparent windows, measured as a section extending the width of the street-facing elevation between the building base (or 30 inches above the sidewalk grade, whichever is less) and a plane 72 inches above the sidewalk grade.

**Findings:** As shown on the submitted building elevations, the east (front/street-facing) elevation is proposed to have less than 1% actual glazing (the transom window) within the specified heights but is proposed to have 60% of the ground floor façade occupied by the window-like framed mural insets (requested as a window exception per 17-3.2.040.D.12 below) and two (2) additional framed windows at the mezzanine level above the framed mural insets, adding an additional 34 SF of glazing to the façade (though not within the designated ground-level area). Due to security concerns, it is not practical to use glass windows in the lower portion of the building. Additionally, UV light is detrimental to the fire department turnout equipment that will

be stored on the ground level of the proposed building. Upper glass windows have been provided to satisfy daylighting requirements of the energy code. This standard will be met with approval of the window exception requested under item 12 below.

7. **Defined Upper Story(ies).** Building elevations shall contain detailing that visually defines street level building spaces (storefronts) from upper stories. The distinction between street level and upper floors shall be established, for example, through the use of awnings, canopies, belt course, or similar detailing, materials, or fenestration. Upper floors may have less window area than ground floors, but shall follow the vertical lines of the lower level piers and the horizontal definition of spandrels and any cornices. Upper floor window orientation shall primarily be vertical, or have a width that is no greater than height. Paired or grouped windows that, together, are wider than they are tall, shall be visually divided to express the vertical orientation of individual windows.

**Findings:** While a mezzanine is proposed for storage, the proposal consists of a single-story building as shown on the submitted building elevations. Upper stories are not proposed. This standard does not apply.

8. **Buildings Not Adjacent to a Street.** Buildings that are not adjacent to a street or a shopping street, such as those that are setback behind another building and those that are oriented to a civic space (e.g., internal plaza or court), shall meet the 60 percent transparency standard on all elevations abutting civic space(s) and on elevations containing a primary entrance.

**Findings:** The proposed building is set back behind (west of) an existing residential structure and is not oriented to a civic space. The primary entrance is on the façade that faces N Molalla Avenue. This facade is held to the 60% transparency requirement, as is addressed throughout this section.

9. **Side and Rear Elevation Windows.** All side and rear elevations, except for zero lot line or common wall elevations, where windows are not required, shall provide not less than 30 percent transparency.

**Findings:** The applicant is requesting a window exception due to security and UV damage concerns under section 17-3.2.040.D.12, and this is addressed below. The north and west elevations are proposed to have framed panels similar to the east (street facing) façade. The proposed framed inset panels are provided for 30% of the north and west elevations as shown on the submitted building elevations. Additionally, the south side elevation contains four garage doors that each contain a row of glazing. This standard will be met with approval of the window exception requested under subsection 12 below.

10. **Window Trim.** At a minimum, windows shall contain trim, reveals, recesses, or similar detailing of not less than four inches in width or depth as applicable. The

use of decorative detailing and ornamentation around windows (e.g., corbels, medallions, pediments, or similar features) is encouraged.

**Findings:** As discussed above and in paragraph 12 below, a window exception is requested to use inset panels to meet the window requirements. The proposed inset panels contain 4" wide trim. This criterion is met.

11. Projecting Windows, Display Cases. Windows and display cases shall not break the front plane of the building (e.g., projecting display boxes are discouraged). [...]

**Findings:** Projecting windows are not proposed. This standard does not apply.

12. Window Exceptions. The Planning Official may approve an exception to the above standards where existing topography makes compliance impractical. Where it is not practicable to use glass, windows for parking garages or similar structures, the building design must incorporate openings or other detailing that resembles window patterns (rhythm and scale).

**Findings:** A window exception is requested for the proposed accessory storage building. Applicant's proposal is for a vehicle storage facility, which is functionally similar to the above mentioned "parking garage" and is not intended to provide internal ambiance. Fire Department Staff will only occupy this facility to retrieve and potentially service the emergency vehicles. Due to security concerns, it is not practical to use glass windows in the lower portion of the building. Additionally, UV light is detrimental to the fire department turnout equipment that will be stored on the ground level of the proposed building. Upper glass windows have been provided to satisfy daylighting requirements of the energy code, but ground level street facing façade windows, and rear and side façade window requirements are proposed to be met with inset smooth fiber cement panels and trim, resembling window patterns in rhythm and scale. The applicant has proposed that the panels on the street facing façade are to be painted with murals reflective of the surrounding community. This application does not constitute mural approval, and the applicant would be required to secure all appropriate mural approvals prior to painting any murals. This criterion is met.

- E. Articulation and Detailing. The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.
  1. Articulation. All building elevations that orient to a street or civic space shall have breaks in the wall plane (articulation) of not less than one break for every 30 feet of building length or width, as applicable, pursuant to the following standards, which are generally illustrated in Figures 17-3.2-10, 17-3.2-11, and 17-3.2-12.
    - a. A "break" for the purposes of this subsection is a change in wall plane of not less than 24 inches in depth. Breaks may include, but are not limited to, an

offset, recess, window reveal, pilaster, frieze, pediment, cornice, parapet, gable, dormer, eave, coursing, canopy, awning, column, building base, balcony, permanent awning or canopy, marquee, or similar architectural feature.

- b. The Planning Official through Site Design Review may approve detailing that does not meet the 24-inch break-in-wall-plane standard where it finds that proposed detailing is more consistent with the architecture of historically significant or historic-contributing buildings existing in the vicinity.
- c. Changes in paint color and features that are not designed as permanent architectural elements, such as display cabinets, window boxes, retractable and similar mounted awnings or canopies, and other similar features, do not meet the 24-inch break-in-wall-plane standard.
- d. Building elevations that do not orient to a street or civic space need not comply with the 24-inch break-in-wall-plane standard but should complement the overall building design.

**Findings:** In lieu of 24-inch offsets every 30', the east building elevation (facing Molalla Avenue) includes articulation in the form of changes in materials, textures, colors, enhanced trim, and inset smooth concrete mural panels. As shown on the submitted building elevations, vertically ribbed siding creates texture, and the decorative red trim breaks the plane into multiple segments while also highlighting the features of the façade. These features include the smooth fiber cement insets that act as window-like insets, as well as the proposed mezzanine level windows and personnel door. The maximum horizontal spacing between the trim and the elements contained within is 12'. The architecture is complementary of the primary Fire Station building. The applicant has applied for a Variance to request alternate articulation methods, and it is addressed below.

2. Change in Materials. Elevations should incorporate changes in material that define a building's base, middle, and top, as applicable, and create visual interest and relief. Side and rear elevations that do not face a street, public parking area, pedestrian access way, or plaza may utilize changes in texture and/or color of materials, provided that the design is consistent with the overall composition of the building.

**Findings:** As depicted on the submitted building elevations, the proposed building incorporates changes in texture and color reflective of the theme of the existing primary fire station building. The storage building offers white vertically ribbed siding, red trim around the windows (and window-like panels), and a grey roof. The faux window panels area a different material, texture, and color from the vertically ribbed white siding, and the roof will also be a different color and texture from the vertically ribbed white siding. These elements differentiate the base, middle, and top of the building. This criterion is met.

3. **Horizontal Lines.** New buildings and exterior remodels shall generally follow the prominent horizontal lines existing on adjacent buildings at similar levels along the street frontage. Examples of such horizontal lines include, but are not limited to: the base below a series of storefront windows, an awning or canopy line, a belt course between building stories, a cornice, or a parapet line. Where existing adjacent buildings do not meet the City's current building design standards, a new building may establish new horizontal lines.

**Findings:** As shown on the submitted plans, the proposed accessory storage building is not located along the street frontage, as the façade is located over 100' from the street lot line. Additionally, over 50% of the proposed building is behind (west of) an existing residential house which is located between the new building and the street lot line. The visible portion of the street facing façade will also be partially blocked by the proposed 6' high slatted chain link fence. Furthermore, the adjacent buildings do not provide consistent horizontal lines as the buildings to the north are residential houses, to the south is the primary fire station building, and across the street is the Hi-School Pharmacy, all of which have different horizontal lines. Due to the proximity to the residential housing, the building is proposed to generally follow the lines of nearby residential houses (the structures located closest to the proposed new building). This is done through the use of the peaked roof line and mezzanine level windows that emulate residential second story windows. This criterion is met.

4. **Ground Floor and Upper Floor Division.** A clear visual division shall be maintained between the ground level floor and upper floors, for example, through the use of a belt course, transom, awning, canopy, or similar division.

**Findings:** The proposal consists of a one-story building. Therefore, no division between the ground floor and upper floors is needed. However, as shown on the submitted building elevations, there are windows high up that mimic residential second story windows, and this adds to the perceived division between ground and upper floors. This standard does not apply.

5. **Vertical Rhythms.** New construction or front elevation remodels shall reflect a vertical orientation, either through breaks in volume or the use of surface details.

**Findings:** As depicted on the submitted building elevations, the proposed building establishes a vertical rhythm through the use of vertically ribbed siding, mezzanine level window placement, and peaked roof. This criterion is met.

- F. **Pedestrian Shelters.** The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.

1. **Minimum Pedestrian Shelter Coverage.** Permanent awnings, canopies, recesses, or similar pedestrian shelters shall be provided along at least 75 percent of the

ground floor elevation(s) of a building where the building abuts a sidewalk, civic space, or pedestrian access way. [...]

**Findings:** This application does not include buildings that abut public sidewalks, accessways, or civic space areas, therefore a pedestrian shelter in accordance with the above standards is not required.

#### G. Mechanical Equipment.

1. Building Walls. Where mechanical equipment, such as utility vaults, air compressors, generators, antennae, satellite dishes, or similar equipment, is permitted on a building wall that abuts a public right-of-way or civic space, it shall be screened pursuant to Chapter 17-3.4. Standpipes, meters, vaults, and similar equipment need not be screened but shall not be placed on a front elevation when other practical alternatives exist; such equipment shall be placed on a side or rear elevation where practical.

**Findings:** This application does not include exterior mechanical equipment abutting a public right-of-way or civic space. This criterion is not applicable.

2. Rooftops. Except as provided below, rooftop mechanical units shall be set back or screened behind a parapet wall so that they are not visible from any public right-of-way or civic space. [...]

**Findings:** Rooftop mechanical units are not included in the proposed application. This criterion is not applicable.

3. Ground-Mounted Mechanical Equipment. Ground-mounted equipment, such as generators, air compressors, trash compactors, and similar equipment, shall be limited to side or rear yards and screened with fences or walls constructed of materials similar to those on adjacent buildings. Hedges, trellises, and similar plantings may also be used as screens where there is adequate air circulation and sunlight, and irrigation is provided. The City may require additional setbacks and noise attenuating equipment for compatibility with adjacent uses.

**Findings:** No ground-mounted mechanical equipment is proposed. This standard does not apply.

#### H. Civic Space. Commercial development projects shall provide civic space pursuant to Section 17-3.2.050.

**Findings:** Section 17-3.2.050 applies only to commercial and mixed-use developments within the Central Commercial C-1 and General Commercial C-2 zones. This project is in the Public and Semi Public zone, and so this criterion does not apply.

## 17-3.3 Access and Circulation

### 17-3.3.030 Vehicular Access and Circulation

- A. Purpose and Intent. Section 17-3.3.030 implements the street access policies of the City of Molalla Transportation System Plan. It is intended to promote safe vehicle access and egress to properties, while maintaining traffic operations in conformance with adopted standards. "Safety," for the purposes of this chapter, extends to all modes of transportation.
- B. Permit Required. Vehicular access to a public street (e.g., a new or modified driveway connection to a street or highway) requires an approach permit approved by the applicable roadway authority.

**Findings:** No new driveways are proposed. This standard does not apply.

- C. Traffic Study Requirements. The City, in reviewing a development proposal or other action requiring an approach permit, may require a traffic impact analysis, pursuant to Section 17-3.6.020, to determine compliance with this Code.

**Findings:** The Transportation Analysis Letter provided by DKS Associates shows the project is not expected to generate any new trips. City Engineering concurs with this finding. This criterion is met.

- D. Approach and Driveway Development Standards. Approaches and driveways shall conform to all of the following development standards: [...]
  - 1. The number of approaches on higher classification streets (e.g., collector and arterial streets) shall be minimized; where practicable, access shall be taken first from a lower classification street.

**Findings:** No new approaches or modifications to existing approaches are proposed. There are currently three driveways on N Molalla Avenue, for both public and emergency response vehicles. There is also an existing driveway on Kennel Avenue used for site circulation. This project does not alter any of the approaches, which are considered pre-existing nonconforming, and the project does not increase the nonconformity of the site.

- 2. Approaches shall conform to the spacing standards of subsections E and F, below, and shall conform to minimum sight distance and channelization standards of the roadway authority.

**Findings:** No new approaches or modifications to existing approaches are proposed. The site has three existing approaches, two on N Molalla Ave and one on Kennel Ave, that conform to approach spacing requirements. As discussed in the applicant's submitted Traffic Analysis Letter (TAL), the sight distance requirements are met and there are no safety concerns associated with

sight distance at the driveways. Neither abutting street has existing channelization along the project frontage. This criterion is met.

3. Driveways shall be paved and meet applicable construction standards. Where permeable paving surfaces are allowed or required, such surfaces shall conform to applicable Public Works Design Standards.

**Findings:** As shown on the submitted site plans, the existing driveways are paved. This standard is met.

4. The City Engineer may limit the number or location of connections to a street, or limit directional travel at an approach to one-way, right-turn only, or other restrictions, where the roadway authority requires mitigation to alleviate safety or traffic operations concerns.
5. Where the spacing standards of the roadway authority limit the number or location of connections to a street or highway, the City Engineer may require a driveway extend to one or more edges of a parcel and be designed to allow for future extension and inter-parcel circulation as adjacent properties develop. The City Engineer may also require the owner(s) of the subject site to record an access easement for future joint use of the approach and driveway as the adjacent property(ies) develop(s).

**Findings:** Adjacent site access through an emergency services facility is not appropriate and will not be recommended by Staff. There is already intra-site connectivity between its several parcels. City Engineering has not requested any mitigation. This standard does not apply.

6. Where applicable codes require emergency vehicle access, approaches and driveways shall be designed and constructed to accommodate emergency vehicle apparatus and shall conform to applicable fire protection requirements. The City Engineer may restrict parking, require signage, or require other public safety improvements pursuant to the recommendations of an emergency service provider.
7. As applicable, approaches and driveways shall be designed and constructed to accommodate truck/trailer-turning movements.

**Findings:** As discussed, no new approaches or driveways are proposed. Existing driveways are designed and regularly used by emergency vehicles. New on-site vehicle circulation areas have been designed to meet applicable fire protection requirements and accommodate anticipated turning movements. This criterion is met.

8. Except where the City Engineer and roadway authority, as applicable, permit an open access with perpendicular or angled parking, driveways shall accommodate

all projected vehicular traffic on-site without vehicles stacking or backing up onto a street.

**Findings:** No new trips are proposed, and current driveways accommodate all vehicular traffic on-site without vehicles stacking or backing up onto a street. This criterion is met.

9. Driveways shall be designed so that vehicle areas, including, but not limited to, drive-up and drive-through facilities and vehicle storage and service areas, do not obstruct any public right-of-way.

**Findings:** No new driveways are proposed, and the existing conditions are designed such that vehicle areas do not obstruct any public right-of-way as shown on the submitted site plans. No drive-up or drive-through facilities are proposed. This criterion is met.

10. Approaches and driveways shall not be wider than necessary to safely accommodate projected peak hour trips and turning movements, and shall be designed to minimize crossing distances for pedestrians.

**Findings:** No new driveways or approaches are proposed. The southern two driveways are in close proximity to each other, but serve different purposes. The middle driveway is for emergency response vehicles, and the southern driveway is to access the office and for the public. This criterion is met.

11. As it deems necessary for pedestrian safety, the City Engineer, in consultation with the roadway authority, as applicable, may require that traffic-calming features, textured driveway surfaces (e.g., pavers or similar devices), curb extensions, signage or traffic control devices, or other features, be installed on or in the vicinity of a site as a condition of development approval.

**Findings:** City Engineering has not requested traffic calming features. This criterion does not apply.

12. Construction of approaches along acceleration or deceleration lanes, and along tapered (reduced width) portions of a roadway, shall be avoided; except where no reasonable alternative exists and the approach does not create safety or traffic operations concern.

**Findings:** As shown on the submitted site plans, the site does not abut acceleration or decelerations lanes or tapered portions of a roadway. This standard does not apply.

13. Approaches and driveways shall be located and designed to allow for safe maneuvering in and around loading areas, while avoiding conflicts with pedestrians, parking, landscaping, and buildings.

**Findings:** No new driveways, approaches, or loading areas are proposed and the site does not have existing loading areas. This standard does not apply.

14. Where sidewalks or walkways occur adjacent to a roadway, driveway aprons constructed of concrete shall be installed between the driveway and roadway edge. The roadway authority may require the driveway apron be installed outside the required sidewalk or walkway surface, consistent with Americans with Disabilities Act (ADA) requirements, and to manage surface water runoff and protect the roadway surface.
15. Where an accessible route is required pursuant to ADA, approaches and driveways shall meet accessibility requirements where they coincide with an accessible route.

**Findings:** The existing sidewalks and concrete driveway aprons were constructed to City standards to ensure compliance with applicable provisions of the ADA and to properly manage stormwater runoff. No changes are proposed to public sidewalks or aprons. This criterion is met.

16. The City Engineer may require changes to the proposed configuration and design of an approach, including the number of drive aisles or lanes, surfacing, traffic-calming features, allowable turning movements, and other changes or mitigation, to ensure traffic safety and operations.

**Findings:** City Engineering has not requested any changes. This standard does not apply.

17. Where a new approach onto a state highway or a change of use adjacent to a state highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The City Engineer may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case the City will work cooperatively with the applicant and ODOT to avoid unnecessary delays.

**Findings:** N Molalla Avenue and Kennel Avenue are local streets under the jurisdiction of the City of Molalla. This standard does not apply.

18. Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.
19. Where a proposed driveway crosses a culvert or drainage ditch, the City Engineer may require the developer to install a culvert extending under and beyond the edges of the driveway on both sides of it, pursuant to applicable Public Works Design Standards.

**Findings:** The existing driveways do not cross a drainage ditch, culvert, canal, railroad, or other feature that is under the jurisdiction of another agency. Additionally, no new approach or driveway is proposed. This standard does not apply.

- 20. Except as otherwise required by the applicable roadway authority or waived by the City Engineer temporary driveways providing access to a construction site or staging area shall be paved or graveled to prevent tracking of mud onto adjacent paved streets.

**Findings:** The site has existing driveways that can be used for construction access. They are paved in accordance with this standard.

- 21. Development that increases impervious surface area shall conform to the storm drainage and surface water management requirements of Section 17-3.6.050.

**Findings:** This standard is addressed below under 17-3.6.050.

- E. Approach Separation from Street Intersections. Except as provided by subsection H, minimum distances shall be maintained between approaches and street intersections consistent with the current version of the Public Works Design Standards and Transportation System Plan.

**Findings:** No new driveways, approaches, or modifications to existing driveways or approaches are proposed, and the existing driveways are appropriately spaced from intersections. The previously approved middle driveway acts as a fourth entrance into a three way stop. This is the exit that the emergency vehicles take, and exiting onto a controlled stopped intersection is perfect for the emergency vehicles that control traffic.

- F. Approach Spacing. Except as provided by subsection H or as required to maintain street operations and safety, the following minimum distances shall be maintained between approaches consistent with the current version of the Public Works Design Standards and Transportation System Plan.

**Findings:** No new driveways, approaches, or modifications to existing driveways or approaches are proposed, and the existing driveway approaches conform to City approach spacing standards.

- G. Vision Clearance. No visual obstruction (e.g., sign, structure, solid fence, or shrub vegetation) greater than 2.5 feet in height shall be placed in "vision clearance areas" at street intersections. The minimum vision clearance area may be modified by the Planning Official through a Type I procedure, upon finding that more or less sight distance is required (i.e., due to traffic speeds, roadway alignment, etc.). Placement of light poles, utility poles, and tree trunks should be avoided within vision clearance areas.

**Findings:** As shown on the submitted site plans, the applicant has marked the clear vision triangles. As an ongoing condition of approval, no visual obstruction (e.g., sign, structure, solid fence, or shrub vegetation) greater than 2.5 feet in height shall be placed in "vision clearance areas" at street intersections.

- H. Exceptions and Adjustments. The City Engineer may approve adjustments to the spacing standards of subsections E and F, above, where an existing connection to a City street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance. The Planning Official through a Type II procedure may also approve a deviation to the spacing standards on City streets where it finds that mitigation measures, such as consolidated access (removal of one access), joint use driveways (more than one property uses same access), directional limitations (e.g., one-way), turning restrictions (e.g., right-in/right-out only), or other mitigation alleviate all traffic operations and safety concerns.

**Findings:** No adjustments to spacing standards are proposed. This standard does not apply.

- I. Joint Use Access Easement and Maintenance Agreement. Where the City approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use of and cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed, defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the City for its records, but the City is not responsible for maintaining the driveway or resolving any dispute between property owners.

**Findings:** No joint use driveways are proposed. This standard does not apply.

### 17-3.3.040 Pedestrian Access and Circulation

- A. Purpose and Intent. Section 17-3.3.040 implements the pedestrian access and connectivity policies of the City of Molalla Transportation System. It is intended to provide for safe, reasonably direct, and convenient pedestrian access and circulation.
- B. Standards. Developments shall conform to all of the following standards for pedestrian access and circulation as generally illustrated in Figure 17-3.3-3:
  - 1. Continuous Walkway System. A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.

**Findings:** No new pedestrian walkways are proposed as part of the storage building development. The proposed building will not be accessed by the public and only necessary staff will use the building, which is connected to the rest of the site by paved fire truck circulation areas. The existing fire station is connected to adjacent sidewalks via an existing pedestrian

walkway. This standard is met to the extent applicable, and does not apply to the proposed storage building.

2. Vehicle/Walkway Separation. Except as required for crosswalks, per subsection 4, below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the Planning Official may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.

**Findings:** The existing pedestrian walkway is separated from vehicle areas by curb stops. This section of the development is pre-existing nonconforming, and the addition of the proposed storage building does not increase the nonconformity of the development. This criterion is met.

3. Crosswalks. Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrasting material). The crosswalk may be part of a speed table to improve driver-visibility of pedestrians. Painted or thermo-plastic striping and similar types of non-permanent applications are discouraged, but may be approved for lesser used crosswalks not exceeding 24 feet in length.

**Findings:** The crosswalks are not proposed to be altered as part of this project, and are considered pre-existing nonconforming. The addition of the proposed storage building does not increase the nonconformity of the development. This criterion is met.

4. Walkway Width and Surface. Walkways, including access ways required for subdivisions pursuant to Chapter 17-4.3, shall be constructed of concrete, asphalt, brick or masonry pavers, or other durable surface, as approved by the City Engineer, and not less than six feet wide. Multi-use paths (i.e., designed for shared use by bicyclists and pedestrians) shall be concrete or asphalt and shall conform to the current version of the Public Works Design Standards and Transportation System Plan.
5. Walkway Construction (Private). Walkway surfaces may be concrete, asphalt, brick or masonry pavers, or other City-approved durable surface meeting ADA requirements. Walkways shall be not less than six feet in width in commercial and mixed use developments and where access ways are required for subdivisions under Division IV.
6. Multi-Use Pathways. Multi-use pathways, where approved, shall be a minimum width and constructed of materials consistent with the current version of the Public Works Design Standards and Transportation System Plan.

**Findings:** This project does not include new walkways or multi-use pathways. These standards do not apply.

## 17-3.4 Landscaping, Fences and Walls, Outdoor Lighting

### 17-3.4.020 Applicability

- A. Section 17-3.4.030 establishes design standards for landscaping and screening. Projects requiring Site Design Review or Land Division approval shall meet the landscape standards of the applicable zone, including the standards in Tables 17-2.2.040.D and 17-2.2.040.E and any Special Use requirements under Chapter 17-2.3, and the requirements of Section 17-3.4.030. Property owners are required to maintain landscaping and screening pursuant to Section 17-3.4.030.G.
- B. Section 17-3.4.040 establishes design standards for when a fence, or a wall not attached to a building, is to be erected, extended, or otherwise altered. It also applies to situations where this Code requires screening or buffering (e.g., outdoor or unenclosed storage uses). The standards of Section 17-3.4.040 supplement the development standards in Tables 17-2.2.030 and 17-2.2.040 and any applicable Special Use requirements under Chapter 17-2.3.
- C. Section 17-3.4.050, Outdoor Lighting, applies to all new outdoor lighting, i.e., lighting that is installed after November 10, 2017.
- D. The Planning Official, through a Type II procedure, may grant adjustments to Chapter 17-3.4, pursuant to the criteria of Chapter 17-4.7 Adjustments and Variances.

**Findings:** This application is subject to this section as it includes an application for Site Design Review. As shown on the submitted site plans, all areas within the 'Area of Disturbance' not utilized for buildings, accessory structures, vehicle maneuvering areas, or parking stalls will be landscaped. This criterion is met.

### 17-3.4.030 Landscaping and Screening

- A. General Landscape Standard. All portions of a lot not otherwise developed with buildings, accessory structures, vehicle maneuvering areas, or parking shall be landscaped.
- B. Minimum Landscape Area. All lots shall conform to the minimum landscape area standards of the applicable zoning district, as contained in Tables 17-2.2.040.D and 17-2.2.040.E. The Planning Official, consistent with the purposes in Section 17-3.4.010, may allow credit toward the minimum landscape area for existing vegetation that is retained in the development.

**Findings:** Approximately 27,850 SF or 27.8% of the site will be vegetated. This includes existing vegetation on tax lot 9300 which will be maintained, or repaired to existing conditions, during the proposed development. This satisfies the 20% minimum landscape area standard of Table 17-2.2.040.D and the 10% minimum landscape area standard of Table 17-2.2.040.E. This criterion is met.

- C. Plant Selection. A combination of deciduous and evergreen trees, shrubs, and ground covers shall be used for all planted areas, the selection of which shall be based on local

climate, exposure, water availability, and drainage conditions, among other factors. When new vegetation is planted, soils shall be amended and irrigation shall be provided, as necessary, to allow for healthy plant growth. The selection of plants shall be based on all of the following standards and guidelines:

1. Use plants that are appropriate to the local climate, exposure, and water availability. The presence of utilities and drainage conditions shall also be considered.
2. Plant species that do not require irrigation once established (naturalized) are preferred over species that require irrigation.
3. Trees shall be not less than two-inch caliper for street trees and one and one-half-inch caliper for other trees at the time of planting. Trees to be planted under or near power lines shall be selected so as to not conflict with power lines at maturity.
4. Shrubs shall be planted from five-gallon containers, minimum, where they are for required screens or buffers, and two-gallon containers minimum elsewhere.
5. Shrubs shall be spaced in order to provide the intended screen or canopy cover within two years of planting.
6. All landscape areas, whether required or not, that are not planted with trees and shrubs or covered with allowable non-plant material, shall have ground cover plants that are sized and spaced to achieve plant coverage of not less than 75 percent at maturity.
7. Bark dust, chips, aggregate, or other non-plant ground covers may be used, but shall cover not more than 35 percent of any landscape area. Non-plant ground covers cannot be a substitute for required ground cover plants.
8. Where stormwater retention or detention, or water quality treatment facilities are proposed, they shall meet the requirements of the current version of the Public Works Design Standards.
9. Existing mature trees that can thrive in a developed area and that do not conflict with other provisions of this Code shall be retained where specimens are in good health, have desirable aesthetic characteristics, and do not present a hazard.
10. Landscape plans shall avoid conflicts between plants and buildings, streets, walkways, utilities, and other features of the built environment.
11. Evergreen plants shall be used where a sight-obscuring landscape screen is required.
12. Deciduous trees should be used where summer shade and winter sunlight is desirable.

13. Landscape plans should provide focal points within a development, for example, by preserving large or unique trees or groves or by using flowering plants or trees with fall color.
14. Landscape plans should use a combination of plants for seasonal variation in color and yearlong interest.
15. Where plants are used to screen outdoor storage or mechanical equipment, the selected plants shall have growth characteristics that are compatible with such features.
16. Landscape plans shall provide for both temporary and permanent erosion control measures, which shall include plantings where cuts or fills, including berms, swales, stormwater detention facilities, and similar grading, is proposed.
17. When new vegetation is planted, soils shall be amended and irrigation provided, as necessary, until the plants are naturalized and able to grow on their own.

**Findings:** The proposed landscaping is limited to the area of disturbance. The applicant proposed to remove one tree where the proposed storage building will be located. The tree will not be replanted as there is not enough room in the area of disturbance. The shrubs and groundcover were selected by Mackenzie landscape architects to meet the site's local climate, exposure, water availability, and utility and drainage conditions, and include Sundance Mexican Mock Orange, Compact Oregon Grape, Wild Mock Orange, Red Flowering Currant, and Salal. The plants selected are drought tolerant; however, irrigation will be provided. Plants are spaced so that they will provide the intended impact within two (2) years of planting.

As shown on submitted plans, non-plant ground cover (gravel) is proposed immediately surrounding the new building. The gravel area extends 2' from the building. This small area is well below the 35% standard. This criterion is met.

- D. Central Commercial C-1 District Streetscape Standard. Developers of projects within the Central Commercial C-1 zoning district [...]

**Findings:** The project is located in the PSP zone. These standards do not apply.

- E. Parking Lot Landscaping. All of the following standards shall be met for parking lots. If a development contains multiple parking lots, then the standards shall be evaluated separately for each parking lot.

**Findings:** As shown on the submitted site plans, no new parking areas are proposed. This section does not apply.

- F. Screening Requirements. Screening is required for outdoor storage areas, unenclosed uses, and parking lots, and may be required in other situations as determined by the

Planning Official. Landscaping shall be provided pursuant to the standards of subsections F.1 through 3. (See also Figure 17-3.4-4.). [...]

**Findings:** No outdoor storage, unenclosed uses, or new parking areas are proposed. This standard does not apply.

- G. Maintenance. All landscaping shall be maintained in good condition, or otherwise replaced by the property owner.

**Findings:** As an ongoing Condition of Approval, all landscaping shall be maintained in good condition, or otherwise replaced by the property owner.

#### 17-3.4.040 Fences and Walls

- A. Purpose. This section provides general development standards for fences, and walls that are not part of a building, such as screening walls and retaining walls.
- B. Applicability. Section 17-3.4.040 applies to all fences, and to walls that are not part of a building, including modifications to existing fences and walls.

**Findings:** As shown on the submitted site plans, a 6' slatted chain link fence is proposed. These standards are applicable.

- C. Height.
  - a. Residential Zones. [...]
  - b. Non-Residential Zones. Fences and freestanding walls (i.e., exclusive of building walls) for non-residential uses shall not exceed the following height above grade, where grade is measured from the base of the subject fence or wall.
    - i. Within Front or Street-Facing Side Yard Setback. Four feet, except the following additional height is allowed for properties located within an industrial, public, or institutional zone:
      - 1. Where approved by the City Planning Official, a fence constructed of open chain link or other "see-through" composition that allows 90 percent light transmission may reach a height of up to eight feet.
    - ii. Within an Interior Side or Rear Yard Setback. Eight feet; except the fence or wall height, as applicable, shall not exceed the distance from the fence or wall line to the nearest primary structure on an adjacent property.

**Findings:** As shown on the submitted site plans, no fences or freestanding walls are proposed within the front or street-facing setback, or the interior side or rear yard setback. These standards do not apply.

- 3. All Zones. Fences and walls shall comply with the vision clearance standards of Section 17-3.3.030.G. Other provisions of this Code, or the requirements of the roadway

authority, may limit allowable height of a fence or wall below the height limits of this section.

**Findings:** As shown on the submitted site plans, no fences or walls are proposed within the vision clearance areas. This criterion is met.

- D. Materials. Prohibited fence and wall materials include straw bales, tarps, barbed or razor wire (except in the M-2 Heavy Industrial zone); scrap lumber, untreated wood (except cedar or redwood), corrugated metal, sheet metal, scrap materials; dead, diseased, or dying plants; and materials similar to those listed herein.

**Findings:** As shown on the submitted site plans, the proposed fencing will be chain link with slats, which are not prohibited materials. This criterion is met.

- E. Permitting. A Type I approval is required to install a fence of six feet or less in height, or a wall that is four feet or less in height. All other walls and fences require review and approval by the Planning Official through a Type II procedure. The Planning Official may require installation of walls or fences as a condition of approval for development, as provided by other Code sections. A building permit may be required for some fences and walls, pursuant to applicable building codes. Walls greater than four feet in height shall be designed by a Professional Engineer licensed in the State of Oregon.

**Findings:** The applicant is requesting approval for the proposed fence as part of this application, which satisfies the Type I approval requirement. This criterion is met.

- F. Maintenance. Fences and walls shall be maintained in good condition, or otherwise replaced by the property owner.

**Findings:** As an ongoing Condition of Approval, fences and walls shall be maintained in good condition, or otherwise replaced by the property owner.

#### 17-3.4.050 Outdoor Lighting

- A. Purpose. This section contains regulations requiring adequate levels of outdoor lighting while minimizing negative impacts of light pollution.

**Findings:** No new outdoor lighting is proposed. This section does not apply.

### 17-3.5 Parking and Loading

#### 17-3.5.030 Automobile Parking

- A. Minimum Number of Off-Street Automobile Parking Spaces. Except as provided by this subsection A, or as required for Americans with Disabilities Act compliance under

subsection G, off-street parking shall be provided pursuant to one of the following three standards:

**Finding:** The proposed development does not include or require any new vehicle parking or loading, and no changes to the existing vehicle parking are proposed. As the building will be used to store existing applicant-owned vehicles, it will not result in additional employee or visitor usage, and no parking is proposed as part of this application. This criterion is met.

#### 17-3.5.040 Bicycle Parking

- A. Standards. Bicycle parking spaces shall be provided with new development and, where a change of use occurs, at a minimum, shall follow the standards in Table 17-3.5.040.A. Where an application is subject to Conditional Use Permit approval or the applicant has requested a reduction to an automobile-parking standard, pursuant to Section 17-3.5.030.C, the Planning Official may require bicycle parking spaces in addition to those in Table 17-3.5.040.A.

Table 17-3.5.040.A Minimum Required Bicycle Parking Spaces	
Use	Minimum Number of Spaces
Community Service	2 bike spaces

**Findings:** The proposed fire station accessory building is a community service use and thus the fire station as a whole requires two (2) bike spaces. As shown on the submitted plans, two bike parking spaces are proposed to be located near the southeast corner of the existing fire station building. This standard is met.

- B. Design. Bicycle parking shall consist of staple-design steel racks or other City-approved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle, consistent with the Public Works Design Standards.

**Findings:** As shown on the submitted plans, one staple-design steel bike rack is proposed consistent with the Public Works Design Standards. This criterion is met.

- C. Exemptions. This section does not apply to single-family and duplex housing, home occupations, and agricultural uses.
- D. Hazards. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall be located so as to not conflict with the vision clearance standards of Section 17-3.3.030.G.

**Findings:** This application does not meet exemption status.

## 17-3.6 Public Facilities

### 17-3.6.010 Purpose and Applicability

- A. Purpose. The standards of Chapter 17-3.6 implement the public facility policies of the City of Molalla Comprehensive Plan and adopted City plans.
- B. Applicability. Chapter 17-3.6 applies to all new development, including projects subject to Land Division (Subdivision or Partition) approval and developments subject to Site Design Review where public facility improvements are required. All public facility improvements within the city shall occur in accordance with the standards and procedures of this chapter. When a question arises as to the intent or application of any standard, the City Engineer shall interpret the Code pursuant to Chapter 17-1.5.
- C. Public Works Design Standards. All public facility improvements, including, but not limited to, sanitary sewer, water, transportation, surface water and storm drainage and parks projects, whether required as a condition of development or provided voluntarily, shall conform to the City of Molalla Public Works Design Standards. Where a conflict occurs between this Code and the Public Works Design Standards, the provisions of the Public Works Design Standards shall govern.
- D. Public Improvement Requirement. No building permit may be issued until all required public facility improvements are in place and approved by the City Engineer, or otherwise bonded, in conformance with the provisions of this Code and the Public Works Design Standards. Improvements required as a condition of development approval, when not voluntarily provided by the applicant, shall be roughly proportional to the impact of the development on public facilities. Findings in the development approval shall indicate how the required improvements directly relate to and are roughly proportional to the impact of development.

**Findings:** Applicant's proposal of a new storage building constitutes new development and public improvement requirements that are proportional to the proposed development, designed and constructed in accordance with Public Works Standards, apply.

### 17-3.6.020 Transportation Standards

1. Except as provided by subsection A.5, existing substandard streets and planned streets within or abutting a proposed development shall be improved in accordance with the standards of Chapter 17-3.6 as a condition of development approval.
2. All street improvements, including the extension or widening of existing streets and public access ways, shall conform to Section 17-3.6.020, and shall be constructed consistent with the City of Molalla Public Works Design Standards.
3. All new streets shall be contained within a public right-of-way. Public access ways (e.g., pedestrian ways) may be contained within a right-of-way or a public access easement, subject to review and approval of the City Engineer.

**Findings:** N Molalla Ave is classified as an arterial street under the jurisdiction of the City of Molalla. The current right-of-way (ROW) width is 60 feet, and the paved width is approximately 38 feet. Arterial street with parking (W/PK) typically requires a right-of-way of 60 feet and a pavement width of 40 feet. Frontage improvements were completed in 2017 as part of Public Improvement project 16-01. The existing access to tax lot 8600 will need to be reassessed for access and circulation, along with spacing standards in accordance with the Transportation Master Plan and Public Works Standards.

Kennel Avenue is classified as a neighborhood Street under the jurisdiction of the City of Molalla. The current right-of-way (ROW) is 50 feet, with an approximate pavement width of 38 feet. No additional street dedication is required, as the existing ROW complies with the City's Transportation System Plan (TSP).

4. The purpose of this subsection is to coordinate the review of land use applications with roadway authorities and to implement Section 660-012-0045(2)(e) of the State Transportation Planning Rule, which requires the City to adopt a process to apply conditions to development proposals in order to minimize impacts and protect transportation facilities. The following provisions also establish when a proposal must be reviewed for potential traffic impacts; when a Transit Analysis Letter (TAL) or Traffic Impact Analysis (TIA) must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect transportation facilities; the required contents of a TAL/TIA; and who is qualified to prepare the analysis.

**Findings:** DKS Associates prepared a Traffic Analysis Letter (TAL) included with the application which has been reviewed and accepted by the City. The TAL assesses the impacts of the proposed development and confirms compliance with city standards. The new storage building will enhance the fire station's storage and maintenance capabilities without increasing the number of employees or adding emergency vehicle trips to the site.

5. The City Engineer may waive or allow deferral of standard street improvements, including side-walk, roadway, bicycle lane, undergrounding of utilities, and landscaping, as applicable, where one or more of the following conditions in subdivisions (a) through (d) is met. Where the City Engineer agrees to defer a street improvement, it shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future. [...]
6. D. Transportation Connectivity and Future Street Plans. The following standards apply to the creation of new streets:
  - E. Engineering Design Standards. Street design shall conform to the standards of the applicable roadway authority; for City streets that is the current version of the Public Works Design Standards and Transportation System Plan. Where a conflict occurs between this Code and the Public Works Design Standards, the provisions of the Design Standards shall govern.

**Findings:** This application does not include the creation of new streets. These standards are not applicable.

- B. Fire Code Standards. Where Fire Code standards conflict with City standards, the City shall consult with the Fire Marshal in determining appropriate requirements. The City shall have the final determination regarding applicable standards.

**Findings:** To avoid a conflict of interest, the Fire Marshal declined comment on this application. Fire Staff have been heavily involved in the preparation of the application to the City and did not identify conflicting areas with City Code.

- C. Substandard Existing Right-of-Way. Where an existing right-of-way adjacent to a proposed development is less than the standard width, the City Engineer may require the dedication of additional rights-of-way at the time of Subdivision, Partition, or Site Plan Review, pursuant to the standards in the Public Works Design Standards and Transportation System Plan.

**Findings:** The site is not adjacent to an existing right-of-way with a substandard width. The requirement is not applicable.

- D. Traffic Calming. The City may require the installation of traffic calming features such as traffic circles, curb extensions, reduced street width (parking on one side), medians with pedestrian crossing refuges, speed tables, speed humps, or special paving to slow traffic in neighborhoods or commercial areas with high pedestrian traffic.

**Findings:** The proposed vehicular and pedestrian circulation system has been designed to accommodate safe pedestrian access to and within the site. The City does not require additional traffic calming measures. This criterion is met.

- E. Sidewalks, Planter Strips, and Bicycle Lanes. Except where the City Engineer grants a deferral of public improvements, pursuant to Chapter 17-4.2 or Chapter 17-4.3, sidewalks, planter strips, and bicycle lanes shall be installed concurrent with development or widening of new streets, pursuant to the requirements of this chapter. Maintenance of sidewalks and planter strips in the right-of-way is the continuing obligation of the adjacent property owner.

**Findings:** The submitted application does not include the creation of new streets. This standard does not apply.

- F. Streets Adjacent to Railroad Right-of-Way. When a transportation improvement is proposed within 300 feet of a railroad crossing, or a modification is proposed to an existing railroad crossing, the Oregon Department of Transportation and the rail service provider shall be notified and given an opportunity to comment, in conformance with the

provisions of Division IV. Private crossing improvements are subject to review and licensing by the rail service provider.

**Findings:** This application does not include streets within 300 feet of a railroad right-of-way. This standard is not applicable.

- G. Street Names. No new street name shall be used which will duplicate or be confused with the names of existing streets in the City of Molalla or vicinity. Street names shall be submitted to the City for review and approval in consultation with Clackamas County and emergency services.

**Findings:** This application does not include the creation of new streets. These standards are not applicable.

- H. Survey Monuments. Upon completion of a street improvement and prior to acceptance by the City, it shall be the responsibility of the developer's registered professional land surveyor to provide certification to the City that all boundary and interior monuments have been reestablished and protected.

**Findings:** No survey monumentation is required as no new lots or streets are proposed. This standard does not apply.

- I. Street Signs. The city, county, or state with jurisdiction shall install all signs for traffic control and street names. The cost of signs required for new development shall be the responsibility of the developer. Street name signs shall be installed at all street intersections. Stop signs and other signs may be required

**Findings:** No new street signs are proposed. This standard does not apply.

- J. Streetlight Standards. Streetlights shall be relocated or new lights installed, as applicable, with street improvement projects. Streetlightmultits shall conform to City standards, be directed downward, and full cutoff and full shielding to preserve views of the night sky and to minimize excessive light spillover onto adjacent properties.

**Findings:** No street improvement projects are proposed. This standard does not apply.

- K. Mail Boxes. Mailboxes shall conform to the requirements of the United States Postal Service and the State of Oregon Structural Specialty Code.

**Findings:** No new mailboxes or changes to the existing mailbox are proposed. This standard does not apply.

- L. Street Cross-Sections. The final lift of pavement shall be placed on all new constructed public roadways prior to final City acceptance of the roadway.

**Findings:** No new public roadways are proposed, and patching will occur in accordance with City standards. This criterion is met.

#### **17-3.6.040 Sanitary Sewer and Water Service Improvements**

A. Sewers and Water Mains Required. All new development is required to connect to City water and sanitary sewer systems. Sanitary sewer and water system improvements shall be installed to serve each new development and to connect developments to existing mains in accordance with the adopted facility master plans and applicable Public Works Design Standards. Where streets are required to be stubbed to the edge of the subdivision, sewer and water system improvements and other utilities shall also be stubbed with the streets, except as may be waived by the City Engineer where alternate alignment(s) are provided.

**Findings:** The properties are currently served by two existing 4-inch sewer service lateral connections from the City sanitary main line located on Kennel and N Molalla Ave. The applicant is proposing a new sewer lateral that will connect the new storage building to the City sewer main on Molalla Ave. Utility connections cannot be made until required permitting has been issued and all SDC & Connection Fees have been paid. The Applicant shall also submit necessary information to calculate System Development Charges at the time of building permit submittal.

Sewer & Water SDC's – In accordance with MMC 13.14 this design review does increase the impact to the public improvement facility and is therefore not exempt from Sewer & Water SDC charges. SDC's shall be calculated in accordance with the SDC methodology. System Development Charges (SDC) credit will be granted for the existing services; however, an SDC will apply if the overall improvements lead to increased usage or additional impact on the City sewer system.

B. Sewer and Water Plan Approval. Development permits for sewer and water improvements shall not be issued until the City Engineer has approved all sanitary sewer and water plans in conformance with City standards.

**Findings:** Applicant shall be required to submit a Public Works Permit and assurances in accordance with Section 1 of the Molalla Standard Specifications for Public Works Construction prior to any public construction beginning.

A. Over-Sizing. The City may require as a condition of development approval that sewer and water lines serving new development be sized to accommodate future development within the area as projected by the applicable facility master plans, and the City may authorize other cost-recovery or cost-sharing methods as provided under state law.

**Findings:** City has not requested oversized water or sewer. This criterion is not applicable.

- B. Inadequate Facilities. Development permits may be restricted or rationed by the Planning Commission where a deficiency exists in the existing water or sewer system that cannot be rectified by the development and which, if not rectified, will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of domestic water and sewerage treatment systems. The City Engineer may require water booster pumps, sanitary sewer lift stations, and other critical facilities be installed with backup power.

**Findings:** There are no identified existing deficiency within Cities Master Plan that indicates Inadequate Facilities within the limits of the proposed development for sewer and water.

#### 17-3.6.050 Storm Drainage and Surface Water Management Facilities

- A. General Provisions. The City shall issue a development permit only where adequate provisions for stormwater runoff have been made in conformance with the requirements of the current version of the Public Works Design Standards and Stormwater Master Plan.

**Findings:** Applicant will be required to submit design and construction requirements for stormwater and surface water management at the time of Public Works Permit application. Design shall be in accordance with Section 3 of the Molalla Standard Specifications for Public Works Construction and Stormwater Master Plan.

Applicant proposes to collect and detain all stormwater onsite and discharge to the storm conveyance system constructed within Kennel Ave. On-site private storm system shall comply with plumbing code requirements. The detention and flow control facilities shall be reviewed, permitted, and inspected by Public Works. The on-site storm conveyance system shall be reviewed and inspected by Clackamas County Building under a plumbing permit in accordance with MMC 13.13 Surface Water Management. The Applicant is also required to record a stormwater crossover easement between adjacent lots, which should be recorded with the County prior to final occupancy.

Stormwater SDC's - In accordance with MMC 13.14 this design review does increase the impacts to the public improvement facility and is therefore not exempt from stormwater SDC charges. SDC's shall be calculated in accordance with the SDC methodology.

- B. Accommodation of Upstream Drainage. Culverts and other drainage facilities shall be large enough to accommodate existing and potential future runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the City Engineer.

**Findings:** Not applicable, as there is no upstream drainage in relation to this development.

- C. Effect on Downstream Drainage. Where it is anticipated by the City Engineer that the additional runoff resulting from the development will overload an existing drainage facility, the City shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with City standards.

**Findings:** The development increases the impervious surface area. As shown on the submitted site plans, applicant proposes stormwater runoff to be collected in a newly developed storm water detention pond on lot 9300. Overflow from the detention pond to be routed to a new flow control manhole before being released into an existing stormwater line located in Kennel Avenue. Applicant will be required to submit design and construction requirements for stormwater and surface water management at the time of Public Works Permit for the onsite private storm system for detention and water quality prior to discharge into a public facility. Design shall be in accordance with Section 3 of the Molalla Standard Specifications for Public Works Construction and Stormwater Master Plan. On-site private storm system shall comply with state plumbing code requirements. The detention and flow control facilities shall be reviewed, permitted, and inspected by Public Works. The onsite storm conveyance system shall be reviewed and inspected by Clackamas County Building under a plumbing permit. in Accordance with MMC 13.13 Surface Water Management.

- D. Over-Sizing. The City may require as a condition of development approval that sewer, water, or storm drainage systems serving new development be sized to accommodate future development within the area as projected by the applicable facility master plan, provided that the City may grant the developer credit toward any required system development charge for the same pursuant to the System Development Charge.

**Findings:** City has not requested oversized water or sewer. This criterion is not applicable.

- E. Existing Watercourse. Where a proposed development is traversed by a watercourse, drainage way, channel, or stream, the City may require a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance to protect the public health and safety.

**Finding:** The site does not have an existing watercourse, drainage way, channel, or stream. This criterion is not applicable.

### 17-3.6.060 Utilities

The following standards apply to new development where extension of electric power, gas, or communication lines is required:

D. General Provision. The developer of a property is responsible for coordinating the development plan with the applicable utility providers and paying for the extension and installation of utilities not otherwise available to the subject property.

E. Underground Utilities.

1. General Requirement. The requirements of the utility service provider shall be met. All utility lines in new subdivisions, including, but not limited to, those required for electric, communication, and lighting, and related facilities, shall be placed underground, except where the City Engineer determines that placing utilities underground would adversely impact adjacent land uses. The Planning Official may require screening and buffering of above ground facilities to protect the public health, safety, or welfare.

**Finding:** All utilities for the project shall be served by underground services. No overhead crossings of public right-of-way shall be approved by the City.

#### **17-3.6.070 Easements**

A. Provision. The developer shall make arrangements with the City and applicable utility providers for each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development.

**Findings:** A 10-foot-wide public utility easement fronting the public right-of-way shall be dedicated to the City. The applicant shall record electrical and storm easements for crossover use. These easements shall be recorded prior to any occupancy.

E. Recordation. All easements for sewers, storm drainage and water quality facilities, water mains, electric lines, or other utilities shall be recorded and referenced on a survey or final plat, as applicable. See Chapter 17-4.2 Site Design Review, and Chapter 17-4.3 Land Divisions and Property Line Adjustments.

**Findings:** Public sanitary, storm sewer, and water lines on private property shall be centered in a permanent easement granted to the City. The minimum width of a public pipeline easement shall be 15 feet, and no permanent structures shall be allowed within an easement area.

#### **17-3.6.080 Construction Plan Approval**

No development, including sanitary sewers, water, streets, parking areas, buildings, or other development, shall commence without plans having been approved by the City of Molalla Public Works Department and permits issued. Permit fees are required to defray the cost and expenses

incurred by the City for construction and other services in connection with the improvement. Permit fees are as set by City Council resolution.

**Findings:** Applicant shall apply for a Public Works Permit in accordance with Section 1.15 Developments Process Requirements of Molalla Standards. No work will be performed, nor materials stored, nor encroachment made on or within right-of-way, Public easement, or Public utility easement until all requirements have been met and permit has been issued.

#### **17-3.6.100 Performance Guarantee and Warranty**

- A. Performance Guarantee Required. The City at its discretion, may approve a final plat or building permit when it determines that all of the public improvements required for the site development or land division, or phase thereof, are complete and the applicant has an acceptable assurance for the balance of said improvements. The applicant shall provide a performance and payment bond in accordance with the current version of the Public Works Design Standards.

**Findings:** A Performance Bond shall be in place prior to issuance of permit and before any public construction begins.

- B. Determination of Sum. The assurance of performance shall be for a sum determined by the City Engineer as required to cover the cost of the improvements and repairs, including related engineering and incidental expenses, plus reasonable inflationary costs. The assurance shall not be less than 150 percent of the estimated improvement costs.

**Findings:** The sum of the Performance Bond will be based on Engineering Cost Estimates provided at the time of application submittal.

- C. Itemized Improvement Estimate. The applicant shall furnish to the City an itemized improvement estimate, certified by a registered civil engineer, to assist the City in calculating the amount of the performance assurance.

**Findings:** See findings under 17-3.6.100 "A" & "B."

- D. Termination of Performance Guarantee. The applicant shall not cause termination, nor allow expiration, of the guarantee without first securing written authorization from the City.

**Findings:** At completion of the project and acceptance of Warranty Bond, the City will release the Performance Bond. If the applicant allows the financial assurance to expire or terminate without written authorization from the City, a stop-work order will be placed on the project, and no occupancy will be granted. Additionally, the city will seek all available remedies under the law.

- E. Warranty Bond. A warranty bond good for two years is required on all public improvements and landscaping when installed in the public right-of-way. The warranty bond shall equal 120 percent of the total cost of improvements and begin upon acceptance of said improvements by the City.

**Findings:** Warranty Bond shall be in place prior to final completion and acceptance of the project, meeting the requirements in subsection 1.15.9 of the Molalla Standards, and subject to all easements and legal documents having been recorded with the County.

- C. The proposal includes required upgrades, if any, to existing development that does not comply with the applicable zoning district standards, pursuant to Chapter 17-1.4 Nonconforming Situations;

**Findings:** Existing development is pre-existing nonconforming. The addition of the proposed storage building will not increase the nonconformity of the development. Thus, pursuant to MMC 17-1.4.030.A, the development or portion thereof may be enlarged or altered in a way that satisfies the current requirements of this Code or moves in the direction of conformity. This criterion is met.

#### 5. Chapter 17-3.7 Signs;

**Findings:** The applicant is not applying for signs. This standard does not apply.

- E. For non-residential uses, all adverse impacts to adjacent properties, such as light, glare, noise, odor, vibration, smoke, dust, or visual impact, are avoided; or where impacts cannot be avoided, they are minimized; and

**Findings:** This application replaces an existing storage building. The proposed building is designed to mimic the residential character of the properties to the north via a sloped roof and mezzanine level windows. The application also includes sight obscuring fencing. No adverse impacts are anticipated by the creation of the storage building. This criterion is met.

- F. The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.

**Findings:** This site meets all existing Conditions of Approval required by previous land use decisions. This criterion is met.

## 17-4.7 Adjustments and Variances

### 17-4-7.040 Variances

- A. Applicability. A Variance is similar to an Adjustment, but does not otherwise meet the criteria under Section 17-4.7.030.

**Findings:** The applicant is seeking approval of a Variance to Section 17-3.2.040.E.1 – Articulation to allow alternate articulation methods, namely utilizing changes in materials, colors, and textures, with additional trim around faux windows, rather than a building offset. The applicant’s proposal exceeds a 20% deviation from a standard in Division III of the Development Code so Variance is the appropriate mechanism for seeking this flexibility.

- B. Approval Criteria. The Planning Commission through a Type III procedure may approve a Variance upon finding that it meets all of the following criteria:

1. The Variance is necessary because the subject Code provision does not account for special or unique physical circumstances of the subject site, existing development patterns, or adjacent land uses. A legal lot determination may be sufficient evidence of a hardship for purposes of approving a variance;

**Findings:** As stated, the applicant is seeking approval of a Variance to Section 17-3.2.040.E.1 – Articulation, to allow alternate articulation methods, namely utilizing changes in materials, colors, and textures, with additional trim around faux windows, rather than a building offset. The Code provision for which this Variance is requested was intended to apply to traditional commercial development along pedestrian walkways. The specific code standard (Section 17-3.2.040.E.1) requires that a non-residential building façade, oriented to a street, should have breaks in the wall plane every 30' to provide building articulation of at least 24" depth offsets. Per the purpose statement (Section 17-3.2.040), this is intended to apply to shopping centers, office complexes, mixed-use developments, and institutional campuses in order to create and maintain a built commercial environment that is conducive to pedestrian accessibility and reducing short automobile trips, while providing civic space for customers, and human scale development.

This code section doesn’t account for the circumstances of the site being an essential public facility with unique building and site design requirements. The seismic requirements of an essential facility demand regularity and efficiency in the building’s gravity and lateral structural systems, which is dependent on rectilinear building forms and which hampers utilizing building offsets to provide articulation. Additionally, the proposed accessory storage building will be set back over 100' from the street lot line and will be located behind security fencing and an existing residential structure. Only a portion of the proposed building will be visible from the street and pedestrian access will not be permitted to this portion of the essential public facility for security reasons.

Additionally, the code provision, which applies to non-residential structures, doesn't account for existing development and adjacent land uses surrounding the proposed accessory storage building. This portion of the site where the accessory structure is proposed has residential properties and structures to the northeast, north, west, and southwest. Due to the proximity of the surrounding residences, the setback from the street, and existing fencing, the design of the proposed accessory building has stylistic elements in common with residential accessory structures as well.

The proposed building is needed at this location for the safe and successful operation of the fire station and supports the Fire District's ability to serve the community. The applicant is therefore seeking Variance approval in recognition of these unique circumstances.

2. The Variance is the minimum necessary to address the special or unique physical circumstances related to the subject site;

**Findings:** Deviating from the articulation standard is a relatively minor variation from the code provisions that still provides human scale and façade articulation through change of materials, textures, colors, and depths. Approving the Variance will enable efficient and secure use of the site for a permitted use at a central location that benefits the surrounding community and City as a whole. Other than this articulation standard, applicable development standards are proposed to be met with this project. This criterion is met.

3. The need for the Variance is not self-imposed by the applicant or property owner. (For example, the Variance request does not arise as a result of a property line adjustment or land division approval previously granted to the applicant);

**Findings:** The Variance is not self-imposed by prior actions of the Owner but rather arises from the unique building and site design needs of a Fire Station facility and from the requirements for an essential emergency service to remain functional following a seismic event. These conditions would be present regardless of the location of a Fire Station and are beyond the applicant's control due to the obligation to protect the public safety by providing fire protection services. This criterion is met.

4. The Variance does not conflict with other applicable City policies or other applicable regulations;

**Findings:** The Variance applies to facade articulation, which does not conflict with other applicable City policies or applicable regulations. This criterion is met.

5. The Variance will result in no foreseeable harm to adjacent property owners or the public;  
and

**Findings:** The applicant is not seeking approval for a prohibited use and is not proposing a Variance that would increase impacts on neighboring uses or the public. The proposed Variance to the required facade articulation will not create a conflict with adjacent uses as special consideration was given to ensure the purpose of the code provision is still met through change of materials, textures, colors, and depths. These design considerations minimize potential conflict with or harm to adjacent uses. This criterion is met.

7. All applicable building code requirements and engineering design standards shall be met.

**Findings:** As demonstrated throughout the consolidated application package, applicable building code and engineering design standards were incorporated into the project's design. This criterion is met.

**Exhibit B:**

*Application Package For SDR03-2026 & VAR02-2026*



# Land Use Action Application (Annexation/Zone Change/Subdivision/Partition/S ite Design Review/Variance/Conditional Use)

Submitted on 6 May 2026, 6:00pm  
 Receipt number 19  
 Related form version 0

Type of land use action requested (more than one may apply) Site Design Review  
 Variance

What's your preliminary plat approval number?

## Applicant Information

Name: Molalla Fire Distric [REDACTED]

Phone: [REDACTED]

Mailing Address: PO Box 655  
 Molalla, OR 97038

Email: [REDACTED]

Is the Owner different than the Applicant? No

## Owner Information

Name:

Phone:

Mailing Address:

Email:

## Property Information

---

Site Address: 320 N Molalla Avenue

---

Zoned: PSP and R-3

---

Overlay:

---

Taxlot Number: 52E09CB08400, 52E09CB08500, 52E09CB08600,  
52E09CB09300, 52E09CB09500, 52E09CB09600

---

Property dimensions: Varies - 300' by 400' at widest points

---

Property acreage: 2.3

## Project Information

---

Proposed Development and/or Use: New 4,800 square foot (SF) storage building and associated site improvements

---

Describe all existing buildings or structures on property: Fire station, detached residences, sheds

---

Current and Historical Uses: Fire station, residences

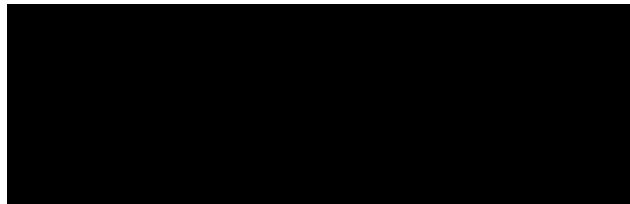
## Documents Required

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Site Plan [00RPT-City of Molalla-Molalla Fire Station 82 Site Design Review-260506.pdf](#)  
[SDR Exhibit 4. Transportation Analysis Letter.pdf](#)  
[SDR Exhibit 1. Aerial Photo.pdf](#)  
[SDR Exhibit 3. Preliminary Storm Report.pdf](#)  
[SDR Exhibit 2. Plans.pdf](#)  
[Molalla Deeds.pdf](#)

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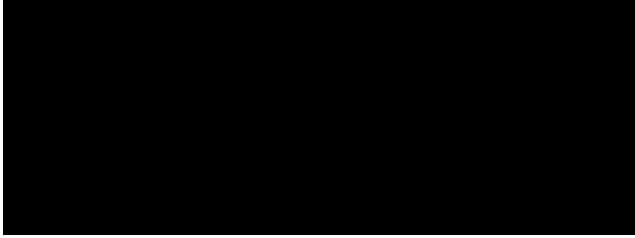
Applicant Signaire:



Date: 05/06/2026

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Property Owner Signature:



---

Date

05/06/2026



**SITE DESIGN REVIEW  
AND VARIANCE**

**To**  
City of Molalla

**For**  
Molalla Fire Station 82  
Accessory Building

**Dated**  
May 6, 2026

**Project Number**  
2250404.00

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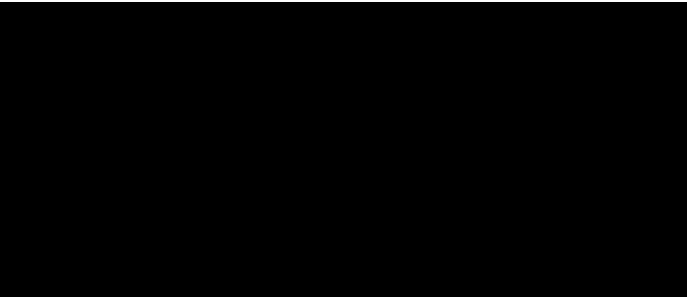
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1. Aerial Photo
2. Plans
3. Preliminary Stormwater Report
4. Transportation Analysis Letter

## I. PROJECT SUMMARY

<b>Applicant:</b>	Molalla Fire District 
<b>Owner:</b>	Molalla Fire District
<b>Site Address:</b>	320 N Molalla Avenue Molalla, OR 97038
<b>Clackamas County Tax Lots:</b>	52E09CB08400, 52E09CB08500, 52E09CB08600, 52E09CB09300, 52E09CB09500, 52E09CB09600
<b>Assessor Site Acreage:</b>	Approximately 2.3 acres
<b>Comprehensive Plan:</b>	Public and Semi-Public District (PSP) Medium-High Density Residential (R-3) – tax lot 9300
<b>Zoning:</b>	Public and Semi-Public District (PSP) (all tax lots except 9300) Medium-High Density Residential (R-3) – tax lot 9300
<b>Adjacent Zoning:</b>	R-3 to the north and west General Commercial (C-1) to the north and east PSP to the south
<b>Existing Structures:</b>	Fire station, detached residences, sheds
<b>Request:</b>	Site Design Review and Variance for new 4,800 square foot (SF) storage building
<b>Project Contact:</b>	

## II. INTRODUCTION

### Description of Request

The applicant is seeking approval for Site Design Review with one (1) Variance for a new single-story accessory storage building and associated landscaping, stormwater pond, and site improvements. The building will serve as an annex to the existing emergency response facilities at Molalla Fire Station 82.

### Existing Site and Surrounding Land Use

The approximately 2.3-acre site is located at 320 N Molalla Avenue in Molalla, Oregon. It consists of six (6) tax lots (8400, 8500, 8600, 9300, 9500, and 9600) on Clackamas County Tax Map 52E09CB, as shown in Figure 1. The site is within the City of Molalla and is primarily zoned Public/Semi-Public (PSP), with the exception of tax lot 9300, which is zoned Medium-High Density Residential (R-3). Adjacent properties are zoned General Commercial (C-1), R-3, and PSP.

The primary Fire Station building is located in the southwest corner of the site on tax lot 8400. To the west of the primary building, on tax lot 9600, is a residential structure. On tax lot 9500 is a small storage structure. On tax lot 8600 there is an existing residential structure (to remain) and residential garage structure which is proposed to be demolished. The northwestern lot (tax lot 9300) is vacant with grass cover.

Abutting uses include housing to the north and west, and Molalla Civic Center and Leonard Long Park to the south. Across N Molalla Avenue are commercial uses and to the west of Kennel Avenue are residential lots.

The site has frontage to the east along Molalla Avenue, an arterial street, which has a 60' wide right-of-way (ROW) and a 38' paved width. The site also has frontage to the west along Kennel Avenue, a neighborhood street, which has a 50' ROW width and a 38' paved width. Frontage improvements on Molalla Avenue were completed in 2017 as part of Public Improvement project 16-01. The right-of-way on Kennel Avenue complies with the City's Transportation System Plan (TSP).

As discussed in the Transportation Analysis Letter (TAL) in Exhibit 4, the site has three (3) driveways on Molalla Avenue: one (1) serving the residential structure and two (2) serving the fire station. The site also has a driveway to Kennel Avenue. See Figure 1.



Figure 1: Aerial Photo - Project Site

## Proposed Development

The Molalla Rural Fire Protection District is proposing to add a new 4,800 SF storage building to the existing Fire Station 82 site located at 320 N Molalla Avenue. The project will include the addition of a new stormwater pond, extension of existing fencing (by approximately 90'), a new bike rack near the main station building, and additional paving and landscaping. The project will require the demolition of an existing structure previously used as a residential garage. No other changes are proposed to the site.

The proposed storage building will not be accessible to the public and will be set back over 100' from both Molalla and Kennel Avenues. The building will include five (5) roll-up doors, four (4) along the southern façade facing the internal drive area and one (1) along the western façade. The building will also have two (2) personnel doors, one (1) on the east façade of the structure (facing Molalla Avenue), and one (1) on the western façade of the structure.

In addition, the project proposes the following new utility easements:

- A 10' wide Public Utility Easement (PUE) along both Molalla and Kennel Avenues.
- A 10' electrical easement running north from the existing Fire Station 82 building, connecting to lot 8600 where the new building is proposed.
- A stormwater easement running west from lot 8600 to Kennel Avenue, ranging in width from 10' to 25' as needed to cover the new stormwater infrastructure.

The water and sanitary sewer utilities from the new building will connect via proposed new laterals to Molalla Avenue mains along the northeastern lot line. The proposed stormwater infrastructure includes new storm lines, catch basins, and the stormwater pond.

No changes are proposed to the existing site access or to existing parking. Access to the new building will be via the central driveways on Molalla Avenue and one (1) driveway off Kennel Avenue.

The proposal also includes community mural insets facing N Molalla Avenue to enhance the visual appeal of the building from the street. Combined with the decorative trim, the proposed façade enhancements help create depth and articulation, while also still complementing the theme of the existing Station 82 building. New landscaping, consisting of shrubs and groundcover, are proposed between the northern façade of the new building and property line, as well as around the north half of the west and east facades, creating additional depth and visual appeal to the building and site.

The applicant is requesting a Variance due to the unique needs associated with essential public facilities, and special physical circumstances of the site, existing development patterns, and adjacent residential land uses. The applicant is requesting a Variance to Section 17-3.2.040.E.1 – Articulation. The proposed Variance is asking for approval to allow alternate articulation methods, namely utilizing changes in materials, colors, and textures, with additional trim and inset painted community murals, in place of a building offset.

Findings in support of these requests are included in the narrative below.

### **Public Facilities and Services Impact Analysis**

The following discussion is provided in accordance with the requirement for a Public Facilities and Services Impact Study specified in Section 17-4.2.040 of the Development Code.

#### ***Parks***

The proposed Fire Station is not anticipated to result in increased impacts on public parks.

#### ***Transportation Analysis***

N Molalla Avenue is classified as an arterial street. Per comments from City staff at the Pre-Application Conference, the current ROW width on Molalla Avenue is 60' and the paved width is approximately 38'. Arterial streets with parking typically require a ROW of 60' and a pavement width of 40'. However, frontage improvements along this stretch of N Molalla Ave were completed in 2017 as part of the Public Improvement project 16-01, and staff has indicated that additional frontage improvements will not be required as the current improvements were designed in accordance with the Downtown Master Plan (DMP).

Kennel Avenue is classified as a neighborhood street. Per comments from City staff at the Pre-Application Conference, the current ROW is 50' wide with an approximate pavement width of 38', and no additional street dedication is required as the existing widths comply with applicable street standards.

DKS transportation engineers have projected trip generation for the proposed development (Exhibit 4) and found that the project is not expected to generate an increase in trips since the proposed storage building will not accommodate additional employees or result in additional emergency response demand from the station. As such, the development is not expected to impact adjacent roadways or intersections, as discussed in Exhibit 4.

### ***Public Utilities***

No off-site improvements to water, sanitary sewer, or storm sewer are necessary to serve the development as prior improvements have been put in place and sized to accommodate site development. The applicant proposes connections from the site to the public lines as needed.

#### *Water System*

The subject site is located within the City of Molalla water service boundary. The proposed development will connect to a public water main in Molalla Avenue. No new public water system is required for the proposed development as the existing system is appropriately sized for the development.

#### *Sanitary Sewer System*

The subject site is located within the City of Molalla sewer service boundary. The proposed development will connect to the public sanitary sewer line in Molalla Avenue. No new public sanitary sewer improvements are required as the existing system is appropriately sized for the development.

#### *Storm Drainage System*

The new building and impervious area on the property will trigger the need to provide on-site water quality and flow control facilities in accordance with Section 3 (Stormwater Design and Construction Standards) of the City of Molalla Standard Specifications for Public Works Construction.

As discussed in the preliminary storm report (Exhibit 3) the construction will include the installation of a stormwater pond to provide water quality and detention, with discharge to an existing storm main in Kennel Avenue.

### III. NARRATIVE AND COMPLIANCE

The following discussion addresses the Molalla Municipal Code (MMC) approval criteria and development standards which apply to the proposed development. In the sections below, applicable approval standards from the MMC are shown in *italics*, while responses are shown in a standard typeface.

#### Molalla Municipal Code Title 17 – Development Code

##### Chapter 17-2.2 Zoning District Regulations

##### 17-2.2.030 Allowed Uses

- A. *Uses Allowed in Base Zones. Allowed uses include those that are permitted, those that are permitted subject to special use standards, and those that are allowed subject to approval of a conditional use permit, as identified by Table 17-2.2.030. Allowed uses fall into four general categories: Residential, Public and Institutional, Commercial, and Other. If Table 17-2.2.030 does not list a specific use, and Division V Definitions does not identify the use or include it as an example of an allowed use, the City may find that use is allowed, or is not allowed, by following the procedures of Section 17-1.5.010 Code Interpretations. Uses not listed in Table 17-2.2.030 and not found to be similar to an allowed use are prohibited.*
- B. *Permitted Uses and Uses Permitted Subject to Special Use Standards. Uses listed as "Permitted (P)" are allowed provided they conform to Section 17-2.2.040 Lot and Development Standards. Uses listed as "Permitted Subject to Special Use Standards (S)" are allowed, provided they conform to the Chapter 17-2.3 Special Use Standards and Section 17-2.2.040 Lot and Development Standards. Uses listed as "Not Allowed (N)" are prohibited. Uses not listed but similar to those allowed may be permitted pursuant to Section 17-1.5.010.*

TABLE 17-2.2.030 USES ALLOWED BY ZONING DISTRICT (EXCERPT)		
Uses	R-3	PSP
Emergency Services; includes Police, Fire, Ambulance	CU	P
Utility Structures and Facilities, City Planned Projects; i.e., utilities identified by an adopted City master plan or development review approval	P	P

**Response:** The applicant is proposing a single-story accessory building to serve as an annex to the existing fire station. The proposed use falls into the Emergency Services use category, which is permitted in the PSP zone. The applicant is also proposing a stormwater management facility on tax lot 9300, which is zoned R-3. Utility facilities approved by development review are permitted in the R-3 zone. This standard is met.

##### 17-2.2.040 Lot and Development Standards

- A. *Development Standards. Section 17-2.2.040 provides the general lot and development standards for each of the City’s base zoning districts. The standards of Section 17-2.2.040 are organized into two tables: Table 17-2.2.040.D applies to residential zones, and Table 17-2.2.040.E applies to non-residential zones.*

**Response:** The site consists of 6 lots, 5 of which are zoned PSP and one lot (TL 9300) zoned Medium-High Residential (R-3). Table 17-2.2.040.D applies to the PSP zone, a non-residential zone, and Table 17-2.2.040.E applies to the residential lot. See subsections D and E below for compliance with development standards.

B. *Design Standards. City standards for Access, Circulation, Site and Building Design, Parking, Landscaping, Fences and Screening, and Public Improvements, among others, are located in Division III. Notwithstanding the provisions of Section 17-2.2.040 and Division III, different standards may apply in specific locations, such as at street intersections, within overlay zones, adjacent to natural features, and other areas as may be regulated by this Code or subject to state or federal requirements. For requirements applicable to the City’s overlay zones, please refer to Chapter 17-2.4.*

**Response:** The proposal complies with the applicable standards, as demonstrated by the applicant’s narrative responses and application package. See each applicable section within this narrative for further, more detailed evidence of compliance. This standard is met.

D. *Lot and Development Standards for Residential Districts. The development standards in Table 17-2.2.040.D apply to all new development as of November 10, 2017 in residential zones.*

<b>TABLE 17-2.2.040.D LOT AND DEVELOPMENT STANDARDS FOR RESIDENTIAL ZONES (EXCERPT)</b>	
<b>Standard</b>	<b>R-3 Zone</b>
<i>Minimum Lot Area (square feet) Non-Residential Uses</i>	<i>2,500 sf</i>
<i>Minimum Lot Width Non-Residential Uses</i>	<i>50 ft</i>
<i>Minimum Lot Depth Single-Family, Not Attached, Duplex, and Non-Residential Uses</i>	<i>80 ft</i>
<i>Fences and Non-Building Walls Max. Height – Front Yard Max. Height – Interior Side Max. Height – Rear Yard Max. Height – Street-Side or Reverse Frontage Lot (rear) (See also Section 17-3.4.040)</i>	<i>4 ft 6 ft 6 ft 6 ft</i>
<i>Minimum Landscape Area (% of lot area). Landscape area may include plan areas and some non-plant areas as allowed under Section 17-3.4.030.</i>	<i>20%</i>

**Response:** Tax Lot 9300 is zoned R-3 and thus subject to the standards of Table 17-2.2.040.D. The existing lot area is 10,000 SF, with a width of 50 feet and depth of 200 feet. The proposed fence extension is not in the residentially zoned lot. As shown in Exhibit 2, the majority of the parcel will continue to be landscaped (the overall site is proposed to have 27.8% landscaped area), well exceeding the 20% minimum. No buildings are proposed on this parcel so building height and setback standards do not apply. This standard is met.

E. *Lot and Development Standards for Non-Residential Districts. The development standards in Table 17-2.2.040.E apply to all new development as of November 10, 2017 in the City’s non-residential zones, as follows.*

<b>TABLE 17-2.2.040.E LOT AND DEVELOPMENT STANDARDS FOR NON-RESIDENTIAL ZONES (EXCERPT)</b>	
<i>Standard</i>	<i>PSP</i>
<i>Minimum Lot Area (square feet) *Development must conform to lot width, depth, yard setback, and coverage standards</i>	None
<i>Minimum Lot Width and Depth</i>	None
<i>Building and Structure Height – standard maximum height</i>	55 ft
<i>*[Height Increase. The City may increase the standard height, above, for specific projects with approval of a Conditional Use Permit (CUP), per Chapter 17-4.4.]</i>	Yes
<i>Fences and Non-Building Walls (See also Section 17-3.4.040.)</i>	
<i>Maximum Height – Front Yard</i>	4 ft
<i>Maximum Height – Interior Side</i>	6 ft
<i>Maximum Height – Rear Yard</i>	6 ft
<i>Maximum Height – Street-Side or Reverse Frontage Lot (rear)</i>	4 ft, or 6 ft with 5 ft landscape buffer
<i>Lot Coverage – Maximum Lot Coverage (foundation plane as % of site area)</i>	NA
<i>Minimum Landscape Area (% of site area), includes required parking lot landscaping and any required screening. This standard does not apply to individual, detached single-family dwellings. Landscape area may include street trees and civic space improvements in some zones, per Sections 17-3.2.050 and 17-3.4.030.</i>	10%
<i>Minimum Setback Yards (feet): (See also Section 17-2.2.050)</i>	
<i>Front, Street-Side, Interior Side, and Rear property lines, except garage or carport, or as required by other code provisions</i>	0 ft
<i>Garage or Carport Entry, set back from street</i>	20 ft
<i>Alley</i>	20 ft
<i>Adjacent to R Districts</i>	10 ft
<i>Build-To Line (feet):</i>	
<i>New Buildings Only: At least one primary building entrance shall be built no farther from the street right-of-way than the build-to line; except where a greater setback is required for a Planned Street Improvement, then the build-to line increases proportionately. The build-to line may also be increased through Site Design Review when pedestrian amenities are provided between a primary building entrance and the street right-of-way. To avoid encroachment into the right-of-way, doorways are not required to be flush with the build-to line.</i>	
<i>Special Setback for Planned Street Improvements: Except as waived by the City Engineer, new structures or additions on lots abutting an existing public street that does not meet the right-of-way standards of Section 17-3.6.020, shall provide setbacks sufficient to allow for the future planned right-of-way, plus the minimum required yard setback.</i>	
<i>Note: Always locate utilities and utility easements before construction.</i>	

**Response:** The proposed building is on Tax Lot 8600, which is zoned PSP and thus subject to the standards of Table 17-2.2.040.E. As shown on Sheet A2.10 of Exhibit 2, the proposed building is approximately 24.5'

at its highest point, complying with the 55' maximum building height standard of Table 17-2.2.040.E. As shown on Sheet C1.10 of Exhibit 2, the proposed fence extension located east of the proposed building is 6' tall. This height meets the 6' maximum height standard for fences of Table 17-2.2.040.E. As shown on Sheet C1.10, the overall site is proposed to have 27.8% landscaped area, meeting the minimum landscape area standard of Table 17-2.2.040.E. As shown on Sheet C1.10, the proposed building is set back 10.5' from the abutting residential property lines to the north and west. There is no required setback for Front, Street-Side, Interior Side, and Rear property lines except for garage entry from the street which requires a 20' setback; the bays are well over 20' from a street lot line and do not face the street. Therefore, all the setback standards of Table 17-2.2.040.E are met. The Build-to-Line requirement does not apply in the PSP zone. The required right-of-way width standards of Section 17-3.6.020 are met for both Molalla and Kennel Avenues as discussed in this narrative's responses to that section. No additional right-of-way dedication will be required. The Special Setback requirement does not apply. No minimum or maximum standards for lot area, lot width and depth, or lot coverage are required for the PSP zone. This standard is met.

### **Chapter 17-3.2 Building Orientation and Design**

#### *17-3.2.040 Non-Residential Buildings*

- A. *Purpose and Applicability. The following requirements apply to non-residential development, including individual buildings and developments with multiple buildings such as shopping centers, office complexes, mixed-use developments, and institutional campuses. The standards are intended to create and maintain a built environment that is conducive to pedestrian accessibility, reducing dependency on the automobile for short trips, while providing civic space for employees and customers, supporting natural surveillance of public spaces, and creating human-scale design. The standards require buildings placed close to streets, with storefront windows (where applicable), with large building walls divided into smaller planes, and with architectural detailing.*

**Response:** The proposal is non-residential development. This standard is applicable.

- B. *Building Orientation. The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.*
1. *Buildings subject to this section shall conform to the applicable build-to line standard in Table 17-2.2.040.E, as generally illustrated in Figure 17-3.2-6. The standard is met when at least 50 percent of the abutting street frontage has a building placed no farther from at least one street property line than the build-to line in Table 17-2.2.040.E; except in the Central Commercial C-1 zone, at least 80 percent of the abutting street frontage shall have a building placed no farther from at least one street property line than the required build-to-line. The Planning Official, through Site Design Review, may waive the build to line standard where it finds that one or more of the conditions in subdivisions a through g occurs.*
    - a. *A proposed building is adjacent to a single-family dwelling, and an increased setback promotes compatibility with the adjacent dwelling.*
    - b. *The standards of the roadway authority preclude development at the build-to line.*
    - c. *The applicant proposes extending an adjacent sidewalk or plaza for public use, or some other pedestrian amenity is proposed to be placed between the building and public right-of-way, pursuant to Section 17-3.2.050 and subject to Site Design Review approval.*
    - d. *The build-to line may be increased to provide a private open space (e.g., landscaped forecourt), pursuant to Section 17-3.2.050, between a residential use*

- in a mixed-use development (e.g., live-work building with ground floor residence) and a front or street property line.*
- e. *A significant tree or other environmental feature precludes strict adherence to the standard and will be retained and incorporated in the design of the project.*
  - f. *A public utility easement or similar restricting legal condition that is outside the applicant's control makes conformance with the build-to line impracticable. In this case, the building shall instead be placed as close to the street as possible given the legal constraint, and pedestrian amenities (e.g., plaza, courtyard, landscaping, outdoor seating area, etc.) shall be provided within the street setback in said location pursuant to Section 17-3.2.050.*
  - g. *An existing building that was lawfully created but does not conform to the above standard is proposed to be expanded and compliance with this standard is not practicable.*

**Response:** Per Table 17-2.2.040.E, the PSP zone is not subject to the build-to line standard. This standard does not apply.

- 2. *Except as provided in subsections C.5 and 6, all buildings shall have at least one primary entrance (i.e., tenant entrance, lobby entrance, breezeway entrance, or courtyard entrance) facing an abutting street (i.e., within 45 degrees of the street property line); or if the building entrance must be turned more than 45 degrees from the street (i.e., front door is on a side or rear elevation) due to the configuration of the site or similar constraints, a pedestrian walkway must connect the primary entrance to the sidewalk in conformance with Section 17-3.3.040.*

**Response:** The proposed storage building will not be accessed by the public, so it does not have a public entrance. Nonetheless, as shown on Sheets C1.10 and A2.10 of Exhibit 2, the proposed building has an entrance facing Molalla Avenue. This standard is met.

- 3. *Off-street parking, trash storage facilities, and ground-level utilities (e.g., utility vaults), and similar obstructions shall not be placed between building entrances and the street(s) to which they are oriented. To the extent practicable, such facilities shall be oriented internally to the block and accessed by alleys or driveways.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, the site has existing off-street parking to serve the public, visitors, and staff. Some of the parking spaces are located between the main fire station and Molalla Avenue, but this is a pre-existing condition. No new parking is proposed as part of the current project. Similarly, the project does not propose new trash storage facilities or ground-level utilities. As a result, this standard does not apply to the application.

- 4. *Off-street parking shall be oriented internally to the site to the extent practicable, and shall meet the Access and Circulation requirements of Chapter 17-3.3, the Landscape and Screening requirements of Chapter 17-3.4, and the Parking and Loading requirements of Chapter 17-3.5.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, the site has existing off-street parking to serve the public, visitors, and staff. Some of the parking spaces are located between the main fire station and Molalla Avenue, but this is a pre-existing condition. No new parking is proposed as part of the current project. This standard does not apply.

- 5. *Where a development contains multiple buildings and there is insufficient street frontage to meet the above building orientation standards for all buildings on the subject site, a building's primary entrance may orient to plaza, courtyard, or similar pedestrian space containing pedestrian amenities and meeting the requirements under Section 17-3.2.050,*

*subject to Site Design Review approval. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a pedestrian walkway conforming to Section 17-3.3.040.*

**Response:** The fire station property consists of multiple buildings, and the applicant is proposing a new accessory storage building as part of this application. Per Table 17-2.2.040.E, the building orientation standards of paragraph 1 do not apply in the PSP zone, and the proposed building will have an entrance facing Molalla Avenue. This standard does not apply.

- C. *Large-Format Developments. Plans for new developments, or any phase thereof, with a total floor plate area (ground floor area of all buildings) greater than 35,000 square feet, shall meet all of the following standards in subsections C.1 through 9, as generally illustrated in Figure 17-3.2-7. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.*

[detailed provisions omitted for brevity]

**Response:** The proposed building will contain approximately 4,800 SF, below the 35,000 SF trigger for large-format developments (see Exhibit 2, Sheet C1.10). This standard does not apply.

- D. *Primary Entrances and Windows. The following standards, as generally illustrated in Figures 17-3.2-8 and 17.3.2-9, apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.*

1. *All Elevations of Building. Architectural designs shall address all elevations of a building. Building forms, detailing, materials, textures, and color shall [to] contribute to a unified design with architectural integrity. Materials used on the front façade must turn the building corners and include at least a portion of the side elevations, consistent with the overall composition and design integrity of the building.*

**Response:** As depicted on Sheet A2.10 of Exhibit 2, the proposed building uses common form, detailing, materials, textures, and color on all four (4) elevations. The same materials and color themes are used on all facades, maintaining consistent composition and design. This standard is met.

2. *Pedestrian Entrances. Ground level entrances oriented to a street shall be at least partly transparent for natural surveillance and to encourage an inviting and successful business environment. This standard may be met by providing a door with a window or windows, a transom window above the door, or sidelights beside the door. Where ATMs or other kiosks are proposed on any street-facing elevation, they shall be visible from the street for security and have a canopy, awning, or other weather protection shelter.*

**Response:** As shown on Sheets C1.10 and A2.10 of Exhibit 2, a pedestrian entrance is proposed on the east elevation facing Molalla Avenue. Sheet A2.10 demonstrates that the proposed entryway includes a transom window above the door. No ATMs or kiosks are proposed. This standard is met.

3. *Corner Entrances. Buildings on corner lots are encouraged to have corner entrances. Where a corner entrance is not provided, the building plan shall provide an architectural element or detailing (e.g., tower, beveled corner, art, special trim, etc.) that accentuates the corner location.*

**Response:** The site is not a corner lot. This standard does not apply.

4. *Street Level Entrances. All primary building entrances shall open to the sidewalk and shall conform to Americans with Disabilities Act (ADA) requirements, as applicable. Primary entrances above or below grade may be allowed where ADA accessibility is provided.*

**Response:** The proposed storage building is an accessory structure and will not be accessed by the public. As the primary use of the site is not carried on in this accessory structure (the new structure is an annex to the existing fire station), no primary entrance is required. However, the proposed building entrances conform to ADA requirements as shown on Sheets C1.10 and A2.10 of Exhibit 2. This standard is met.

5. *Windows—General. Except as approved for parking structures or accessory structures, the front/street-facing elevations of buildings shall provide display windows, windowed doors, and where applicable, transom windows to express a storefront character.*

**Response:** The proposed storage building is an accessory structure, however, as shown on Sheet A2.10 of Exhibit 2, the street-facing front (east) façade expresses a storefront character by providing a door with a transom window and two (2) windows at the mezzanine level. The design also includes smooth fiber cement insets surrounded by painted trim to resemble windows; these are proposed to be painted with community murals along the ground level. The mural insets in painted trim are proposed to meet the window exception of Section 17-3.2.040.D.12 below. This standard is met.

6. *Storefront Windows. Storefront windows shall consist of framed picture or bay windows, which may be recessed. Framing shall consist of trim detailing such as piers or pilasters (sides), lintels or hoods (tops), and kick plates or bulkheads (base)—or similar detailing—consistent with a storefront character. The ground floor, street-facing elevation(s) of all buildings shall comprise at least 60 percent transparent windows, measured as a section extending the width of the street-facing elevation between the building base (or 30 inches above the sidewalk grade, whichever is less) and a plane 72 inches above the sidewalk grade.*

**Response:** As shown on Sheet A2.10 of Exhibit 2, the east (front/street-facing) elevation is proposed to have less than 1% actual glazing (the transom window) within the specified heights but is proposed to have 60% of the ground floor façade occupied by the window-like framed mural insets (requested as a window exception per 17-3.2.040.D.12 below) and two (2) additional framed windows at the mezzanine level above the framed mural insets, adding an additional 34 SF of glazing to the façade (though not within the designated ground-level area).

Due to security concerns, it is not practical to use glass windows in the lower portion of the building. Additionally, UV light is detrimental to the fire department turnout equipment that will be stored on the ground level of the proposed building. Upper glass windows have been provided to satisfy daylighting requirements of the energy code.

This standard will be met with approval of the window exception requested under item 12 below.

7. *Defined Upper Story(ies). Building elevations shall contain detailing that visually defines street level building spaces (storefronts) from upper stories. The distinction between street level and upper floors shall be established, for example, through the use of awnings, canopies, belt course, or similar detailing, materials, or fenestration. Upper floors may have less window area than ground floors, but shall follow the vertical lines of the lower level piers and the horizontal definition of spandrels and any cornices. Upper floor window orientation shall primarily be vertical, or have a width that is no greater than height.*

*Paired or grouped windows that, together, are wider than they are tall, shall be visually divided to express the vertical orientation of individual windows.*

**Response:** While a mezzanine is proposed for storage, the proposal consists of a single-story building as shown on Sheet A3.10 in Exhibit 2. Upper stories are not proposed. This standard does not apply.

8. *Buildings Not Adjacent to a Street. Buildings that are not adjacent to a street or a shopping street, such as those that are setback behind another building and those that are oriented to a civic space (e.g., internal plaza or court), shall meet the 60 percent transparency standard on all elevations abutting civic space(s) and on elevations containing a primary entrance.*

**Response:** The proposed storage building is an accessory structure and will not be accessed by the public. As the primary use of the site is not carried on in this accessory structure (the new structure is an annex to the existing fire station), no primary entrance is required. Additionally, the proposed building is set back behind (west of) an existing residential structure and is not oriented to a civic space. This standard does not apply; however, the window-like framed insets satisfy the intent of this standard and the applicant has requested a window exception requested under item 12 below.

9. *Side and Rear Elevation Windows. All side and rear elevations, except for zero lot line or common wall elevations, where windows are not required, shall provide not less than 30 percent transparency.*

**Response:** Proposed side and rear elevations do not meet the 30% transparency standard due to security and UV damage concerns of the proposed storage use. As discussed above and in paragraph 12 below, a window exception is requested. The north and west elevations are proposed to have framed panels similar to the east (street facing) façade. The proposed framed inset panels are provided at 30% of the north and west elevations as shown on Sheet A2.10 of Exhibit 2. The south side elevation contains the roll-up doors for the building; these doors do not facilitate strict compliance with this standard but will each contain a row of glazing as shown on Sheet A2.10. This standard will be met with approval of the window exception requested under subsection 12 below.

10. *Window Trim. At a minimum, windows shall contain trim, reveals, recesses, or similar detailing of not less than four inches in width or depth as applicable. The use of decorative detailing and ornamentation around windows (e.g., corbels, medallions, pediments, or similar features) is encouraged.*

**Response:** As discussed above and in paragraph 12 below, a window exception is requested to use inset panels to meet the window requirements. The proposed inset panels contain 4" wide trim. This standard is met.

11. *Projecting Windows, Display Cases. Windows and display cases shall not break the front plane of the building (e.g., projecting display boxes are discouraged). For durability and aesthetic reasons, display cases, when provided, shall be flush with the building façade (not affixed to the exterior) and integrated into the building design with trim or other detailing. Window flower boxes are allowed, provided they do not encroach into the pedestrian through-zone.*

**Response:** Projecting windows are not proposed. This standard does not apply.

12. *Window Exceptions. The Planning Official may approve an exception to the above standards where existing topography makes compliance impractical. Where it is not*

*practicable to use glass, windows for parking garages or similar structures, the building design must incorporate openings or other detailing that resembles window patterns (rhythm and scale).*

**Response:** A window exception is requested for the proposed accessory storage building. Due to security concerns, it is not practical to use glass windows in the lower portion of the building. Additionally, UV light is detrimental to the fire department turnout equipment that will be stored on the ground level of the proposed building. Upper glass windows have been provided to satisfy daylighting requirements of the energy code, but ground level street facing façade windows, and rear and side façade window requirements are proposed to be met with inset smooth fiber cement panels and trim, resembling window patterns in rhythm and scale. The panels on the street facing façade are proposed to be painted with murals reflective of the surrounding community. This standard is met.

E. *Articulation and Detailing. The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.*

1. *Articulation. All building elevations that orient to a street or civic space shall have breaks in the wall plane (articulation) of not less than one break for every 30 feet of building length or width, as applicable, pursuant to the following standards, which are generally illustrated in Figures 17-3.2-10, 17-3.2-11, and 17-3.2-12.*
  - a. *A “break” for the purposes of this subsection is a change in wall plane of not less than 24 inches in depth. Breaks may include, but are not limited to, an offset, recess, window reveal, pilaster, frieze, pediment, cornice, parapet, gable, dormer, eave, coursing, canopy, awning, column, building base, balcony, permanent awning or canopy, marquee, or similar architectural feature.*
  - b. *The Planning Official through Site Design Review may approve detailing that does not meet the 24-inch break-in-wall-plane standard where it finds that proposed detailing is more consistent with the architecture of historically significant or historic-contributing buildings existing in the vicinity.*
  - c. *Changes in paint color and features that are not designed as permanent architectural elements, such as display cabinets, window boxes, retractable and similar mounted awnings or canopies, and other similar features, do not meet the 24-inch break-in-wall-plane standard.*
  - d. *Building elevations that do not orient to a street or civic space need not comply with the 24-inch break-in-wall-plane standard but should complement the overall building design.*

**Response:** In lieu of 24-inch offsets every 30', the east building elevation (facing Molalla Avenue) includes articulation in the form of changes in materials, textures, colors, enhanced trim, and inset smooth concrete mural panels. As shown on Sheet A2.10 of Exhibit 2, vertically ribbed siding creates texture and the decorative red trim breaks the plane into multiple segments while also highlighting the features of the façade. These features include the smooth fiber cement insets that will be painted with community murals, as well as the proposed windows and personnel door. The artwork of the community murals will add further articulation through the art itself. The maximum horizontal spacing between the trim and the elements contained within is 12'. The architecture is complementary of the primary Fire Station building. There are not any historically significant or historic-contributing buildings nearby to serve as visual precedents for the proposed design. This standard will be met with approval of the Variance request to provide alternate articulation methods – see responses to approval criteria below under Section 17-4.7.040.

2. *Change in Materials. Elevations should incorporate changes in material that define a building's base, middle, and top, as applicable, and create visual interest and relief. Side and rear elevations that do not face a street, public parking area, pedestrian access way, or plaza may utilize changes in texture and/or color of materials, provided that the design is consistent with the overall composition of the building.*

**Response:** As depicted in the building elevations (Exhibit 2, Sheet A2.10), the proposed building incorporates changes in texture and color reflective of the theme of the existing primary fire station building. The storage building offers white vertically ribbed siding, red trim around the windows (and window-like panels), and a grey roof. The smooth fiber cement panels and mural panels are proposed to be a different material, texture, and color from the vertically ribbed white siding, and the roof will also be a different color and texture from the vertically ribbed white siding. These elements differentiate the base, middle, and top of the building. The proposed murals on the street facing façade create additional visual interest and relief. This standard is met.

3. *Horizontal Lines. New buildings and exterior remodels shall generally follow the prominent horizontal lines existing on adjacent buildings at similar levels along the street frontage. Examples of such horizontal lines include, but are not limited to: the base below a series of storefront windows, an awning or canopy line, a belt course between building stories, a cornice, or a parapet line. Where existing adjacent buildings do not meet the City's current building design standards, a new building may establish new horizontal lines.*

**Response:** As shown on Sheet C1.10, the proposed accessory storage building is not located along the street frontage, as the façade is located over 100' from the street lot line. Additionally, over 50% of the proposed building is behind (west of) an existing residential house which is located between the new building and the street lot line (see Sheet A2.10, detail 2 which shows the approximate location of the house relative to the street facing facade). The visible portion of the street facing façade will also be partially blocked by the proposed 6' high slatted chain link fence. Furthermore, the adjacent buildings do not provide consistent horizontal lines as the buildings to the north are residential houses, to the south is the primary fire station building, and across the street is the Hi-School Pharmacy, all of which have different horizontal lines. Due to the proximity to the residential housing, the building is proposed to generally follow the lines of nearby residential houses (the structures located closest to the proposed new building). This is done through the use of the peaked roof line and mezzanine level windows that emulate residential second story windows. This standard is met.

4. *Ground Floor and Upper Floor Division. A clear visual division shall be maintained between the ground level floor and upper floors, for example, through the use of a belt course, transom, awning, canopy, or similar division.*

**Response:** The proposal consists of a one (1)-story building as shown in Exhibit 2. Therefore, no division between the ground floor and upper floors is needed. This standard does not apply.

5. *Vertical Rhythms. New construction or front elevation remodels shall reflect a vertical orientation, either through breaks in volume or the use of surface details.*

**Response:** As depicted in the building elevations (Exhibit 2, Sheet A2.10), the proposed building establishes a vertical rhythm through the use of vertically ribbed siding, mezzanine level window placement, and peaked roof. This standard is met.

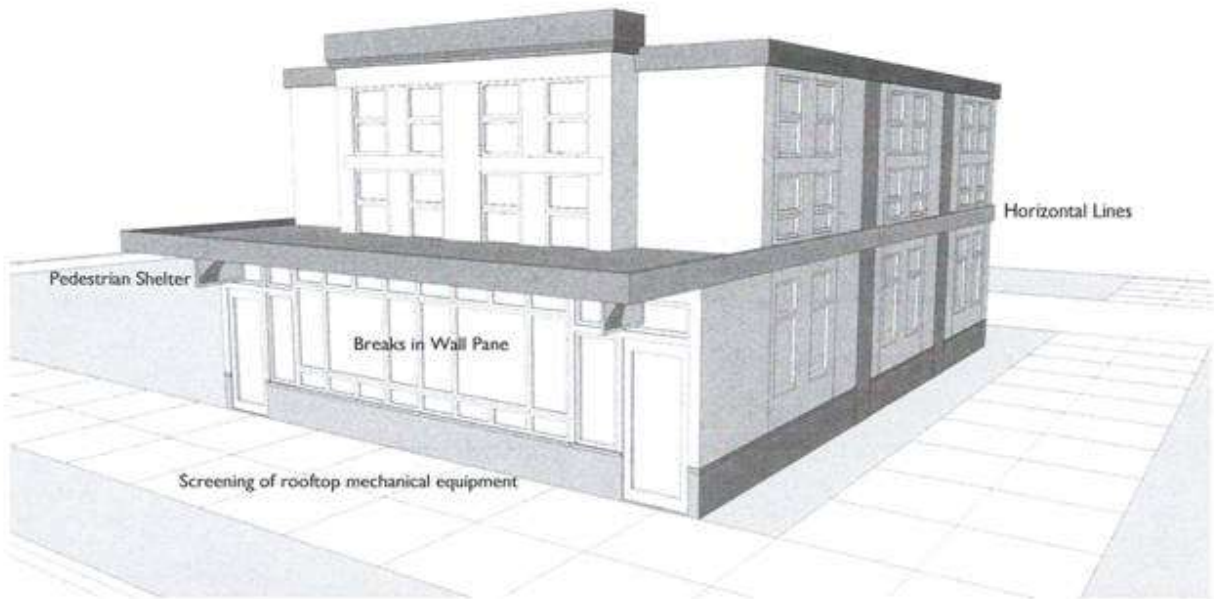


Figure 17-3.2-10 Articulation of Multi-Story Building and Typical Pedestrian Shelter

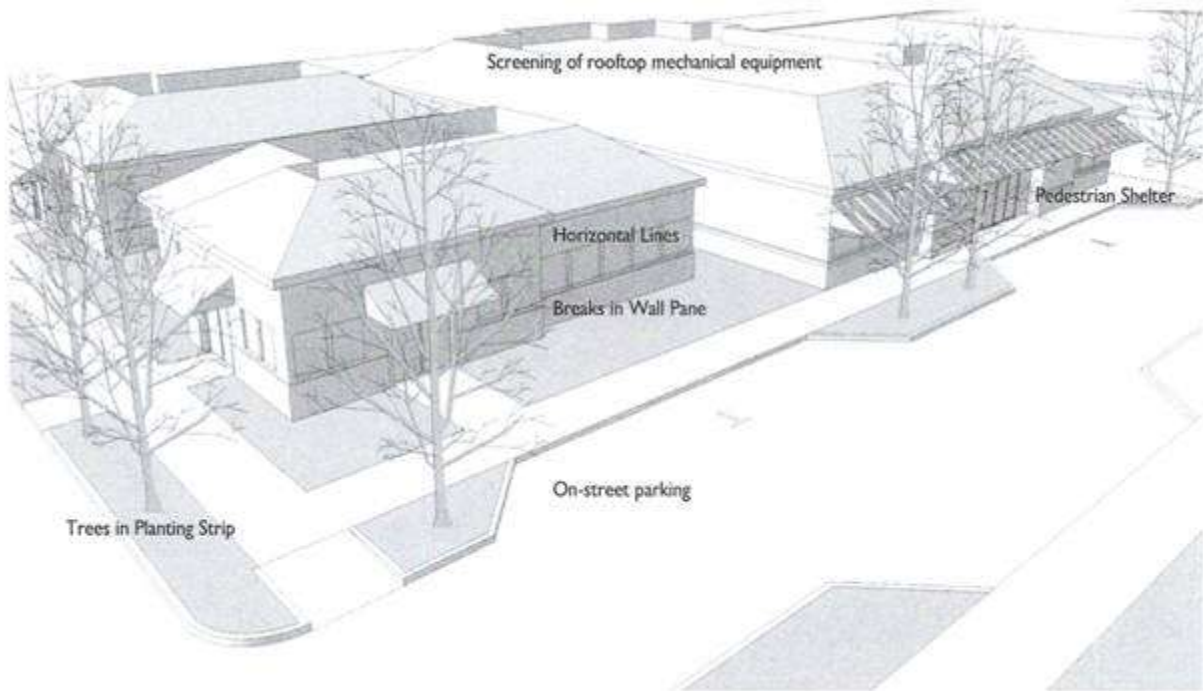


Figure 17-3.2-11 Articulation of Single-Story Buildings and Typical Pedestrian Shelters

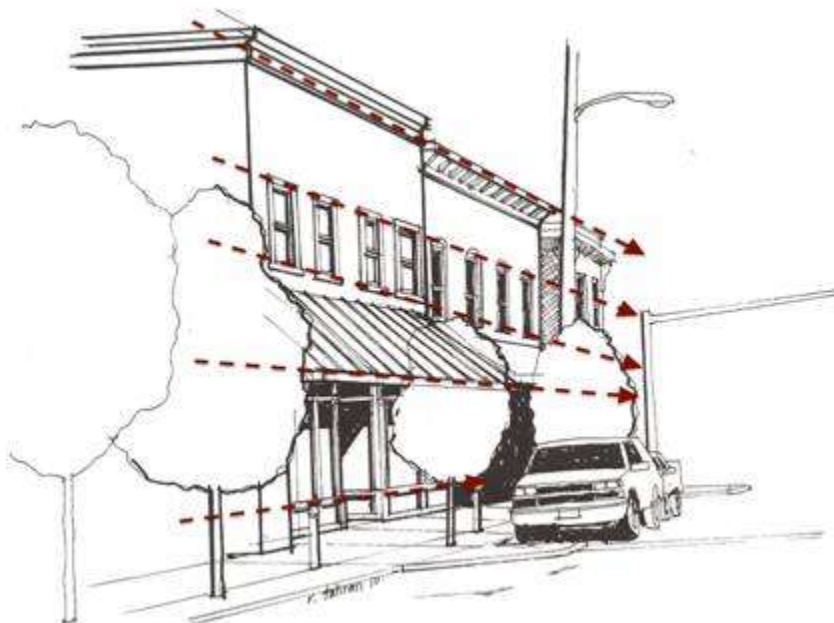


Figure 17-3.2-12 Articulation of Horizontal Lines, Ground Floor and Upper Floor Division

- F. *Pedestrian Shelters. The following standards apply to new buildings and building additions that are subject to Site Design Review. The Planning Official may approve adjustments to the standards as part of a Site Design Review approval, pursuant to Chapters 17-4.2 and 17-4.7, respectively.*
1. *Minimum Pedestrian Shelter Coverage. Permanent awnings, canopies, recesses, or similar pedestrian shelters shall be provided along at least 75 percent of the ground floor elevation(s) of a building where the building abuts a sidewalk, civic space, or pedestrian access way. Pedestrian shelters used to meet the above standard shall extend at least five feet over the pedestrian area; except that the Planning Official, through Site Design Review, may reduce the above standards where it finds that existing right-of-way dimensions, easements, or building code requirements preclude standard shelters. In addition, the above standards do not apply where a building has a ground floor dwelling, as in a mixed-use development or live-work building, and the dwelling has a covered entrance. The Planning Official shall waive the above standards if the pedestrian shelter would extend into the right-of-way and the roadway authority does not allow encroachments in the right-of-way.*
  2. *Pedestrian Shelter Design. Pedestrian shelters shall comply with applicable building codes, and shall be designed to be visually compatible with the architecture of a building. If mezzanine or transom windows exist, the shelter shall be below such windows where practical. Where applicable, pedestrian shelters shall be designed to accommodate pedestrian signage (e.g., blade signs), while maintaining required vertical clearance.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, the proposed building does not abut a sidewalk, civic space, or pedestrian access way. Therefore, the pedestrian shelter standard does not apply.

G. *Mechanical Equipment.*

1. *Building Walls.* Where mechanical equipment, such as utility vaults, air compressors, generators, antennae, satellite dishes, or similar equipment, is permitted on a building wall that abuts a public right-of-way or civic space, it shall be screened pursuant to Chapter 17-3.4. Standpipes, meters, vaults, and similar equipment need not be screened but shall not be placed on a front elevation when other practical alternatives exist; such equipment shall be placed on a side or rear elevation where practical.

**Response:** No mechanical equipment is proposed on a building wall abutting the public right-of-way or civic space. This standard does not apply.

2. *Rooftops.* Except as provided below, rooftop mechanical units shall be set back or screened behind a parapet wall so that they are not visible from any public right-of-way or civic space. Where such placement and screening is not practicable, the Planning Official may approve painting of mechanical units in lieu of screening; such painting may consist of colors that make the equipment visually subordinate to the building and adjacent buildings, if any.

**Response:** No roof-top mechanical equipment is proposed. This standard does not apply.

3. *Ground-Mounted Mechanical Equipment.* Ground-mounted equipment, such as generators, air compressors, trash compactors, and similar equipment, shall be limited to side or rear yards and screened with fences or walls constructed of materials similar to those on adjacent buildings. Hedges, trellises, and similar plantings may also be used as screens where there is adequate air circulation and sunlight, and irrigation is provided. The City may require additional setbacks and noise attenuating equipment for compatibility with adjacent uses.

**Response:** No ground-mounted mechanical equipment is proposed. This standard does not apply.

H. *Civic Space.* Commercial development projects shall provide civic space pursuant to Section 17-3.2.050.

**Response:** Per Section 17-3.2.050, civic space and pedestrian amenities are not required in the PSP zone. This standard does not apply.

I. *Drive-Up and Drive-Through Facilities.* Drive-up and drive-through facilities shall comply with the requirements of Section 17-3.2.060.

**Response:** Per the Molalla Municipal Code, drive-up/drive-through facilities are defined as “a facility or structure that is designed to allow drivers to remain in their vehicles before and during an activity on the site.” No activities are proposed that would require drivers to remain in their vehicles. This standard does not apply.

17-3.2.050 Civic Space and Pedestrian Amenities

- B. *Applicability.* All new commercial and mixed use developments with more than 10,000 square feet of gross leasable floor area within the Central Commercial C-1 and General Commercial C-2 zones are required to meet the standards of this section.

**Response:** The provision of civic space and pedestrian amenities is not required in the PSP zone.

### Chapter 17-3.3 Access and Circulation

#### 17-3.3.030 Vehicular Access and Circulation

- B. *Permit Required. Vehicular access to a public street (e.g., a new or modified driveway connection to a street or highway) requires an approach permit approved by the applicable roadway authority.*

**Response:** No new driveways are proposed. This standard does not apply.

- C. *Traffic Study Requirements. The City, in reviewing a development proposal or other action requiring an approach permit, may require a traffic impact analysis, pursuant to Section 17-3.6.020, to determine compliance with this Code.*

**Response:** The Transportation Analysis Letter (TAL, Exhibit 4) provided by DKS Associates shows the project is not expected to generate any new trips. As discussed in the responses to Section 17-3.6.020, none of the thresholds for a traffic impact analysis are met, therefore a TAL will satisfy the applicable code provisions and a full Transportation Impact Analysis is not required. This standard is met.

- D. *Approach and Driveway Development Standards. Approaches and driveways shall conform to all of the following development standards:*

**Response:** No new driveways, approaches, or modifications to existing driveways or approaches are proposed. As discussed in the TAL in Exhibit 4, there are some access spacing standards not met by the existing conditions, but the existing driveways were previously approved by City staff and have no reported safety or operational issues. Since the proposed development will not add any trips to the driveways, no safety or operational impacts are anticipated and therefore, no mitigations are recommended. The following responses provide further detail on the existing conditions, but no changes are proposed.

1. *The number of approaches on higher classification streets (e.g., collector and arterial streets) shall be minimized; where practicable, access shall be taken first from a lower classification street.*

**Response:** N Molalla Avenue is classified as an arterial street. The subject site has three (3) driveways on Molalla Avenue. The two (2) southern driveways serve the public and emergency response vehicles. The northern driveway provides access to the existing single-family residence on tax lot 8600. These driveways will remain. No new approaches or modifications to existing approaches are proposed.

2. *Approaches shall conform to the spacing standards of subsections E and F, below, and shall conform to minimum sight distance and channelization standards of the roadway authority.*

**Response:** See responses to subsections E and F for compliance with those standards. As discussed in the TAL in Exhibit 4, the sight distance requirements are met and there are no safety concerns associated with sight distance at the driveways. Neither abutting street has existing channelization along the project frontage. This standard is met.

3. *Driveways shall be paved and meet applicable construction standards. Where permeable paving surfaces are allowed or required, such surfaces shall conform to applicable Public Works Design Standards.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, the existing driveways are paved. This standard is met.

4. *The City Engineer may limit the number or location of connections to a street, or limit directional travel at an approach to one-way, right-turn only, or other restrictions, where the roadway authority requires mitigation to alleviate safety or traffic operations concerns.*
5. *Where the spacing standards of the roadway authority limit the number or location of connections to a street or highway, the City Engineer may require a driveway extend to one or more edges of a parcel and be designed to allow for future extension and inter-parcel circulation as adjacent properties develop. The City Engineer may also require the owner(s) of the subject site to record an access easement for future joint use of the approach and driveway as the adjacent property(ies) develop(s).*

**Response:** As discussed in the TAL in Exhibit 4, the proposed development will not add any trips to the driveways and no safety or operational impacts are anticipated, therefore no mitigations are recommended. The applicant has not been made aware of any limitations or restrictions required to mitigate safety or traffic operations concerns. This standard does not apply.

6. *Where applicable codes require emergency vehicle access, approaches and driveways shall be designed and constructed to accommodate emergency vehicle apparatus and shall conform to applicable fire protection requirements. The City Engineer may restrict parking, require signage, or require other public safety improvements pursuant to the recommendations of an emergency service provider.*
7. *As applicable, approaches and driveways shall be designed and constructed to accommodate truck/trailer-turning movements.*

**Response:** As discussed, no new approaches or driveways are proposed as existing driveways have already been approved for emergency vehicle use. New on-site vehicle circulation areas have been designed to meet applicable fire protection requirements and accommodate anticipated turning movements. This standard is met.

8. *Except where the City Engineer and roadway authority, as applicable, permit an open access with perpendicular or angled parking, driveways shall accommodate all projected vehicular traffic on-site without vehicles stacking or backing up onto a street.*

**Response:** No new driveways are proposed but as no new trips are proposed, the existing conditions are anticipated to accommodate all projected vehicular traffic on-site without vehicles stacking or backing up onto a street. This standard is met.

9. *Driveways shall be designed so that vehicle areas, including, but not limited to, drive-up and drive-through facilities and vehicle storage and service areas, do not obstruct any public right-of-way.*

**Response:** No new driveways are proposed but the existing conditions are designed such that vehicle areas do not obstruct any public right-of-way as shown on Sheet C1.10 of Exhibit 2. No drive-up or drive-through facilities are proposed. This standard is met.

10. *Approaches and driveways shall not be wider than necessary to safely accommodate projected peak hour trips and turning movements, and shall be designed to minimize crossing distances for pedestrians.*

**Response:** No new driveways or approaches are proposed, but as discussed in the TAL in Exhibit 4, existing driveways were previously permitted by the City and have no safety or operational issues. This standard is met.

11. *As it deems necessary for pedestrian safety, the City Engineer, in consultation with the roadway authority, as applicable, may require that traffic-calming features, textured*

*driveway surfaces (e.g., pavers or similar devices), curb extensions, signage or traffic control devices, or other features, be installed on or in the vicinity of a site as a condition of development approval.*

**Response:** The applicant has not been made aware of any required traffic calming features by the City Engineer. This standard does not apply.

12. *Construction of approaches along acceleration or deceleration lanes, and along tapered (reduced width) portions of a roadway, shall be avoided; except where no reasonable alternative exists and the approach does not create safety or traffic operations concern.*

**Response:** As depicted in Exhibit 1 and on Sheet C1.10 of Exhibit 2, the site does not abut acceleration or decelerations lanes or tapered portions of a roadway. This standard does not apply.

13. *Approaches and driveways shall be located and designed to allow for safe maneuvering in and around loading areas, while avoiding conflicts with pedestrians, parking, landscaping, and buildings.*

**Response:** No new driveways, approaches, or loading areas are proposed and the site does not have existing loading areas. This standard does not apply.

14. *Where sidewalks or walkways occur adjacent to a roadway, driveway aprons constructed of concrete shall be installed between the driveway and roadway edge. The roadway authority may require the driveway apron be installed outside the required sidewalk or walkway surface, consistent with Americans with Disabilities Act (ADA) requirements, and to manage surface water runoff and protect the roadway surface.*

**Response:** The existing sidewalks and concrete driveway aprons were constructed to City standards to ensure compliance with applicable provisions of the ADA and to properly manage stormwater runoff. As no changes are proposed to public sidewalks or aprons, this standard does not apply.

15. *Where an accessible route is required pursuant to ADA, approaches and driveways shall meet accessibility requirements where they coincide with an accessible route.*

**Response:** The existing sidewalks and concrete driveway aprons were constructed to City standards to ensure compliance with applicable provisions of the ADA. As no changes are proposed to public sidewalks or aprons, this standard does not apply.

16. *The City Engineer may require changes to the proposed configuration and design of an approach, including the number of drive aisles or lanes, surfacing, traffic-calming features, allowable turning movements, and other changes or mitigation, to ensure traffic safety and operations.*

**Response:** The applicant's transportation engineer has not identified any safety deficiencies and staff has not informed the applicant of any required changes to the proposed approach configuration and design to ensure traffic safety and operations. See Exhibit 4. This standard does not apply.

17. *Where a new approach onto a state highway or a change of use adjacent to a state highway requires ODOT approval, the applicant is responsible for obtaining ODOT approval. The City Engineer may approve a development conditionally, requiring the applicant first obtain required ODOT permit(s) before commencing development, in which case the City will work cooperatively with the applicant and ODOT to avoid unnecessary delays.*

**Response:** No new approach onto a state highway is proposed. This standard does not apply.

18. *Where an approach or driveway crosses a drainage ditch, canal, railroad, or other feature that is under the jurisdiction of another agency, the applicant is responsible for obtaining all required approvals and permits from that agency prior to commencing development.*

**Response:** No new approach or driveway is proposed. This standard does not apply.

19. *Where a proposed driveway crosses a culvert or drainage ditch, the City Engineer may require the developer to install a culvert extending under and beyond the edges of the driveway on both sides of it, pursuant to applicable Public Works Design Standards.*

**Response:** No new approach or driveway is proposed. This standard does not apply.

20. *Except as otherwise required by the applicable roadway authority or waived by the City Engineer temporary driveways providing access to a construction site or staging area shall be paved or graveled to prevent tracking of mud onto adjacent paved streets.*

**Response:** The site has existing driveways that can be used for construction access. This standard does not apply.

21. *Development that increases impervious surface area shall conform to the storm drainage and surface water management requirements of Section 17-3.6.050.*

**Response:** See responses to Section 17-3.6.050 for compliance. This standard is met.

E. *Approach Separation from Street Intersections. Except as provided by subsection H, minimum distances shall be maintained between approaches and street intersections consistent with the current version of the Public Works Design Standards and Transportation System Plan.*

**Response:** No new driveways, approaches, or modifications to existing driveways or approaches are proposed, and the existing driveway approach was previously approved by the City. This standard does not apply.

F. *Approach Spacing. Except as provided by subsection H or as required to maintain street operations and safety, the following minimum distances shall be maintained between approaches consistent with the current version of the Public Works Design Standards and Transportation System Plan.*

**Response:** No new driveways, approaches, or modifications to existing driveways or approaches are proposed, and the existing driveway approach was previously approved by the City. This standard does not apply.

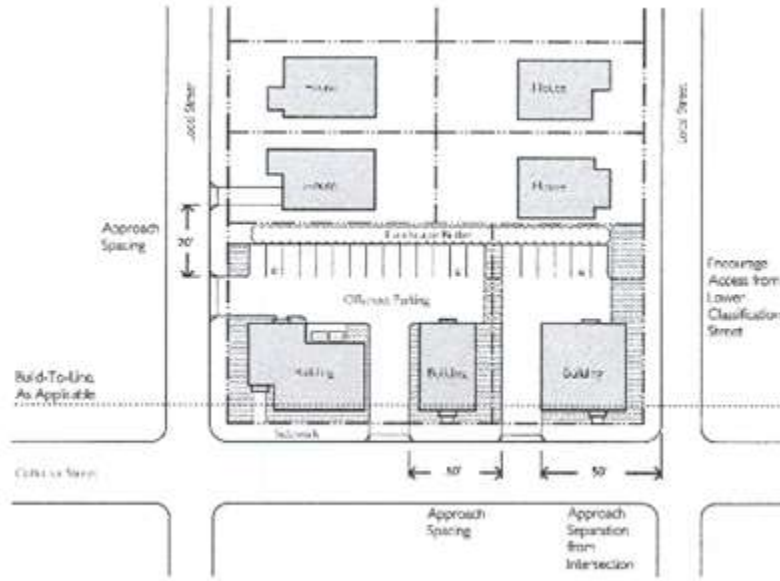


Figure 17-3.3-1 Approach Spacing

- G. *Vision Clearance. No visual obstruction (e.g., sign, structure, solid fence, or shrub vegetation) greater than 2.5 feet in height shall be placed in “vision clearance areas” at street intersections. The minimum vision clearance area may be modified by the Planning Official through a Type I procedure, upon finding that more or less sight distance is required (i.e., due to traffic speeds, roadway alignment, etc.). Placement of light poles, utility poles, and tree trunks should be avoided within vision clearance areas.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, visual clearance areas will not be impacted by the proposed development. No obstructions are proposed within these areas. This standard is met.

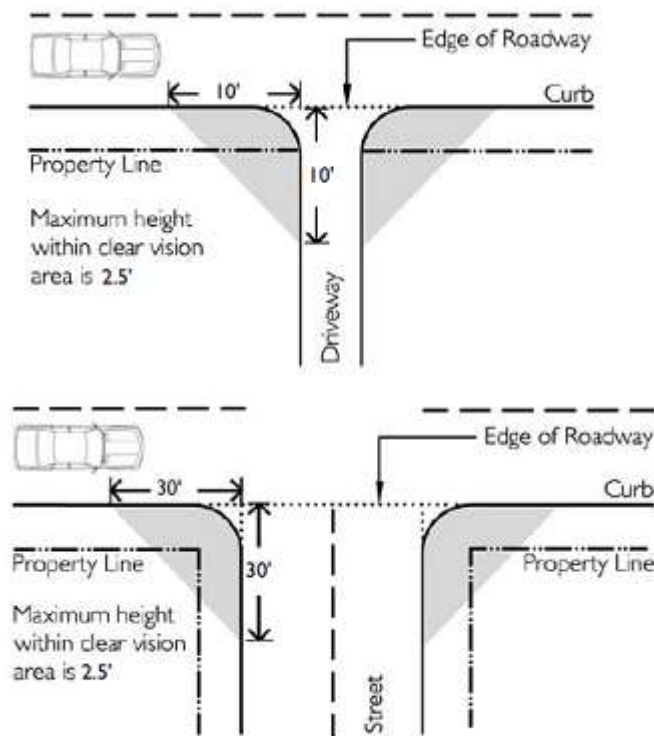


Figure 17-3.3-2 Vision Clearance

- H. *Exceptions and Adjustments. The City Engineer may approve adjustments to the spacing standards of subsections E and F, above, where an existing connection to a City street does not meet the standards of the roadway authority and the proposed development moves in the direction of code compliance. The Planning Official through a Type II procedure may also approve a deviation to the spacing standards on City streets where it finds that mitigation measures, such as consolidated access (removal of one access), joint use driveways (more than one property uses same access), directional limitations (e.g., one-way), turning restrictions (e.g., right-in/right-out only), or other mitigation alleviate all traffic operations and safety concerns.*

**Response:** No adjustments to spacing standards are proposed. This standard does not apply.

- I. *Joint Use Access Easement and Maintenance Agreement. Where the City approves a joint use driveway, the property owners shall record an easement with the deed allowing joint use of and cross access between adjacent properties. The owners of the properties agreeing to joint use of the driveway shall record a joint maintenance agreement with the deed, defining maintenance responsibilities of property owners. The applicant shall provide a fully executed copy of the agreement to the City for its records, but the City is not responsible for maintaining the driveway or resolving any dispute between property owners.*

**Response:** No joint use driveways are proposed. This standard does not apply.

17-3.3.040 Pedestrian Access and Circulation

- B. *Standards. Developments shall conform to all of the following standards for pedestrian access and circulation as generally illustrated in Figure 17-3.3-3:*

1. *Continuous Walkway System.* A pedestrian walkway system shall extend throughout the development site and connect to adjacent sidewalks, if any, and to all future phases of the development, as applicable.
2. *Safe, Direct, and Convenient.* Walkways within developments shall provide safe, reasonably direct, and convenient connections between primary building entrances and all adjacent parking areas, recreational areas, playgrounds, and public rights-of-way conforming to the following standards:
  - a. *The walkway is reasonably direct when it follows a route that does not deviate unnecessarily from a straight line or it does not involve a significant amount of out-of-direction travel.*
  - b. *The walkway is designed primarily for pedestrian safety and convenience, meaning it is reasonably free from hazards and provides a reasonably smooth and consistent surface and direct route of travel between destinations. The Planning Official may require landscape buffering between walkways and adjacent parking lots or driveways to mitigate safety concerns.*
  - c. *The walkway network connects to all primary building entrances, consistent with the building design standards of Chapter 17-3.2 and, where required, Americans with Disabilities Act (ADA) requirements.*
3. *Vehicle/Walkway Separation.* Except as required for crosswalks, per subsection 4, below, where a walkway abuts a driveway or street it shall be raised six inches and curbed along the edge of the driveway or street. Alternatively, the Planning Official may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is physically separated from all vehicle-maneuvering areas. An example of such separation is a row of bollards (designed for use in parking areas) with adequate minimum spacing between them to prevent vehicles from entering the walkway.
4. *Crosswalks.* Where a walkway crosses a parking area or driveway ("crosswalk"), it shall be clearly marked with contrasting paving materials (e.g., pavers, light-color concrete inlay between asphalt, or similar contrasting material). The crosswalk may be part of a speed table to improve driver-visibility of pedestrians. Painted or thermo-plastic striping and similar types of non-permanent applications are discouraged, but may be approved for lesser used crosswalks not exceeding 24 feet in length.
5. *Walkway Width and Surface.* Walkways, including access ways required for subdivisions pursuant to Chapter 17-4.3, shall be constructed of concrete, asphalt, brick or masonry pavers, or other durable surface, as approved by the City Engineer, and not less than six feet wide. Multi-use paths (i.e., designed for shared use by bicyclists and pedestrians) shall be concrete or asphalt and shall conform to the current version of the Public Works Design Standards and Transportation System Plan.
6. *Walkway Construction (Private).* Walkway surfaces may be concrete, asphalt, brick or masonry pavers, or other City-approved durable surface meeting ADA requirements. Walkways shall be not less than six feet in width in commercial and mixed use developments and where access ways are required for subdivisions under Division IV.
7. *Multi-Use Pathways.* Multi-use pathways, where approved, shall be a minimum width and constructed of materials consistent with the current version of the Public Works Design Standards and Transportation System Plan.

**Response:** No new pedestrian walkways are proposed as part of the storage building development which has a limited area of disturbance as shown on Sheet C1.10 of Exhibit 2. The proposed building will not be accessed by the public and only necessary staff will use the building, which is connected to the rest of the site by paved fire truck circulation areas. Due to the nature of the existing emergency service facility and the proposed accessory storage building, this section does not apply.

**Chapter 17-3.4 Landscaping, Fences and Walls, Outdoor Lighting**

**17-3.4.030 Landscaping and Screening**

A. *General Landscape Standard. All portions of a lot not otherwise developed with buildings, accessory structures, vehicle maneuvering areas, or parking shall be landscaped.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, all areas within the ‘Area of Disturbance’ not utilized for buildings, accessory structures, vehicle maneuvering areas, or parking stalls will be landscaped. This standard is met.

B. *Minimum Landscape Area. All lots shall conform to the minimum landscape area standards of the applicable zoning district, as contained in Tables 17-2.2.040.D and 17-2.2.040.E. The Planning Official, consistent with the purposes in Section 17-3.4.010, may allow credit toward the minimum landscape area for existing vegetation that is retained in the development.*

**Response:** Approximately 27,850 SF or 27.8% of the site will be vegetated. This includes existing vegetation on tax lot 9300 which will be maintained, or repaired to existing conditions, during the proposed development. This satisfies the 20% minimum landscape area standard of Table 17-2.2.040.D and the 10% minimum landscape area standard of Table 17-2.2.040.E. See Sheets C1.10 and L1.10 of Exhibit 2. This standard is met.

C. *Plant Selection. A combination of deciduous and evergreen trees, shrubs, and ground covers shall be used for all planted areas, the selection of which shall be based on local climate, exposure, water availability, and drainage conditions, among other factors. When new vegetation is planted, soils shall be amended and irrigation shall be provided, as necessary, to allow for healthy plant growth. The selection of plants shall be based on all of the following standards and guidelines:*

1. *Use plants that are appropriate to the local climate, exposure, and water availability. The presence of utilities and drainage conditions shall also be considered.*

**Response:** The proposed landscape area between the proposed building and the property lines does not allow for new trees as they would be too close to the building. As stated in subsection 10 of this standard, “*Landscape plans shall avoid conflicts between plants and buildings...*”. However, the shrubs and groundcover were selected by Mackenzie landscape architects to meet the site’s local climate, exposure, water availability, and utility and drainage conditions as detailed in Sheet L1.10 of Exhibit 2. This standard is met.

2. *Plant species that do not require irrigation once established (naturalized) are preferred over species that require irrigation.*

**Response:** Mackenzie landscape architects selected native plants that are drought tolerant; however, irrigation will be provided, as noted on Sheet L1.10 of Exhibit 2. This standard is met.

3. *Trees shall be not less than two-inch caliper for street trees and one and one-half-inch caliper for other trees at the time of planting. Trees to be planted under or near power lines shall be selected so as to not conflict with power lines at maturity.*

**Response:** No trees are proposed as the proposed new landscape area immediately surrounds the new building. Trees would be too close to the building in this landscape area, and in the stormwater facility they would interfere with its operation. This standard does not apply.

4. *Shrubs shall be planted from five-gallon containers, minimum, where they are for required screens or buffers, and two-gallon containers minimum elsewhere.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, the proposed shrubs will be planted from two (2)-gallon containers. As no screening or buffering is required, this standard is met.

5. *Shrubs shall be spaced in order to provide the intended screen or canopy cover within two years of planting.*

**Response:** The proposed shrubs were selected by Mackenzie landscape architects to provide the intended impact within two (2) years of planting, however, there is no required screening or canopy cover included in this development. This standard is met.

6. *All landscape areas, whether required or not, that are not planted with trees and shrubs or covered with allowable non-plant material, shall have ground cover plants that are sized and spaced to achieve plant coverage of not less than 75 percent at maturity.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, landscape areas not planted with trees, shrubs, or allowable non-plant material, are proposed to have ground cover plants that will meet the specified standards. This standard is met.

7. *Bark dust, chips, aggregate, or other non-plant ground covers may be used, but shall cover not more than 35 percent of any landscape area. Non-plant ground covers cannot be a substitute for required ground cover plants.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, non-plant ground cover (gravel) is proposed immediately surrounding the new building. The gravel area extends 2' from the building. This small area is well below the 35% standard. This standard is met.

8. *Where stormwater retention or detention, or water quality treatment facilities are proposed, they shall meet the requirements of the current version of the Public Works Design Standards.*

**Response:** The proposed detention pond to the west of the proposed building was designed by Mackenzie professional Civil Engineers with plantings chosen by Mackenzie professional Landscape Architects to meet the Public Works Design Standards. See Sheets C1.10 and L1.10. This standard is met.

9. *Existing mature trees that can thrive in a developed area and that do not conflict with other provisions of this Code shall be retained where specimens are in good health, have desirable aesthetic characteristics, and do not present a hazard.*

**Response:** There is one (1) tree proposed for removal as it conflicts with the proposed building location(see Sheet C1.10). No other trees are proposed for removal. This standard does not apply.

10. *Landscape plans shall avoid conflicts between plants and buildings, streets, walkways, utilities, and other features of the built environment.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, the proposed landscape area and plants will not conflict with buildings, walkways, streets, utilities, or other feature of the built environment. This standard is met.

11. *Evergreen plants shall be used where a sight-obscuring landscape screen is required.*

**Response:** The proposed development does not require any sight-obscuring landscape screen. This standard does not apply.

12. *Deciduous trees should be used where summer shade and winter sunlight is desirable.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, the proposed landscape area is not an area where summer shade or winter sunlight is desirable since the new landscape area (other than the stormwater pond) is a small strip of space along the back side of the proposed storage building. Additionally, planting trees in this space would cause conflicts with the building due to the width of the planting area. Instead, shrubs and groundcover are proposed for aesthetic and environmental benefit. This standard is met.

13. *Landscape plans should provide focal points within a development, for example, by preserving large or unique trees or groves or by using flowering plants or trees with fall color.*

**Response:** The proposed development and associated area of impact is a small portion of the overall existing site. The proposed storage building, associated vehicle circulation area, and stormwater pond are the only proposed changes. As such, the proposed new landscaping is limited to the area surrounding the new building that is not being used for other development features (and the pond). The location of the area to be landscaped is not suited to be a focal point of the site. This standard does not apply.

14. *Landscape plans should use a combination of plants for seasonal variation in color and yearlong interest.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, a combination of flowering and colorful plants is proposed in the new landscape area. The plans include four (4) different shrub species chosen by Mackenzie landscape architects to provide seasonal interest as shown in the plant schedule. This standard is met.

15. *Where plants are used to screen outdoor storage or mechanical equipment, the selected plants shall have growth characteristics that are compatible with such features.*

**Response:** Plants are not proposed to screen outdoor storage or mechanical equipment. This standard does not apply.

16. *Landscape plans shall provide for both temporary and permanent erosion control measures, which shall include plantings where cuts or fills, including berms, swales, stormwater detention facilities, and similar grading, is proposed.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, permanent erosion control will be maintained through the shrubs, groundcover, and stormwater plantings proposed. Per Sheet C1.10 of Exhibit 2, the site is relatively level which will minimize erosion potential. No berms or swales are proposed. This standard is met.

17. *When new vegetation is planted, soils shall be amended and irrigation provided, as necessary, until the plants are naturalized and able to grow on their own.*

**Response:** As shown on Sheet L1.10 of Exhibit 2, landscape areas with new vegetation will be amended with 18" of amended topsoil. Irrigation for the proposed new plants will connect to the existing irrigation system as discussed in the landscape notes. This standard is met.

- D. *Central Commercial C-1 District Streetscape Standard. Developers of projects within the Central Commercial C-1 zoning district can meet the landscape area requirement of subsection B, in part, by installing street trees in front of their projects. The Planning Official shall grant credit toward the landscape area requirement using a ratio of 1:1, where one square foot of planted area (e.g., tree well or planter surface area) receives one square foot of credit. The Planning Official may grant additional landscape area credit by the same ratio where the developer widens the sidewalk or creates a plaza or other civic space pursuant to Section 17-3.2.050.*

**Response:** The subject site is located in the Public and Semi-Public (PSP) zoning district, not the Central Commercial (C-1) zoning district. Therefore, this standard does not apply.

- E. *Parking Lot Landscaping. All of the following standards shall be met for parking lots. If a development contains multiple parking lots, then the standards shall be evaluated separately for each parking lot.*

[detailed provisions omitted for brevity]

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, no new parking areas are proposed. This section does not apply.

F. *Screening Requirements. Screening is required for outdoor storage areas, unenclosed uses, and parking lots, and may be required in other situations as determined by the Planning Official. Landscaping shall be provided pursuant to the standards of subsections F.1 through 3. (See also Figure 17-3.4-4.)*

1. *Outdoor Storage and Unenclosed Uses. All areas of a site containing or proposed to contain outdoor storage of goods, materials, equipment, and vehicles (other than required parking lots and service and delivery areas, per Site Design Review), and areas containing junk, salvage materials, or similar contents, shall be screened from view from adjacent rights-of-way and residential uses by a sight-obscuring fence, wall, landscape screen, or combination of screening methods. See also Section 17-3.4.040 for related fence and wall standards.*
2. *Parking Lots. The edges of parking lots shall be screened to minimize vehicle headlights shining into adjacent rights-of-way and residential yards. Parking lots abutting a sidewalk or walkway shall be screened using a low-growing hedge or low garden wall to a height of between three feet and four feet.*
3. *Other Uses Requiring Screening. The Planning Official may require screening in other situations as authorized by this Code, including, but not limited to, outdoor storage areas, blank walls, Special Uses pursuant to Chapter 17-2.3, flag lots, and as mitigation where an applicant has requested an adjustment pursuant to Chapter 17-4.7.*

**Response:** No outdoor storage, unenclosed uses, or new parking areas are proposed. The applicant has not been made aware of any other uses requiring screening by the Planning Official. This standard does not apply.

G. *Maintenance. All landscaping shall be maintained in good condition, or otherwise replaced by the property owner.*

**Response:** The applicant acknowledges its ongoing responsibility to maintain landscaping. This standard is met.

#### 17-3.4.040 Fences and Walls

B. *Applicability. Section 17-3.4.040 applies to all fences, and to walls that are not part of a building, including modifications to existing fences and walls.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, a 6' slatted chain link fence is proposed. This standard is applicable.

C. *Height.*

2. *Non-Residential Zones. Fences and freestanding walls (i.e., exclusive of building walls) for non-residential uses shall not exceed the following height above grade, where grade is measured from the base of the subject fence or wall.*
  - a. *Within Front or Street-Facing Side Yard Setback. Four feet, except the following additional height is allowed for properties located within an industrial, public, or institutional zone:*
    - (1) *Where approved by the City Planning Official, a fence constructed of open chain link or other "see-through" composition that allows 90 percent light transmission may reach a height of up to eight feet.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, no fences or freestanding walls are proposed within the front or street-facing setback. This standard does not apply.

- b. *Within an Interior Side or Rear Yard Setback. Eight feet; except the fence or wall height, as applicable, shall not exceed the distance from the fence or wall line to the nearest primary structure on an adjacent property.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, no fences or walls are proposed within an interior side or rear yard setback. This standard does not apply.

3. *All Zones. Fences and walls shall comply with the vision clearance standards of Section 17-3.3.030.G. Other provisions of this Code, or the requirements of the roadway authority, may limit allowable height of a fence or wall below the height limits of this section.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, no fences or walls are proposed within visual clearance areas. This standard is met.

- D. *Materials. Prohibited fence and wall materials include straw bales, tarps, barbed or razor wire (except in the M-2 Heavy Industrial zone); scrap lumber, untreated wood (except cedar or redwood), corrugated metal, sheet metal, scrap materials; dead, diseased, or dying plants; and materials similar to those listed herein.*

**Response:** As shown on Sheet C1.10 of Exhibit 2, the proposed fencing will be chain link with slats, which are not prohibited materials. This standard is met.

- E. *Permitting. A Type I approval is required to install a fence of six feet or less in height, or a wall that is four feet or less in height. All other walls and fences require review and approval by the Planning Official through a Type II procedure. The Planning Official may require installation of walls or fences as a condition of approval for development, as provided by other Code sections. A building permit may be required for some fences and walls, pursuant to applicable building codes. Walls greater than four feet in height shall be designed by a Professional Engineer licensed in the State of Oregon.*

**Response:** The applicant is requesting approval for the proposed fence as part of this application. Responses to this section (Chapter 17-3.4) demonstrate compliance with the applicable fence and wall standards of the Molalla Municipal Code. This standard is met.

- F. *Maintenance. Fences and walls shall be maintained in good condition, or otherwise replaced by the property owner.*

**Response:** The applicant acknowledges its ongoing responsibility to maintain fences and walls. This standard is met.

#### 17-3.4.050 Outdoor Lighting

- B. *Applicability. All outdoor lighting shall comply with the standards of this section.*

**Response:** No new outdoor lighting is proposed. This section does not apply.

### Chapter 17-3.5 Parking and Loading

#### 17-3.5.020 Applicability and General Regulations

- A. *Where the Regulations Apply. The regulations of this chapter apply to all parking areas in all zones, at all times, whether parking is required by this Code or put in for the convenience of property owners or users.*

**Response:** The proposed development does not include or require any new vehicle parking or loading, and no changes to the existing vehicle parking are proposed. Two (2) new bicycle parking spaces are proposed. Only applicable sections of the Parking and Loading Chapter are addressed below.

17-3.5.030 Automobile Parking

- A. *Minimum Number of Off-Street Automobile Parking Spaces. Except as provided by this subsection A, or as required for Americans with Disabilities Act compliance under subsection G, off-street parking shall be provided pursuant to one of the following three standards:*
1. *The standards in Table 17-3.5.030.A;*
  2. *A standard from Table 17-3.5.030.A for a use that the Planning Official determines is similar to the proposed use; or*
  3. *Subsection B Exceptions, which includes a Parking Demand Analysis option.*

TABLE 17-3.5.030.A - AUTOMOBILE PARKING SPACES BY USE (EXCERPT)	
Use Categories	Minimum Parking per Land Use
Office	General Office: 1 space per 500 sq. ft. floor area
Community Service, including Government Offices and Services	Parking based on applicant's projected parking demand, subject to City approval, except as specifically required elsewhere in this table for individual uses (See public assembly, office, retail, housing, etc.)
Public Assembly	1 space per 75 sq. ft. of public assembly area; or as required by Conditional Use Permit (Chapter 17-4.4)

**Response:** The applicable parking standard is "Community service, including Government Offices and Services," which does not have a specified parking ratio. The proposed building will be dedicated to storage and will not have public assembly, office, retail, or housing uses. As the building will be used to store existing applicant-owned vehicles, it will not result in additional employee or visitor usage, and no parking is proposed as part of this application. This standard is met.

17-3.5.040 Bicycle Parking

- A. *Standards. Bicycle parking spaces shall be provided with new development and, where a change of use occurs, at a minimum, shall follow the standards in Table 17-3.5.040.A. Where an application is subject to Conditional Use Permit approval or the applicant has requested a reduction to an automobile-parking standard, pursuant to Section 17-3.5.030.C, the Planning Official may require bicycle parking spaces in addition to those in Table 17-3.5.040.A.*

TABLE 17-3.5.040.A – MINIMUM REQUIRED BICYCLE PARKING SPACES (EXCERPT)	
Use	Minimum Parking per Land Use
Community Service	2 bike spaces

**Response:** The proposed fire station accessory building is a community service use and thus the fire station as a whole requires two (2) bike spaces. As shown on Sheet C1.10, two (2) bike parking spaces are proposed to be located near the southeast corner of the existing fire station building. This standard is met.

- B. *Design. Bicycle parking shall consist of staple-design steel racks or other City-approved racks, lockers, or storage lids providing a safe and secure means of storing a bicycle, consistent with the Public Works Design Standards.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, one (1) staple-design steel bike rack is proposed consistent with the Public Works Design Standards. This standard is met.

- C. *Exemptions. This section does not apply to single-family and duplex housing, home occupations, and agricultural uses.*

**Response:** The proposed development does not include the uses specified above. This standard does not apply.

D. *Hazards. Bicycle parking shall not impede or create a hazard to pedestrians or vehicles, and shall be located so as to not conflict with the vision clearance standards of Section 17-3.3.030.G.*

**Response:** As shown on Sheets C1.10 and L1.10 of Exhibit 2, the proposed bicycle parking does not conflict with any vision clearance areas and does not impede pedestrian flow. This standard is met.

### **Chapter 17-3.6 Public Facilities**

#### *17-3.6.020 Transportation Standards*

##### A. *General Requirements.*

1. *Except as provided by subsection A.5, existing substandard streets and planned streets within or abutting a proposed development shall be improved in accordance with the standards of Chapter 17-3.6 as a condition of development approval.*

**Response:** N Molalla Avenue is classified as an arterial street. Per comments from City staff in the Pre-Application Conference Notes, the current right-of-way (ROW) width on Molalla Avenue is 60' and the paved width is approximately 38'. Arterial streets with parking typically require a ROW of 60' and a pavement width of 40'. However, frontage improvements along this stretch of N Molalla Ave were completed in 2017 as part of the Public Improvement project 16-01, and staff has indicated that additional frontage improvements will not be required as the current improvements were designed in accordance with the Downtown Master Plan (DMP).

Kennel Avenue is classified as a neighborhood street. Per comments from City staff in the Pre-Application Conference Notes, the current ROW is 50' wide with an approximate pavement width of 38', and no additional street dedication is required as the existing widths comply with applicable street standards.

This standard is met.

2. *All street improvements, including the extension or widening of existing streets and public access ways, shall conform to Section 17-3.6.020, and shall be constructed consistent with the City of Molalla Public Works Design Standards.*

**Response:** No street improvements are proposed to existing streets or public accessways. The existing conditions are either compliant with Public Works Design Standards or were improved with the 2017 Public Improvement project 16-01. This standard does not apply.

3. *All new streets shall be contained within a public right-of-way. Public access ways (e.g., pedestrian ways) may be contained within a right-of-way or a public access easement, subject to review and approval of the City Engineer.*

**Response:** No new streets or public access ways are proposed. This standard does not apply.

4. *The purpose of this subsection is to coordinate the review of land use applications with roadway authorities and to implement Section 660-012-0045(2)(e) of the State Transportation Planning Rule, which requires the City to adopt a process to apply conditions to development proposals in order to minimize impacts and protect transportation facilities. The following provisions also establish when a proposal must be reviewed for potential traffic impacts; when a Transit Analysis Letter (TAL) or Traffic Impact Analysis (TIA) must be submitted with a development application in order to determine whether conditions are needed to minimize impacts to and protect*

transportation facilities; the required contents of a TAL/TIA; and who is qualified to prepare the analysis.

a. *Determining the Required Level of Transportation Analysis and Documentation. A Transportation Impact Analysis (TIA) is required for developments that are expected to have an impact on the transportation system. The analysis shall be based upon the latest edition of the ITE Trip Generation Manual or an agreed-upon alternative methodology where credible data is available to support the alternative methodology. When specific criteria generally associated with small developments are met, a Transportation Analysis Letter (TAL) may be substituted for the required TIA. At the discretion of the City Engineer, a TAL may satisfy the City's transportation analysis requirements, in lieu of a TIA when a development meets all the following criteria:*

(1) *The development generates fewer than 25 peak hour trips during either the AM or PM peak hour. (Two examples of common developments generating fewer trips than these threshold levels are: a subdivision containing 25 or fewer single-family residences or a general office building less than 15,000 square feet.)*

**Response:** The project is not expected to generate an increase in trips since the proposed storage building will not accommodate additional employees or result in additional emergency response demand from the station. As there are no new trips expected to be generated by the development, the applicant's transportation engineer has provided a Transportation Analysis Letter (TAL) in Exhibit 4. This standard is met.

(2) *The development is not expected to impact intersections that currently fail to meet the City's level of service standards or intersections that are operating near the limits of the acceptable level of service thresholds during a peak operating hour.*

**Response:** As discussed in the TAL (Exhibit 4), the proposed storage building will not result in an increase in intersection movements. This standard is met.

(3) *The development is not expected to significantly impact adjacent roadways and intersections that are high accident locations, areas that contain an identified safety concern, or high concentration of pedestrians or bicyclists such as school zones.*

**Response:** As discussed in the TAL (Exhibit 4), the development is not expected to impact adjacent roadways or intersections since the project is not expected to generate any new trips. This standard is met.

(4) *The development generates an increase in use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by less than 10 vehicles per day.*

**Response:** As discussed in the TAL (Exhibit 4), the development is not expected to generate any new trips. This standard is met.

b. *Transportation Analysis Letter Contents. If the City determines, based on information provided by the applicant and in accordance with the criteria specified in Section 3.1, that a TAL is the appropriate document to submit. The following requirements shall apply.*

- (1) *The TAL shall be prepared by or prepared under the direct supervision of a registered professional engineer who shall sign and stamp the TAL.*
- (2) *The TAL shall include the following:*
  - i. *The expected trip generation of the proposed development including the AM peak hour, the PM peak hour, daily traffic, and other germane periods as may be appropriate, together with appropriate documentation and references.*
  - ii. *Site plan showing the location of all access driveways or private streets where they intersect with public streets plus driveways of abutting properties and driveways on the opposite side of the street from the proposed development.*
  - iii. *Documentation that all site access driveways meet City of Molalla Private Access Driveway Width Standards.*
  - iv. *Documentation that all site access driveways meet City of Molalla's Minimum City Street Intersection Spacing Standards.*
  - v. *Documentation that all new site accesses and/or public street intersections meet AASHTO intersection sight distance guidelines.*
  - vi. *Documentation that there are no inherent safety issues associated with the design and location of the site access driveways.*
  - vii. *Documentation that the applicant has reviewed the City's TSP and that proposed streets and frontage improvements do or will comply with any applicable standards regarding the functional classification, typical sections, access management, traffic calming and other attributes as appropriate.*

**Response:** The applicant has attached a TAL (Exhibit 4) consistent with the requirements outlined above. This standard is met.

5. *The City Engineer may waive or allow deferral of standard street improvements, including sidewalk, roadway, bicycle lane, undergrounding of utilities, and landscaping, as applicable, where one or more of the following conditions in subdivisions (a) through (d) is met. Where the City Engineer agrees to defer a street improvement, it shall do so only where the property owner agrees not to remonstrate against the formation of a local improvement district in the future.*
  - a. *The standard improvement conflicts with an adopted capital improvement plan.*
  - b. *The standard improvement would create a safety hazard.*
  - c. *It is unlikely due to the developed condition of adjacent property that the subject improvement would be extended in the foreseeable future, and the improvement under consideration does not by itself significantly improve transportation operations or safety.*
  - d. *The improvement under consideration is part of an approved partition and the proposed partition does not create any new street.*

**Response:** The applicant is not seeking any waivers or deferrals of standard street improvements. This standard does not apply.

**B. Street Location, Alignment, Extension, and Grades.**

[Detailed provisions omitted for brevity]

**Response:** No new streets or street extensions are proposed. This standard does not apply.

**C. Rights-of-Way and Street Section Widths.**

1. *Street rights-of-way and section widths shall comply with the current version of the Public Works Design Standards and Transportation System Plan. The standards are intended: to provide for streets of suitable location, width, and design to accommodate expected vehicle, pedestrian, and bicycle traffic; to afford satisfactory access to law enforcement, fire protection, sanitation, and road maintenance equipment; and to provide a convenient and accessible network of streets, avoiding undue hardships to adjoining properties.*
2. *All streets shall be improved in accordance with the construction standards and specifications of the applicable roadway authority, including requirements for pavement, curbs, drainage, striping, and traffic control devices. Where a planter strip is provided it shall consist of a minimum five-foot-wide strip between the sidewalk and the curb or roadway. Where a swale is provided, it shall either be placed between the roadway and sidewalk or behind the sidewalk on private property, subject to City Engineer approval and recording of required public drainage way and drainage way maintenance easements. Streets with parking on one side only should be avoided. When used, they must be posted NO PARKING.*
3. *Where a range of street width or improvement options is indicated, the City Engineer shall determine requirements based on the advice of a qualified professional and all of the following factors:*
  - a. *Street classification and requirements of the roadway authority, if different than the City's street classifications and requirements;*
  - b. *Existing and projected street operations relative to applicable standards;*
  - c. *Safety of motorists, pedestrians, bicyclists, and South Clackamas Transit District (SCTD) users, including consideration of accident history;*
  - d. *Convenience and comfort for pedestrians, bicyclists, and SCTD users;*
  - e. *Provision of on-street parking;*
  - f. *Placement of utilities;*
  - g. *Street lighting;*
  - h. *Slope stability, erosion control, and minimizing cuts and fills;*
  - i. *Surface water management and storm drainage requirements;*
  - j. *Emergency vehicles or apparatus and emergency access, including evacuation needs;*
  - k. *Transitions between varying street widths (i.e., existing streets and new streets); and*
  - l. *Other factors related to public health, safety, and welfare.*

**Response:** As discussed above in subsection A, no changes are proposed to existing streets. The existing conditions are either compliant with the Transportation System Plan or were improved with the 2017 Public Improvement project 16-01. As the project would not generate additional trips (per Exhibit 4), street improvements are not warranted. These standards do not apply.

**D. *Transportation Connectivity and Future Street Plans. The following standards apply to the creation of new streets:***

1. *Intersections. Streets shall be located and designed to intersect as nearly as possible to a right angle. Street intersections shall meet the current requirements of the Public Works Design Standards and Transportation System Plan.*

**Response:** No new streets or street intersections are proposed. This standard does not apply.

2. *Access Ways. The Planning Commission, in approving a land use application with conditions shall require a developer to provide an access way where the creation of a cul-de-sac or dead-end street is unavoidable and the access way connects or may in the future connect, the end of the street to another street, a park, or a public access way, except*

where the City Engineer and City Planner determine the access way is not feasible. Where an access way is required, it shall be not less than 10 feet wide and shall contain a minimum eight-foot-wide concrete surface or other all-weather surface approved by the City Engineer. Access ways shall be contained within a public right-of-way or public access easement, as required by the City.

**Response:** There are no cul-de-sac or dead-end streets proposed, so no access ways are merited. This standard does not apply.

3. *Connectivity to Abutting Lands. The street system of a proposed subdivision shall be designed to connect to existing, proposed, and planned streets adjacent to the subdivision. Wherever a proposed development abuts unplatted land or a future development phase of an existing development, street stubs shall be provided to allow access to future abutting subdivisions and to logically extend the street system into the surrounding area. Street ends shall be designed to facilitate future extension in terms of grading, width, and temporary barricades.*

**Response:** No subdivision is proposed, and no new streets are proposed. This standard does not apply.

4. *Street Connectivity and Formation of Blocks. In order to promote efficient vehicular and pedestrian circulation throughout the City, subdivisions and site developments shall be served by an interconnected street network, pursuant to the current version of the Public Works Design Standards and Transportation System Plan. Where a street connection cannot be made due to physical site constraints, approach spacing requirements, access management requirements, or similar restrictions; a pedestrian access way connection shall be provided pursuant to Chapter 17-3.3. Streets and accessways need not be required where one or more of the following conditions exist:
 
  - a. *Physical or topographic conditions make a street or accessway connection impracticable. Such conditions include, but are not limited to, freeways, railroads, steep slopes, wetlands or other bodies of water where a connection could not reasonably be provided:*
  - b. *Buildings or other existing development on adjacent lands physically preclude a connection now or in the future considering the potential for redevelopment; or*
  - c. *Where streets or accessways would violate provisions of leases, easements, covenants, restrictions or other agreements existing as of May 1, 1995, which preclude a required street or accessway connection.**

**Response:** No subdivision is proposed, and no new streets are proposed, so there is no opportunity to alter the existing block pattern. This standard does not apply.

5. *Cul-de-Sac Streets. A cul-de-sac street shall only be used where the City Engineer determines that environmental or topographical constraints, existing development patterns, or compliance with other applicable City requirements preclude a street extension. Where the City determines that a cul-de-sac is allowed, cul-de-sac length, turn-around type, and pedestrian access to adjoining properties shall meet the requirements of the current version of the Public Works Design Standards and Transportation System Plan and subsection D.2.*

**Response:** No cul-de-sac streets are proposed. This standard does not apply.

6. *Future Street Plan. Where a subdivision is proposed adjacent to other developable land, a future street plan shall be filed by the applicant in conjunction with an application for a subdivision in order to facilitate orderly development of the street system. The plan shall*

*show the pattern of existing and proposed future streets from the boundaries of the proposed land division and shall include other divisible parcels within 600 feet surrounding and adjacent to the proposed subdivision. The street plan is binding when part of a multi-phased master planned development. The plan must demonstrate, pursuant to City standards, that the proposed development does not preclude future street connections to adjacent development land.*

**Response:** No subdivision is proposed, and the applicant is not proposing to alter the existing street pattern. This standard does not apply.

7. *Private Streets and Gated Drives. Private streets and gated drives serving more than two dwellings (i.e., where a gate limits access to a development from a public street), are prohibited.*

**Response:** No private streets or gated drives are proposed. This standard does not apply.

E. *Engineering Design Standards. Street design shall conform to the standards of the applicable roadway authority; for City streets that is the current version of the Public Works Design Standards and Transportation System Plan. Where a conflict occurs between this Code and the Public Works Design Standards, the provisions of the Design Standards shall govern.*

**Response:** As previously stated, no changes are proposed to existing street design. This standard does not apply.

F. *Fire Code Standards. Where Fire Code standards conflict with City standards, the City shall consult with the Fire Marshal in determining appropriate requirements. The City shall have the final determination regarding applicable standards.*

**Response:** The applicant has incorporated comments from the Fire Marshal into the site design and the Fire Marshal did not identify conflicts between the Fire Code and City standards. This standard does not apply.

G. *Substandard Existing Right-of-Way. Where an existing right-of-way adjacent to a proposed development is less than the standard width, the City Engineer may require the dedication of additional rights-of-way at the time of Subdivision, Partition, or Site Plan Review, pursuant to the standards in the Public Works Design Standards and Transportation System Plan.*

**Response:** Staff have indicated that no new dedication of right-of-way will be required. This standard does not apply.

H. *Traffic Calming. The City may require the installation of traffic calming features such as traffic circles, curb extensions, reduced street width (parking on one side), medians with pedestrian crossing refuges, speed tables, speed humps, or special paving to slow traffic in neighborhoods or commercial areas with high pedestrian traffic.*

**Response:** Neither the City Engineer nor the applicant's transportation engineer (Exhibit 4) has identified the need for traffic calming measures. This standard does not apply.

I. *Sidewalks, Planter Strips, and Bicycle Lanes. Except where the City Engineer grants a deferral of public improvements, pursuant to Chapter 17-4.2 or Chapter 17-4.3, sidewalks, planter strips, and bicycle lanes shall be installed concurrent with development or widening of new streets, pursuant to the requirements of this chapter. Maintenance of sidewalks and planter strips in the right-of-way is the continuing obligation of the adjacent property owner.*

**Response:** No improvements to sidewalks, planter strips, or bike lanes are proposed. The continuing maintenance of sidewalk and planter strips obligation is understood. This standard is met.

*J. Streets Adjacent to Railroad Right-of-Way. When a transportation improvement is proposed within 300 feet of a railroad crossing, or a modification is proposed to an existing railroad crossing, the Oregon Department of Transportation and the rail service provider shall be notified and given an opportunity to comment, in conformance with the provisions of Division IV. Private crossing improvements are subject to review and licensing by the rail service provider.*

**Response:** The site is not adjacent to a railroad right-of-way. This standard does not apply.

*K. Street Names. No new street name shall be used which will duplicate or be confused with the names of existing streets in the City of Molalla or vicinity. Street names shall be submitted to the City for review and approval in consultation with Clackamas County and emergency services.*

**Response:** There are no new streets proposed, therefore there are no new street names proposed. This standard does not apply.

*L. Survey Monuments. Upon completion of a street improvement and prior to acceptance by the City, it shall be the responsibility of the developer's registered professional land surveyor to provide certification to the City that all boundary and interior monuments have been reestablished and protected.*

**Response:** No survey monumentation is required as no new lots or streets are proposed. This standard does not apply.

*M. Street Signs. The city, county, or state with jurisdiction shall install all signs for traffic control and street names. The cost of signs required for new development shall be the responsibility of the developer. Street name signs shall be installed at all street intersections. Stop signs and other signs may be required.*

**Response:** No new street signs are proposed. This standard does not apply.

*N. Streetlight Standards. Streetlights shall be relocated or new lights installed, as applicable, with street improvement projects. Streetlights shall conform to City standards, be directed downward, and full cutoff and full shielding to preserve views of the night sky and to minimize excessive light spillover onto adjacent properties.*

**Response:** No street improvement projects are proposed. This standard does not apply.

*O. Mail Boxes. Mailboxes shall conform to the requirements of the United States Postal Service and the State of Oregon Structural Specialty Code.*

**Response:** No new mailboxes or changes to the existing mailbox are proposed. This standard does not apply.

*P. Street Cross-Sections. The final lift of pavement shall be placed on all new constructed public roadways prior to final City acceptance of the roadway.*

**Response:** No new public roadways are proposed, and patching will occur in accordance with City standards. This standard is met.

#### 17-3.6.030 Required Parkland Dedication or Fee in Lieu

*A. Minimum Parkland Dedication Requirements. Residential subdivisions, master planned developments, and multifamily developments shall be required to dedicate parkland to the City. Middle housing land divisions pursuant to ORS 92.031 shall not be subject to the parkland dedication requirements in this section.*

[detailed provisions omitted for brevity]

**Response:** The proposed development is not a residential subdivision, master planned development, or multifamily development. This standard does not apply.

### 17-3.6-040 Sanitary Sewer and Water Service Improvements

- A. *Sewers and Water Mains Required. All new development is required to connect to City water and sanitary sewer systems. Sanitary sewer and water system improvements shall be installed to serve each new development and to connect developments to existing mains in accordance with the adopted facility master plans and applicable Public Works Design Standards. Where streets are required to be stubbed to the edge of the subdivision, sewer and water system improvements and other utilities shall also be stubbed with the streets, except as may be waived by the City Engineer where alternate alignment(s) are provided.*

**Response:** As illustrated on Sheet C1.30 of Exhibit 2, the applicant proposes to connect to existing public water and sanitary lines in Molalla Avenue with private connections. No extensions of the public utilities are required to serve the development or nearby properties. This standard is met.

- B. *Sewer and Water Plan Approval. Development permits for sewer and water improvements shall not be issued until the City Engineer has approved all sanitary sewer and water plans in conformance with City standards.*

**Response:** As illustrated on Sheet C1.30 of Exhibit 2, water and sanitary sewer connections are proposed to the public lines in Molalla Avenue. Construction drawings will be submitted to the City's Engineering staff and Clackamas County Building staff for review prior to construction to ensure compliance with applicable design standards. This standard is met.

- C. *Over-Sizing. The City may require as a condition of development approval that sewer and water lines serving new development be sized to accommodate future development within the area as projected by the applicable facility master plans, and the City may authorize other cost-recovery or cost-sharing methods as provided under state law.*

**Response:** The proposed development is connecting to a public water main and to a public sanitary sewer line in Molalla Avenue. No new public water system or sanitary sewer system is proposed as part of this application as the existing systems are appropriately sized for the development. Additionally, based on the site's location, there are no nearby properties which would benefit from construction of an oversized public water main or sanitary sewer line. This standard does not apply.

- D. *Inadequate Facilities. Development permits may be restricted or rationed by the Planning Commission where a deficiency exists in the existing water or sewer system that cannot be rectified by the development and which, if not rectified, will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of domestic water and sewerage treatment systems. The City Engineer may require water booster pumps, sanitary sewer lift stations, and other critical facilities be installed with backup power.*

**Response:** No public water system or public sanitary sewer system deficiencies have been identified by City staff, so there is no need to withhold development permit approval. This standard does not apply.

### 17-3.6.050 Storm Drainage and Surface Water Management Facilities

- A. *General Provisions. The City shall issue a development permit only where adequate provisions for stormwater runoff have been made in conformance with the requirements of the current version of the Public Works Design Standards and Stormwater Master Plan.*

**Response:** As shown on Sheet C1.30 of Exhibit 2, stormwater will be directed into the proposed stormwater pond with overflow routed to the public stormwater line in Kennel Avenue via a 6" private storm line. The proposal includes a stormwater easement as shown on Sheet C1.30. The Preliminary Stormwater Report (Exhibit 3) demonstrates compliance with the applicable City stormwater management requirements. This standard is met.

- B. *Accommodation of Upstream Drainage. Culverts and other drainage facilities shall be large enough to accommodate existing and potential future runoff from the entire upstream drainage area, whether inside or outside the development. Such facilities shall be subject to review and approval by the City Engineer.*

**Response:** No culverts or other additions to existing public conveyance systems are necessary to accommodate development of the site or nearby properties. This standard does not apply.

- C. *Effect on Downstream Drainage. Where it is anticipated by the City Engineer that the additional runoff resulting from the development will overload an existing drainage facility, the City shall withhold approval of the development until provisions have been made for improvement of the potential condition or until provisions have been made for storage of additional runoff caused by the development in accordance with City standards.*

**Response:** As shown on Sheet C1.30 of Exhibit 2, stormwater from the roof will flow into downspouts and stormwater from new paving will flow into catch basins in the parking areas, after which it will be routed to the proposed stormwater pond west of the proposed building. Following detention, the stormwater will be routed to the City stormwater system in Kennel Avenue. The Preliminary Stormwater Report (Exhibit 3) demonstrates compliance with the applicable City stormwater management requirements. No negative effect on downstream drainage is anticipated as a result of this development as the development will provide onsite treatment and detention. This standard is met.

- D. *Over-Sizing. The City may require as a condition of development approval that sewer, water, or storm drainage systems serving new development be sized to accommodate future development within the area as projected by the applicable facility master plan, provided that the City may grant the developer credit toward any required system development charge for the same pursuant to the System Development Charge.*

**Response:** The proposed development is connecting to public water main and public sanitary sewer in Molalla Avenue, and a public storm drain line in Kennel Avenue. No new public water, sanitary sewer, or storm drainage system is proposed as part of this application as the existing systems are appropriately sized for the development. Additionally, based on the site's location, there are no nearby properties which would benefit from construction of an oversized public water main, sanitary sewer line, or storm drainage line. This standard does not apply.

- E. *Existing Watercourse. Where a proposed development is traversed by a watercourse, drainage way, channel, or stream, the City may require a stormwater easement or drainage right-of-way conforming substantially with the lines of such watercourse and such further width as will be adequate for conveyance and maintenance to protect the public health and safety.*

**Response:** As shown on Sheets C1.10-C1.30 in Exhibit 2, the site is not traversed by an existing watercourse, drainage way, channel, or stream. This standard does not apply.

#### 17-3.6.060 Utilities

*The following standards apply to new development where extension of electric power, gas, or communication lines is required:*

- A. *General Provision. The developer of a property is responsible for coordinating the development plan with the applicable utility providers and paying for the extension and installation of utilities not otherwise available to the subject property.*

**Response:** The applicant will coordinate with utility companies as necessary to secure provision of utility service to the site. All required utility lines are all available near the site for the applicant to make appropriate connections. This standard is met.

**B. Underground Utilities.**

1. *General Requirement. The requirements of the utility service provider shall be met. All utility lines in new subdivisions, including, but not limited to, those required for electric, communication, and lighting, and related facilities, shall be placed underground, except where the City Engineer determines that placing utilities underground would adversely impact adjacent land uses. The Planning Official may require screening and buffering of above ground facilities to protect the public health, safety, or welfare.*

**Response:** As shown on Sheet C1.30 of Exhibit 2, proposed utilities are underground. This standard is met.

2. *Subdivisions. In order to facilitate underground placement of utilities, the following additional standards apply to all new subdivisions:*
  - a. *The developer shall make all necessary arrangements with the serving utility to provide the underground services. Care shall be taken to ensure that no aboveground equipment obstructs vision clearance areas for vehicular traffic, per Chapter 17-3.3 Access and Circulation.*
  - b. *The City Engineer reserves the right to approve the location of all surface-mounted facilities.*
  - c. *All underground utilities installed in streets must be constructed and approved by the applicable utility provider prior to the surfacing of the streets.*
  - d. *Stubs for service connections shall be long enough to avoid disturbing the street improvements when service connections are made.*

**Response:** No subdivisions are proposed within this application. This standard does not apply.

- C. *Exception to Undergrounding Requirement. The City Engineer may grant exceptions to the undergrounding standard where existing physical constraints, such as geologic conditions, streams, or existing development conditions make underground placement impractical.*

**Response:** As shown on Sheet C1.30 of Exhibit 2, the proposed utility lines are underground. The applicant is not seeking an exception to this provision. This standard does not apply.

**17-3.6.070 Easements**

- A. *Provision. The developer shall make arrangements with the City and applicable utility providers for each utility franchise for the provision and dedication of utility easements necessary to provide full services to the development.*
- B. *Standard. Utility easements shall conform to the requirements of the utility service provider. All other easements shall conform to the City of Molalla Public Works Design Standards.*
- C. *Recordation. All easements for sewers, storm drainage and water quality facilities, water mains, electric lines, or other utilities shall be recorded and referenced on a survey or final plat, as applicable. See Chapter 17-4.2 Site Design Review, and Chapter 17-4.3 Land Divisions and Property Line Adjustments.*

**Response:** As shown on Sheet C1.30 of Exhibit 2, the applicant proposes to provide a new 10' wide public utility easement (PUE) adjacent to both Kennel and Molalla Avenues, a new 10' wide electrical easement north of the existing primary Station 82 building, and a new storm easement ranging from 10' to 25' wide on lot 9300. No other utility easements are proposed or necessary to serve the project. The easements will be recorded in Clackamas County deed records as required. This standard is met.

17-3.6.080 Construction Plan Approval

No development, including sanitary sewers, water, streets, parking areas, buildings, or other development, shall commence without plans having been approved by the City of Molalla Public Works Department and permits issued. Permit fees are required to defray the cost and expenses incurred by the City for construction and other services in connection with the improvement. Permit fees are as set by City Council resolution.

**Response:** The applicant will begin improvements after appropriate permits are issued. Compliance with this provision will be demonstrated during permitting. This standard is met.

17-3.6.090 Facility Installation

A. *Conformance Required. Improvements installed by the developer, either as a requirement of these regulations or at the developer's option, shall conform to the requirements of this chapter, approved construction plans, and to improvement standards and specifications adopted by the City.*

**Response:** The applicant's civil engineer has designed improvements in accordance with applicable standards, as will be confirmed by City Engineering staff as part of the permitting phase. This standard is met.

B. *Adopted Installation Standards. The City of Molalla has adopted Public Works Design Standards for public improvements and private utility installation within the public right-of-way.*

**Response:** The applicant's civil engineer has designed utility connections in accordance with the City of Molalla Standard Specifications for Public Works Construction, as will be confirmed by City Engineering staff as part of the permitting phase. No public improvements are proposed or required for this project. This standard is met.

C. *Commencement. Work in a public right-of-way shall not begin until all applicable agency permits have been approved and issued.*

**Response:** The applicant will obtain required permits and provide sufficient notice to staff as specified by City code prior to commencing work in the public right-of-way. This standard is met.

D. *Resumption. If work is discontinued for more than six months, it shall not be resumed until the Public Works Director is notified in writing and grants approval of an extension.*

**Response:** If work in the public right-of-way pauses for more than six (6) months, the applicant will provide sufficient notice to staff as specified by City code. This standard is met.

E. *City Inspection. Improvements shall be constructed under the inspection of the City Engineer. The City Engineer may approve minor changes in typical sections and details if unusual conditions arising during construction warrant such changes in the public interest, except that substantive changes to the approved design shall be subject to review under Chapter 17-4.5 Modifications to Approved Plans and Conditions of Approval. Any survey monuments that are disturbed before all improvements are completed by the developer or subdivider shall be replaced at the developer or subdivider's expense prior to final acceptance of the improvements.*

**Response:** The applicant intends all improvements to be constructed to the satisfaction of the City. The applicant understands the City's authority to request changes in typical sections and details if unusual conditions arise during construction which warrant changes in the public interest. The applicant's contractor will need to address any disturbed survey monuments in accordance with state law. This standard is met.

- F. *Engineer's Certification and As-Built Plans.* In accordance with the current version of the Public Works Design Standards, a registered civil engineer shall provide written certification in a form required by the City that all improvements, workmanship, and materials meet current and standard engineering and construction practices, conform to approved plans and conditions of approval, and are of high grade, prior to City's acceptance of the public improvements, or any portion thereof, for operation and maintenance. The developer's engineer shall also provide two sets of "as-built" plans, one paper set and one electronic set for permanent filing with the City. If required by the City, the developer or subdivider shall provide a warranty bond pursuant to Section 17-3.6.100.

**Response:** The applicant will obtain required permits and coordinate with staff for certifications. As no public improvements are proposed, no record drawings are anticipated. This will be verified as part of closeout of the utility connections. This standard is met.

#### 17-3.6.100 Performance Guarantee and Warranty

- A. *Performance Guarantee Required.* The City at its discretion may approve a final plat or building permit when it determines that all of the public improvements required for the site development or land division, or phase thereof, are complete and the applicant has an acceptable assurance for the balance of said improvements. The applicant shall provide a performance and payment bond in accordance with the current version of the Public Works Design Standards.
- B. *Determination of Sum.* The assurance of performance shall be for a sum determined by the City Engineer as required to cover the cost of the improvements and repairs, including related engineering and incidental expenses, plus reasonable inflationary costs. The assurance shall not be less than 125 percent of the estimated improvement costs.
- C. *Itemized Improvement Estimate.* The applicant shall furnish to the City an itemized improvement estimate, certified by a registered civil engineer, to assist the City in calculating the amount of the performance assurance.
- D. *Agreement.* A written agreement between the City and applicant shall be signed recorded. The agreement may include a provision for the construction of the improvements in stages and for the extension of time under specific conditions. The agreement shall contain all of the following:
1. The period within which all required improvements and repairs shall be completed;
  2. A provision that if work is not completed within the period specified, the City may complete the work and recover the full cost and expenses from the applicant;
  3. The required improvement fees and deposits.
- E. *When Applicant Fails to Perform.* In the event the applicant fails to carry out all provisions of the agreement and the City has un-reimbursed costs or expenses resulting from such failure, the City shall call on the bond, cash deposit, or letter of credit for reimbursement.
- F. *Termination of Performance Guarantee.* The applicant shall not cause termination, nor allow expiration, of the guarantee without first securing written authorization from the City.
- G. *Warranty Bond.* A warranty bond good for two years is required on all public improvements and landscaping when installed in the public right-of-way. The warranty bond shall equal 120 percent of the total cost of improvements and begin upon acceptance of said improvements by the City.

**Response:** No public improvements are proposed. These standards do not apply.

#### Chapter 17-4.1 General Review Procedures

##### 17-4.1.010 Purpose and Applicability

- B. *Applicability of Review Procedures.* All land use and development permit applications and approvals, except building permits, shall be decided by using the procedures contained in this

chapter. The procedure “type” assigned to each application governs the decision-making process for that permit or approval. There are four types of permit and approval procedures as described in subsections B.1 through 4. Table 17-4.1.010 lists the City’s land use and development approvals and corresponding review procedure(s).

1. *Type I Procedure (Staff Review—Zoning Checklist).* Type I decisions are made by the Planning Official, or his or her designee, without public notice and without a public hearing. A Type I procedure is used in applying City standards and criteria that do not require the use of discretion (i.e., there are clear and objective standards).
2. *Type II Procedure (Administrative or Staff Review with Notice).* Type II decisions are made by the Planning Official, with public notice and an opportunity for appeal to the Planning Commission. Alternatively the Planning Official may refer a Type II application to the Planning Commission for its review and decision in a public meeting.
3. *Type III Procedure (Quasi-Judicial Review—Public Hearing).* Type III decisions are made by the Planning Commission after a public hearing, with an opportunity for appeal to the City Council; or in the case of a Quasi-Judicial zone change (e.g., a change in zoning on one property to comply with the Comprehensive Plan), a Type III decision is made by the City Council on recommendation of the Planning Commission. Quasi-Judicial decisions involve discretion but implement established policy.
4. *Type IV Procedure (Legislative Review).* The Type IV procedure applies to the creation, revision, or large-scale implementation of public policy (e.g., adoption of regulations, zone changes, annexation, and comprehensive plan amendments). Type IV reviews are considered by the Planning Commission, which makes a recommendation to City Council. City Council makes the final decision on a legislative proposal through the enactment of an ordinance.

Approval	Review Procedures	Applicable Regulations
Site Design Review	Type II or III	Chapter 17-4.2
Variance	Type III	Chapter 17-4.7

**Response:** This application seeks approval for Site Design Review (Type III) for a new 4,800 SF accessory storage building for Molalla Fire Station 82 and associated landscaping, utility connections, and storm pond. The applicant also requests Variance approval for alternate articulation methods as part of this application. This standard is applicable.

*17-4.1.040 Type III Procedure (Quasi-Judicial Review – Public hearing)*

*Type III decisions are made by the Planning Commission after a public hearing, with an opportunity for appeal to the City Council.*

**A. Application Requirements.**

1. *Application Forms.* Applications requiring Quasi-Judicial Review shall be made on forms provided by the Planning Official.
2. *Submittal Information.* The Planning Official shall advise the applicant on application submittal requirements. At a minimum, the application shall include all of the following information:
  - a. *The information requested on the application form;*
  - b. *Plans and exhibits required for the specific approval(s) being sought;*
  - c. *A written statement or letter explaining how the application satisfies each and all of the relevant criteria and standards in sufficient detail;*

- d. Information demonstrating compliance with prior decision(s) and conditions of approval for the subject site, as applicable; and
- e. The required fee.
- f. Comments, if obtained from neighborhood contact per Section 17-4.1.070.

**Response:** The application package contains the requested information including the required application forms, plans, exhibits, compliance narrative, and required fee for the Site Design Review and Variance requests. Per Section 17-4.1.070, neighborhood contact is not required as the proposed 'Area of Impact' is not larger than one (1) acre and no zone change is proposed as part of this application. This standard is met.

### **Chapter 17-4.2 Site Design Review**

#### *17-4.2.030 Review Procedure*

*Site Design Review shall be conducted using the Type II procedure in Section 17-4.1.030, except that proposals exceeding any one of the thresholds below shall be reviewed using the Type III procedure in Section 17-4.1.040:*

- A. *The proposed use's estimated vehicle trip generation exceeds 100 average daily trips, based on the latest edition of the Institute of Transportation Engineers (ITE) Manual;*
- B. *The use exceeds 5,000 square feet of gross leasable floor area; or the project involves more than one acre total site area;*
- C. *The proposal involves a Conditional Use (new or expanded);*
- D. *The proposal involves a variance under Chapter 17-4.7;*
- E. *The proposal involves expansion of a nonconforming use; or*
- F. *The Planning Official determines that, due to the nature of the proposal, a public hearing is the most effective way to solicit public input in reviewing the application.*

**Response:** The proposal is for the development of a new, approximately 4,800 SF accessory building to serve as an annex to the existing fire station on an approximately 2.3-acre site, which falls under subparagraph B. Type III Site Design review is required. This standard is applicable.

#### *17-4.2.040 Application Submission Requirements*

*All of the following information is required for Site Design Review application submittal, except where the Planning Official and the City Engineer determines that some information is not pertinent and therefore is not required.*

- A. *General Submission Requirements.*
  - 1. *Information required for Type II or Type III review, as applicable (see Chapter 17-4.1).*
  - 2. *Public Facilities and Services Impact Study. The impact study shall quantify and assess the effect of the development on public facilities and services. The City shall advise as to the scope of the study. The study shall address, at a minimum, the transportation system, including required improvements for vehicles and pedestrians; the drainage system; the parks system; water system; and sewer system. For each system and type of impact, the study shall propose improvements necessary to meet City requirements. The City may require a Traffic Impact Analysis pursuant to Section 17-3.6.020.A(4).*

**Response:** The applicant has provided a narrative (this document), plans (Exhibit 2), preliminary stormwater report (Exhibit 3), and transportation analysis letter (Exhibit 4), to afford the Planning Commission sufficient information on which to evaluate the application. The Public Facilities and Services Impact Study is contained in the Introduction to this document. This standard is met.

B. *Site Design Review Information. In addition to the general submission requirements, an applicant for Site Design Review shall provide the following information, as deemed applicable by the Planning Official. The Planning Official may request any information that he or she needs to review the proposal and prepare a complete staff report and recommendation to the approval body.*

1. *Site Analysis Map. The site analysis map shall contain all the following information, as the Planning Official deems applicable:*

[detailed provisions omitted for brevity]

**Response:** The site plans (Exhibit 2, Sheets C1.10-C1.30) and landscape plan (Exhibit 2, Sheet L1.10) provide the requested information. This standard is met.

2. *Proposed Site Plan. The site plan shall contain all the following information:*

[detailed provisions omitted for brevity]

**Response:** The site plans (Exhibit 2, Sheets C1.10-C1.30) and landscape plan (Exhibit 2, Sheet L1.10), provide the requested information. This standard is met.

3. *Architectural Drawings. Architectural drawings shall include, as applicable:*

a. *Building elevations with dimensions;*

b. *Building materials, colors, and type; and*

c. *Name and contact information of the architect or designer.*

**Response:** The architectural drawings in Exhibit 2 provide the requested information. This standard is met.

4. *Preliminary Grading Plan. A preliminary grading plan prepared by a registered engineer shall be required for development sites one-half acre or larger, or where otherwise required by the City. The preliminary grading plan shall show the location and extent to which grading will take place, indicating general changes to contour lines, slope ratios, slope stabilization proposals, and location and height of retaining walls, if proposed. Surface water detention and treatment plans may also be required, in accordance with Section 17-3.6.040.*

**Response:** The preliminary grading plan (Exhibit 2, Sheet C1.20) provides the requested information. This standard is met.

5. *Landscape Plan. Where a landscape plan is required, it shall show the following, pursuant to Chapter 17-3.4:*

[detailed provisions omitted for brevity]

**Response:** The landscape plan (Exhibit 2, Sheet L1.10) provides the requested information. This standard is met.

6. *Deed Restrictions. Copies of all existing and proposed restrictions or covenants, including those for roadway access control.*

**Response:** The applicant is unaware of any existing restrictions or covenants, and no new restrictions or covenants are proposed. This standard does not apply.

7. *Narrative. Letter or narrative report documenting compliance with the applicable approval criteria contained in Section 17-4.2.050.*

**Response:** The applicant has responded to all applicable criteria and standards within this application narrative. This standard is met.

8. *Traffic Impact Analysis, when required by Section 17-3.6.020.A(4).*

**Response:** Per the applicant’s responses to Section 17-3.6.020.A(4), a Transportation Impact Analysis (TIA) is not required as explained by the Transportation Analysis Letter (TAL) in Exhibit 4. This standard is met.

- 9. *Other information determined by the Planning Official. The City may require studies or exhibits prepared by qualified professionals to address specific site features or project impacts (e.g., traffic, noise, environmental features, natural hazards, etc.), as necessary to determine a proposal’s conformance with this Code.*

**Response:** Per comments from City staff in the Pre-Application Conference Notes, no additional studies or exhibits have been requested from the Planning Official. This criterion is met.

*17-4.2.050 Approval Criteria*

*An application for Site Design Review shall be approved if the proposal meets all of the following criteria. The Planning Official, in approving the application, may impose reasonable conditions of approval, consistent with the applicable criteria.*

- A. *The application is complete, in accordance with Section 17-4.2.040;*

**Response:** As explained in the applicant’s responses to Section 17-4.2.040, the applicant has provided all requirements for a Type III Site Design Review. This criterion is met.

- B. *The application complies with all of the applicable provisions of the underlying Zoning District (Division II), including, but not limited to, building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards;*

**Response:** As explained in the applicant’s responses to Chapter 17-2.2, the proposal complies with the applicable provisions of the PSP zoning district. This criterion is met.

- C. *The proposal includes required upgrades, if any, to existing development that does not comply with the applicable zoning district standards, pursuant to Chapter 17-1.4 Nonconforming Situations;*

**Response:** No required upgrades were identified pursuant to Chapter 17-1.4. This standard does not apply.

- D. *The proposal complies with all of the Development and Design Standards of Division III, as applicable, including, but not limited to:*

1. *Chapter 17-3.3 Access and Circulation,*
2. *Chapter 17-3.4 Landscaping, Fences and Walls, Outdoor Lighting,*
3. *Chapter 17-3.5 Parking and Loading,*
4. *Chapter 17-3.6 Public Facilities, and*
5. *Chapter 17-3.7 Signs;*

**Response:** As explained in the applicant’s responses to the listed chapters, the proposal complies with the applicable development and design standards. This criterion is met.

- E. *For non-residential uses, all adverse impacts to adjacent properties, such as light, glare, noise, odor, vibration, smoke, dust, or visual impact, are avoided; or where impacts cannot be avoided, they are minimized; and*

**Response:** No new lighting is proposed and visual impact to adjacent properties has been minimized through landscaping and building design. Other adverse impacts including noise, odor, smoke, vibration, and dust are not expected from the proposed development.

F. *The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.*

*Note: Compliance with other City codes and requirements, though not applicable land use criteria, may be required prior to issuance of building permits.*

**Response:** There are no existing conditions of approval for the site or use that require compliance for this proposal. This criterion is met.

#### 17-4.2.060 Assurances

*Public improvement required as part of a Site Design Review approval shall be subject to the performance guarantee and warranty bond provisions of Section 17-3.6.090, as applicable.*

**Response:** As explained in the applicant’s responses to Section 17-3.6.100, no public improvements are proposed. This standard does not apply.

### **Chapter 17-4.7 Adjustments and Variances**

#### 17-4.7.010 Purpose

*Chapter 17-4.7 provides standards and procedures for adjustments and variances, which are modifications to development standards that are not otherwise permitted elsewhere in this Code.*

**Response:** In accordance with this Chapter, the applicant is requesting a Variance due to the unique needs of this essential facility construction. The applicant has provided evidence below responding to applicable approval criteria for the requested Variance to Section 17-3.2.040.E.1 – Articulation.

#### 17-4.7.020 Intent

*Adjustments are variances that are intended to provide relief from code standards in specific situations. Both procedures are intended to ensure that the resulting development is compatible with adjacent properties and is consistent with the intent of the Code.*

- A. *Adjustments. Adjustments provide relief from specific code provisions when a code provision has the unintended effect of preventing reasonable development in conformance with all other code requirements. Adjustments are allowed in limited situations pursuant to Section 17-4.7.030.*
- B. *Variances. Variances provide greater flexibility to code standards than adjustments, where the physical characteristics of a site or its surroundings prevent reasonable development in compliance with a code standard.*

**Response:** In this case, a needed public facility (Fire Station storage building for emergency apparatus) must be designed to meet essential facility construction standards and specific security needs that are not typical for private commercial, industrial, or other buildings in the community. In the context of an emergency facility, “reasonable development” necessarily includes the building and site design features and essential facility construction type necessary for its Fire Station functions. The proposed building design attempts to come as close as possible to compliance with code standards while also providing the required building and site design.

#### 17-4.7.030 Adjustments

*Adjustments are minor modifications to code standards that are intended to provide reasonable flexibility for planned land uses and development. Adjustments are subject to the following standards and procedures. Permitted uses, as provided in Division II, shall not be adjusted.*

- A. *Applicability. The Planning Official or Planning Commission, through a Type II procedure, may adjust the following standards:*
  1. *Setbacks. Up to a 20 percent reduction to a minimum setback.*
  2. *Lot Coverage. Up to a 20 percent increase to the maximum lot coverage.*

3. *Lot Dimensions. Up to a 20 percent decrease to a minimum lot dimension.*
4. *Lot Area. Up to a 20 percent decrease in minimum lot area.*
5. *Other Dimensional Standards. Up to a 20 percent increase or decrease in a quantitative (numerical) standard not listed above. This option is limited to standards in Division II (Tables 17-2.2.040.D and 17-2.2.040.E, and Chapter 17-2.3 Special Use Standards) and Division III; it does not include building code requirements, engineering design standards, public safety standards, or standards implementing state or federal requirements, as determined by the Planning Official.*

**Response:** The applicant is seeking approval of a modification to the requirement for articulation on building elevations that orient to a street by more than 20%, which does not qualify for an Adjustment. A Variance is required. See Variances section.

#### 17-4.7.040 Variances

- A. *Applicability. A Variance is similar to an Adjustment, but does not otherwise meet the criteria under Section 17-4.7.030.*

**Response:** The applicant is seeking approval of a Variance to Section 17-3.2.040.E.1 – Articulation to allow alternate articulation methods, namely utilizing changes in materials, colors, and textures, with additional trim and inset painted community murals, rather than a building offset. The applicant’s proposal exceeds a 20% deviation from a standard in Division III of the Development Code so Variance is the appropriate mechanism for seeking this flexibility.

- B. *Approval Criteria. The Planning Commission through a Type III procedure may approve a Variance upon finding that it meets all of the following criteria:*

1. *The Variance is necessary because the subject Code provision does not account for special or unique physical circumstances of the subject site, existing development patterns, or adjacent land uses. A legal lot determination may be sufficient evidence of a hardship for purposes of approving a variance;*

**Response:** As stated, the applicant is seeking approval of a Variance to Section 17-3.2.040.E.1 – Articulation, to allow alternate articulation methods, namely utilizing changes in materials, colors, and textures, with additional trim and inset painted community murals, rather than a building offset.

The Code provision for which this Variance is requested was intended to apply to traditional commercial development along pedestrian walkways. The specific code standard (Section 17-3.2.040.E.1) requires that a non-residential building façade, oriented to a street, should have breaks in the wall plane every 30' to provide building articulation of at least 24" depth offsets. Per the purpose statement (Section 17-3.2.040), this is intended to apply to shopping centers, office complexes, mixed-use developments, and institutional campuses in order to create and maintain a built commercial environment that is conducive to pedestrian accessibility and reducing short automobile trips, while providing civic space for customers, and human scale development.

This code section doesn’t account for the circumstances of the site being an essential public facility<sup>1</sup> with unique building and site design requirements. The seismic requirements of an essential facility demand regularity and efficiency in the building’s gravity and lateral structural systems, which is dependent on rectilinear building forms and which hampers utilizing building offsets to provide articulation. Additionally, the proposed accessory storage building will be set back over 100' from the street lot line and will be located behind security fencing and an existing

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<sup>1</sup> The Oregon Structural Specialty Code classifies Fire Stations as essential public facilities.

residential structure. Only a portion of the proposed building will be visible from the street and pedestrian access will not be permitted to this portion of the essential public facility for security reasons.

Additionally, the code provision, which applies to non-residential structures, doesn't account for existing development and adjacent land uses surrounding the proposed accessory storage building. This portion of the site where the accessory structure is proposed has residential properties and structures to the northeast, north, west, and southwest. Due to the proximity of the surrounding residences, the setback from the street, and existing fencing, the design of the proposed accessory building has stylistic elements in common with residential accessory structures as well.

The proposed building is needed at this location for the safe and successful operation of the fire station and supports the Fire District's ability to serve the community. The applicant is therefore seeking Variance approval in recognition of these unique circumstances. This standard is met.

2. *The Variance is the minimum necessary to address the special or unique physical circumstances related to the subject site;*

**Response:** The design team took measures to ensure the proposed Variance is the minimum necessary to meet the special building and site functions. Deviating from the articulation standard is a relatively minor variation from the code provisions that still provides human scale and façade articulation through change of materials, textures, colors, and depths. Approving the Variance will enable efficient and secure use of the site for a permitted use at a central location that benefits the surrounding community and City as a whole. Other than this articulation standard, applicable development standards are proposed to be met with this project. This standard is met.

3. *The need for the Variance is not self-imposed by the applicant or property owner. (For example, the Variance request does not arise as a result of a property line adjustment or land division approval previously granted to the applicant);*

**Response:** The Variance is not self-imposed by prior actions of the Owner but rather arises from the unique building and site design needs of a Fire Station facility and from the requirements for an essential emergency service to remain functional following a seismic event. These conditions would be present regardless of the location of a Fire Station and are beyond the applicant's control due to the obligation to protect the public safety by providing fire protection services. This standard is met.

4. *The Variance does not conflict with other applicable City policies or other applicable regulations;*

**Response:** The Variance applies to facade articulation, which does not conflict with other applicable City policies or applicable regulations. In fact, granting the Variance would allow the fire facility to remain consistent with the Comprehensive Plan which states under the Police and Fire Protection section that the Fire District is responsible for maintaining and upgrading fire-fighting apparatus and making necessary capital improvements. Granting the Variance would also allow consistency with the following Comprehensive Plan policies:

*Policy 3. The City shall coordinate with the Molalla fire and police departments to ensure residents have a safe environment in which to live.*

*Policy 84. The Fire District coverage includes all areas within the existing Urban Growth Boundary.*

*Policy 85. Provide fire protection consistent with the health, welfare, and safety of Molalla citizens.*

This standard is met.

5. *The Variance will result in no foreseeable harm to adjacent property owners or the public; and*

**Response:** The applicant is not seeking approval for a prohibited use and is not proposing a Variance that would increase impacts on neighboring uses or the public. The proposed Variance to the required facade articulation will not create a conflict with adjacent uses as special consideration was given to ensure the purpose of the code provision is still met through change of materials, textures, colors, and depths as shown on Sheet A2.10. These design considerations minimize potential conflict with or harm to adjacent uses. This standard is met.

6. *All applicable building code requirements and engineering design standards shall be met.*

**Response:** As demonstrated throughout this narrative and the attached exhibits, applicable building code and engineering design standards were incorporated into the project's design. Compliance with these requirements and standards will be further confirmed through the permitting process. This standard is met.

#### *17-4.7.050 Expiration*

*Approvals granted under Chapter 17-4.7 shall expire if not acted upon by the property owner within one year of the City approving the variance. Where the owner has applied for a building permit or final plat, has made site improvements consistent with an approved development plan (e.g., Site Design Review or preliminary subdivision plan), or provides other evidence of working in good faith toward completing the project, the Planning Official may extend an approval accordingly.*

**Response:** The applicant will obtain required permits and initiate site improvements within the specified time period. This standard is met.

## **Molalla Municipal Code Title 21 – Additional Regulations**

### **Chapter 21.90 Trees**

#### *21.90.020 Tree retention*

- A. *A group of trees that is, on average, 8 feet tall or greater shall not be reduced by more than 30%.*
- B. *This section does not apply to hazardous trees or when there is no practicable alternative to removing the trees, as determined by the Planning Director in the Director's sole discretion. A "hazardous tree" is one that has foreseeable potential for falling and causing property damage or personal injury. Within commercial and industrial zones, an arborist shall provide a written statement verifying the hazard. In residential zones, the property owner shall provide a written statement and photographs verifying the hazard.*
- C. *Any tree that is removed shall be replanted on the same lot within 1 year unless the Planning Director determines that an adequate number of trees remain on the property.*
- D. *All new development shall incorporate trees within the development.*
- E. *When a protected tree lies within an area where development is scheduled to occur and no alternative exists to removing the tree, a new tree must be planted for every tree that is removed.*

**Response:** An existing tree is located within the central portion of the footprint of the proposed building, so the applicant proposes to remove the tree as depicted on Sheet C1.10 of Exhibit 2. There is no practicable alternative to removing the tree since that would require shrinking the building to a size that no longer serves the function of storing a sufficient number of emergency vehicles. Therefore, with the approval of the Planning Director, per standard B this section would not be applicable.

The applicant does not propose to plant a new tree within the limits of disturbance as the proposed new landscape area immediately surrounds the new building so a new tree would be too close to the building. A tree would also be inappropriate in the stormwater facility as its roots would interfere with the operation of the stormwater facility and its underground piping. The applicant requests under standard C that the Planning Director determine that the Fire District property contains an adequate number of trees (particularly along its perimeter) as planting new trees would hamper the flexibility for future site operations and potential expansion as the District's emergency response needs may expand in the future.

*21.90.030 Permit required*

*A permit shall be obtained for removal of any tree that stands 8 feet tall or higher. A fee shall be paid as approved by the City Council.*

**Response:** The tree proposed to be removed exceeds 8' so the applicant will obtain a tree permit as required.

#### **IV. CONCLUSION**

Based on the information presented and discussed in this narrative and the attached supporting plans and documentation, this application meets applicable standards necessary for land use approval. The proposed development complies with applicable standards of the Molalla Development Code. The applicant respectfully requests approval by the City.



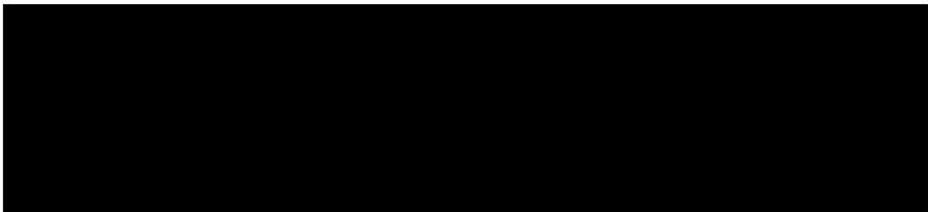
**PRELIMINARY  
STORMWATER  
REPORT**

**To**  
City of Molalla

**For**  
Molalla Fire – Station 82  
320 N Molalla Avenue,  
Molalla OR 97038

**Dated**  
April 24, 2026

**Project Number**  
2250404.00





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## I. PROJECT OVERVIEW AND DESCRIPTION

The proposed Molalla Fire Station 82 project is located on Molalla Avenue, south of the intersection of Molalla Avenue and West Heintz Street in Molalla, Oregon. The project consists of a new building, paved vehicle maneuvering area, utility connections and a stormwater pond.

### Existing Conditions

This project involves tax lots 8400, 8500, 8600 and 9300 (map number 52E09CB). The lots contain the main fire station building and several accessory buildings, a combination of gravel, asphalt and concrete surfaces, and landscaping areas. The proposed building will be placed on tax lot 8600 and the proposed stormwater pond will be located on tax lot 9300.

Stormwater from the property currently sheet flows to catch basins and is discharged to the public storm systems in Kennel Avenue and Molalla Avenue.

Per the Natural Resources Conservation Service Web Soil Survey, the site is entirely hydrologic soil group C (Sawtell silt loam) with slopes from 0-8%. See Appendix A for full Web Soil Survey results. As indicated in the Geotechnical Report (see Appendix D), depth to groundwater is approximately 8.5 feet below ground surface. Due to the shallow groundwater table and slow infiltration rate of the near-surface soil, infiltration is not feasible.



Figure 1: Vicinity Map

## Proposed Improvements

The proposed site consists of a new building (for use by Molalla Fire) positioned on tax lot 8600 with vehicle maneuvering area to the south on tax lots 8500 and 8600. Site upgrades also include landscaping of the proposed stormwater facility, and supporting utility upgrades.

Stormwater from new impervious areas (building roof and new paved area) will be captured via downspout (roof water) or catch basins (paved areas) and be conveyed via storm pipe to a storm pond located on tax lot 9400.

The entire property (tax lots 8400, 8500, 8600 and 9300) is 100,126 ft<sup>2</sup>, but for the purposes of this project areas will be based on a “project area” (limits of disturbance) equal to 18,470 ft<sup>2</sup>.

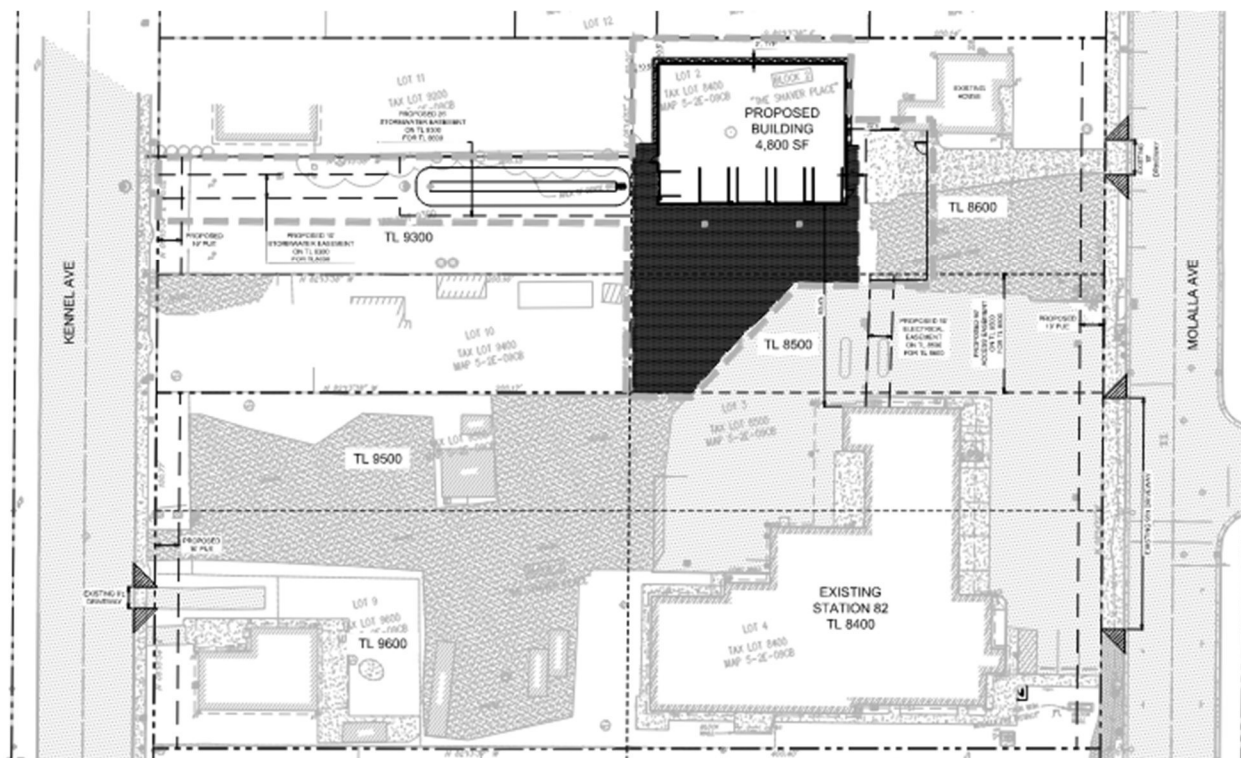


Figure 2: Site Plan

## II. BASIS OF DESIGN

The Basis of Design for Stormwater Quality and Flow Control, as determined by the 2020 Molalla Standard Specifications for Public Works Construction, Section 3, is as follows:

Per sections 3.4.2 and 3.5.1, because the project will establish or increase more than 5,000 ft<sup>2</sup> of pollution-generating impervious surface area, on-site water quantity and water quality facilities will be required.

Water quantity on-site detention/retention facilities must be designed so that predevelopment runoff rates are not exceeded by post-development conditions based upon a 2- through 25-year, 24 hour return storm. Facilities must be designed to include inlet energy dissipation and a sediment forebay with respect to anticipated flow rate. Forebay must have adequate means of maintenance and be able to operate under full sediment accumulation.

Water quality facilities must be designed to remove pollutants of concern including suspended solids (TSS), heavy metals, nutrients, bacteria and viruses, and organics. Methods of removal include sedimentation, filtration, plant uptake, ion exchange, absorption, and bacterial decomposition. Water quality facilities must be designed to capture and treat 80 percent of the average annual runoff volume and remove 70 percent of total suspended solids. Additionally, facilities must be designed for a dry weather storm event totaling 0.36 inches of precipitation in four hours with an average storm return period of 96 hours. Water quality treatment methods and criteria are further detailed in section 3.5.4.

Per section 3.3, hydrologic analyses must be conducted using either the Rational Method (for sub-basin areas not exceeding 25 acres) or the unit hydrograph method (primarily the Santa Barbara Urban Hydrograph) using the NRCS type 1A rainfall distribution.

Conveyance must be designed for the 25-year design storm per Section 3.2.6 using either the Rational Method of SBUH.

### III. ANALYSIS

#### Methodology

Per the Geotechnical Report (Appendix D), infiltration is not feasible due to the depth to groundwater and slow infiltration rate of the near-surface soil. Section 3.1.2 of the Molalla Standards encourages the use of Low Impact Development (LID) facilities to the maximum extent feasible, so a pond is proposed to provide water quality and detention for the new impervious areas.

Table 1: Area Summary						
Cover Type	Pre-Development Conditions			Post Development Conditions		
	Area (ft <sup>2</sup> )	Hydrologic Soil Group	CN	Area (ft <sup>2</sup> )	Hydrologic Soil Group	CN
Open Space – Good Condition*	14,750	C	86*	6,675	C	74**
Paved Parking Lots, Building Roof, etc	3,720	C	98	11,795	C	98

Areas are based on a “project area” (18,470 ft<sup>2</sup>) as defined on the basin maps in Appendix B

Curve numbers are based on NRCS CN tables for hydrologic soil group C.

\*Full developed urban area, poor condition (grass cover < 50%)

\*\* Full developed urban area, good condition (grass cover > 75%)

Table 2: Precipitation Rates	
Storm Event	24-HR Precipitation (inches)
WQ Storm	0.36 (4 hours)
2-year	2.50
5-year	3.10
10-year	3.45
25-year	3.90
100-year	4.50

## Water Quality

Water quality will be provided for the site using a pond sized to treat the City of Molalla water quality storm of 0.36 inches over 4 hours. Per Section 3.3.4 of the City of Molalla Standard Specifications for Public Works Construction (SSPWS), the required water quality volume and flow are calculated as follows:

Water quality storm = 0.36 inches/4 hours, return period = 96 hours

*Water Quality Volume,*

$$WQV = \frac{(0.36 \text{ in}) * (\text{Area ft}^2)}{12 \text{ in/ft}} \quad (\text{Area} = \text{total proposed impervious area})$$

$$WQV = (0.36 \text{ in}) * (12,696 \text{ ft}^2) / (12 \text{ in/ft})$$

$$WQV = 381 \text{ ft}^3$$

Table 3: Pond Storage Data		
Contour	Area (ft <sup>2</sup> )	Volume (ft <sup>3</sup> )
364.5	310	0
365.0	565	438
366.0	1,115	840
367.0	1,725	1,420

The water quality depth (dead storage) is approximately 0.4' (elevation 364.9'). Detention volume sizing will therefore begin at elevation 365.0'.

## Water Quantity & Flow Control

Water quantity and flow control will be addressed using a pond with a flow control manhole. The pond is sized using the SBUH method and the Hydraflow Hydrographs extension from Autodesk Civil 3D. The pond and flow control manhole are designed to capture runoff and detain such that the post-development runoff rates do not exceed the pre-development runoff rates for the 2-year, 10-year, and 25-year 24-hour storm events.

The pre-developed condition of the site is assumed to be the site as it exists today, which within the project area is mostly landscaped with an existing building, and some asphalt, concrete and gravel. Per section 3.3.3 of the City of Molalla SSPWS, curve numbers are based on soil type and land use as given by the National Resource Conservation Service (NRCS) tables. Per Appendix A, the site has a hydrological soil group of C. See Table 4 for pre and post development curve number values based on this soil type.

Table 4: Pre vs. Post Construction Flow Rates							
Peak Flow Rate (CFS) for a 24-hr Storm						Time of Concentration (minutes)	
2-year		10-year		25-year			
Pre	Post	Pre	Post	Pre	Post	Pre	Post
0.133	0.113	0.220	0.194	0.263	0.228	5.0	5.0

### Conveyance

Per section 3.2.6 of the Molalla Standard Specifications for Public Works Construction, conveyance systems must be designed to convey and contain at least the peak runoff for the 25-year design storm. Detailed conveyance calculations can be found in Appendix C.

#### **IV. ENGINEERING CONCLUSIONS**

Based on compliance with the City of Molalla Standard Specifications for Public Works Construction (2020), water quality and detention will be provided via a pond with a flow control structure.

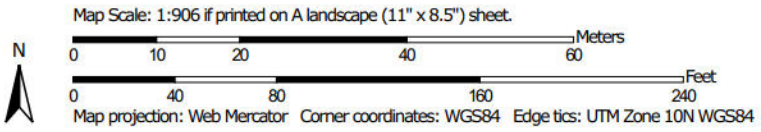
APPENDIX A

**WEB SOIL SURVEY**

Hydrologic Soil Group—Clackamas County Area, Oregon



Soil Map may not be valid at this scale.



## Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
17	Clackamas silt loam	C/D	0.1	1.6%
79B	Sawtell silt loam, 0 to 8 percent slopes	C	3.5	98.4%
<b>Totals for Area of Interest</b>			<b>3.5</b>	<b>100.0%</b>

### Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

### Rating Options

*Aggregation Method:* Dominant Condition



APPENDIX B

**WATER QUANTITY  
CALCULATIONS**



# LEGEND

PROJECT AREA LIMITS



LANDSCAPE AREA

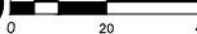


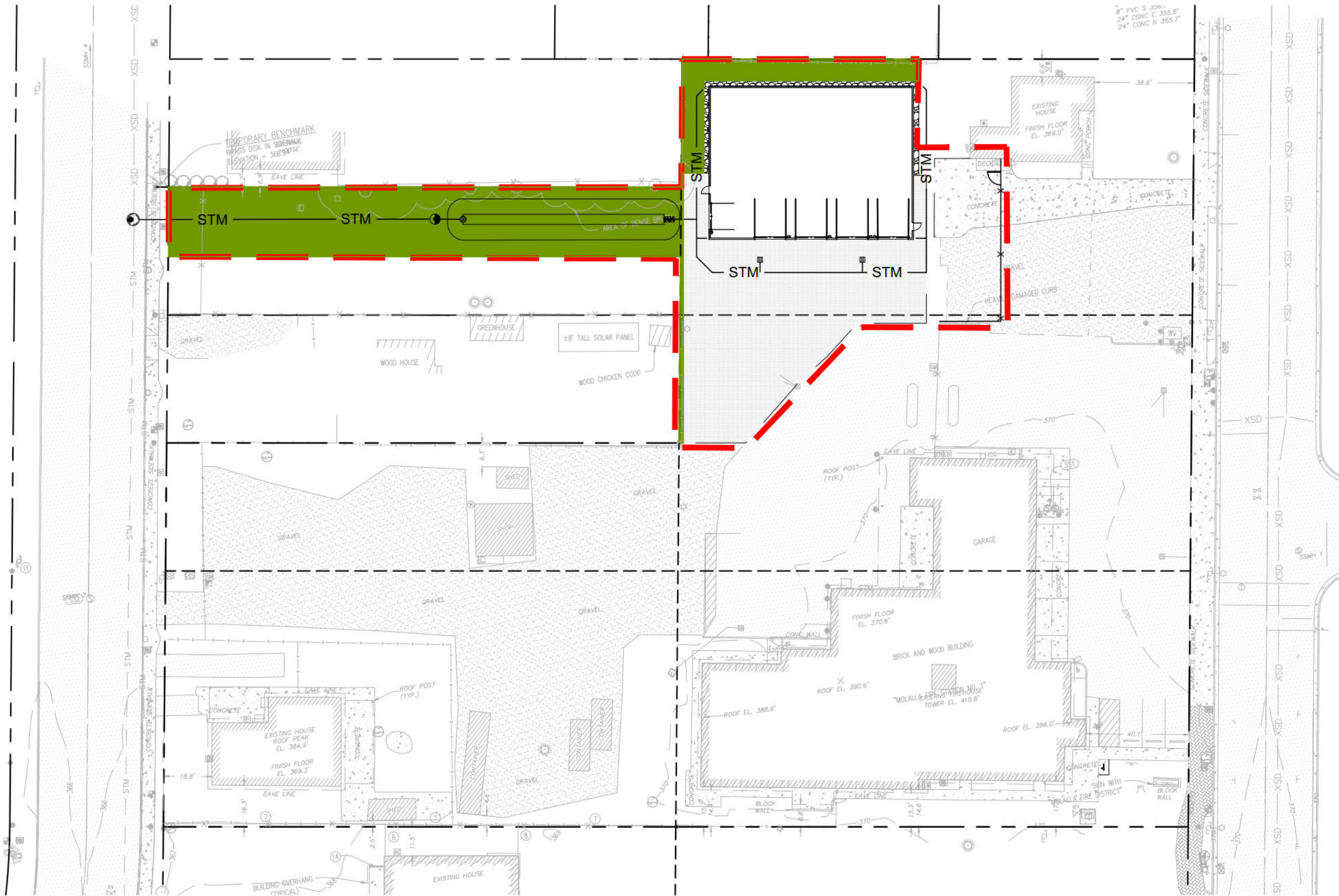
## MOLALLA FIRE - STATION 82 BASIN MAP - EXISTING

April 29, 2026  
Job # 2250404.00



SCALE: 1"=40'



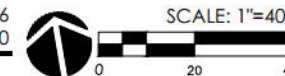


# LEGEND

- PROJECT AREA LIMITS
- LANDSCAPE AREA

## MOLALLA FIRE - STATION 82 BASIN MAP - PROPOSED

April 29, 2026  
Job # 2250404.00



## Pond No. 1 - Total Site Pond Area

### Pond Data

Contours -User-defined contour areas. Conic method used for volume calculation. Begining Elevation = 365.50 ft

### Stage / Storage Table

Stage (ft)	Elevation (ft)	Contour area (sqft)	Incr. Storage (cuft)	Total storage (cuft)
0.00	365.50	835	0	0
0.50	366.00	1,115	486	486
1.50	367.00	1,725	1,409	1,895

### Culvert / Orifice Structures

	[A]	[B]	[C]	[PrfRsr]
Rise (in)	= 4.50	Inactive	0.00	0.00
Span (in)	= 4.50	5.50	0.00	0.00
No. Barrels	= 1	1	0	0
Invert El. (ft)	= 365.50	366.00	0.00	0.00
Length (ft)	= 0.00	0.00	0.00	0.00
Slope (%)	= 0.00	0.00	0.00	n/a
N-Value	= .013	.013	.013	n/a
Orifice Coeff.	= 0.60	0.60	0.60	0.60
Multi-Stage	= n/a	No	No	No

### Weir Structures

	[A]	[B]	[C]	[D]
Crest Len (ft)	Inactive	0.00	0.00	0.00
Crest El. (ft)	= 366.00	0.00	0.00	0.00
Weir Coeff.	= 3.33	3.33	3.33	3.33
Weir Type	= 1	---	---	---
Multi-Stage	= Yes	No	No	No
Exfil.(in/hr)	= 0.000 (by Contour)			
TW Elev. (ft)	= 0.00			

Note: Culvert/Orifice outflows are analyzed under inlet (ic) and outlet (oc) control. Weir risers checked for orifice conditions (ic) and submergence (s).

### Stage / Storage / Discharge Table

Stage ft	Storage cuft	Elevation ft	Civ A cfs	Civ B cfs	Civ C cfs	PrfRsr cfs	Wr A cfs	Wr B cfs	Wr C cfs	Wr D cfs	Exfil cfs	User cfs	Total cfs
0.00	0	365.50	0.00	0.00	---	---	0.00	---	---	---	---	---	0.000
0.50	486	366.00	0.30 ic	0.00	---	---	0.00	---	---	---	---	---	0.297
1.50	1,895	367.00	0.61 ic	0.00	---	---	0.00	---	---	---	---	---	0.609

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

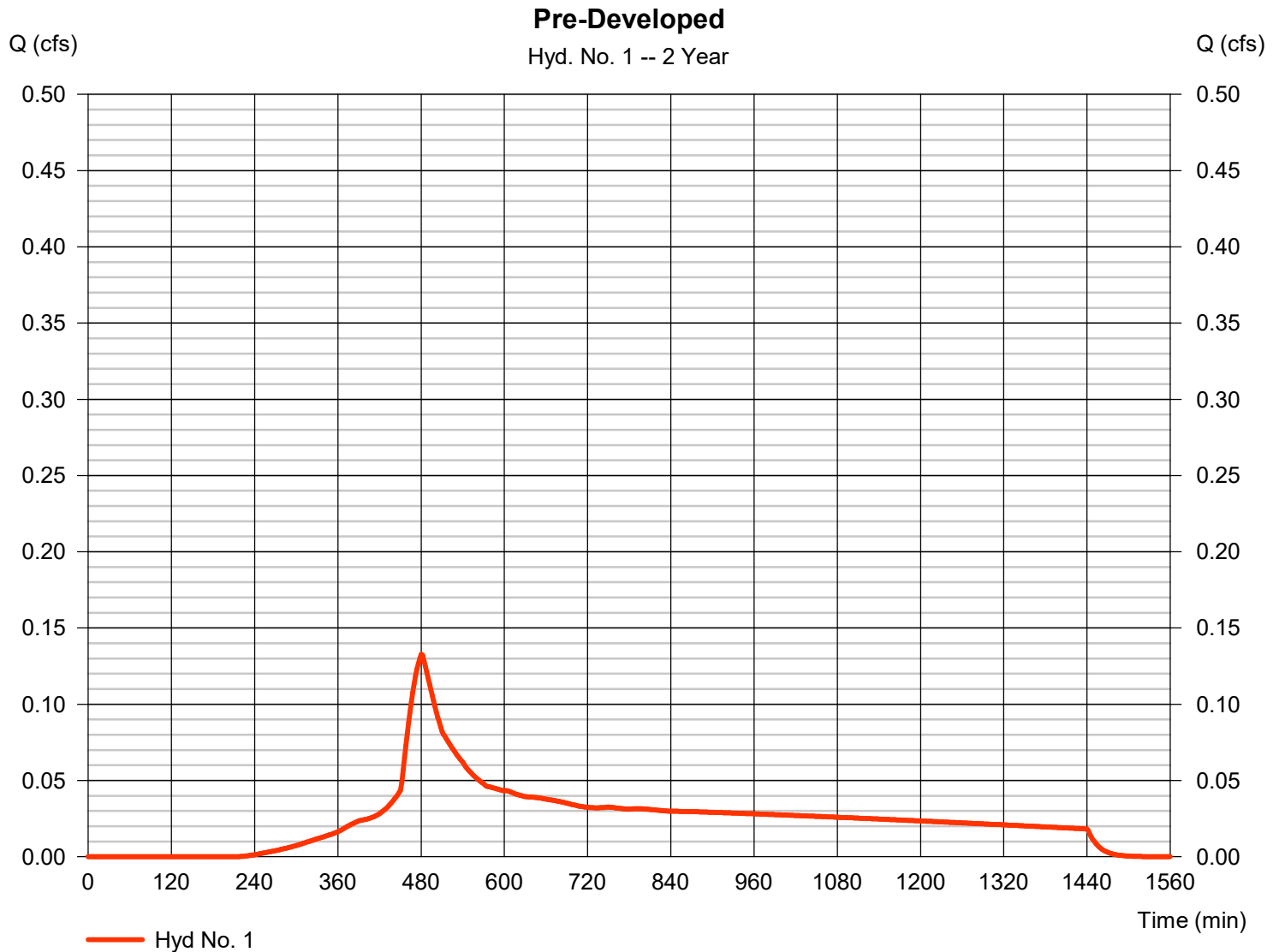
Wednesday, 04 / 29 / 2026

## Hyd. No. 1

Pre-Developed

Hydrograph type	= SBUH Runoff	Peak discharge	= 0.133 cfs
Storm frequency	= 2 yrs	Time to peak	= 480 min
Time interval	= 2 min	Hyd. volume	= 2,271 cuft
Drainage area	= 0.430 ac	Curve number	= 89*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 15.60 min
Total precip.	= 2.50 in	Distribution	= Type IA
Storm duration	= 24 hrs	Shape factor	= n/a

\* Composite (Area/CN) = [(0.340 x 86) + (0.090 x 98)] / 0.430



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

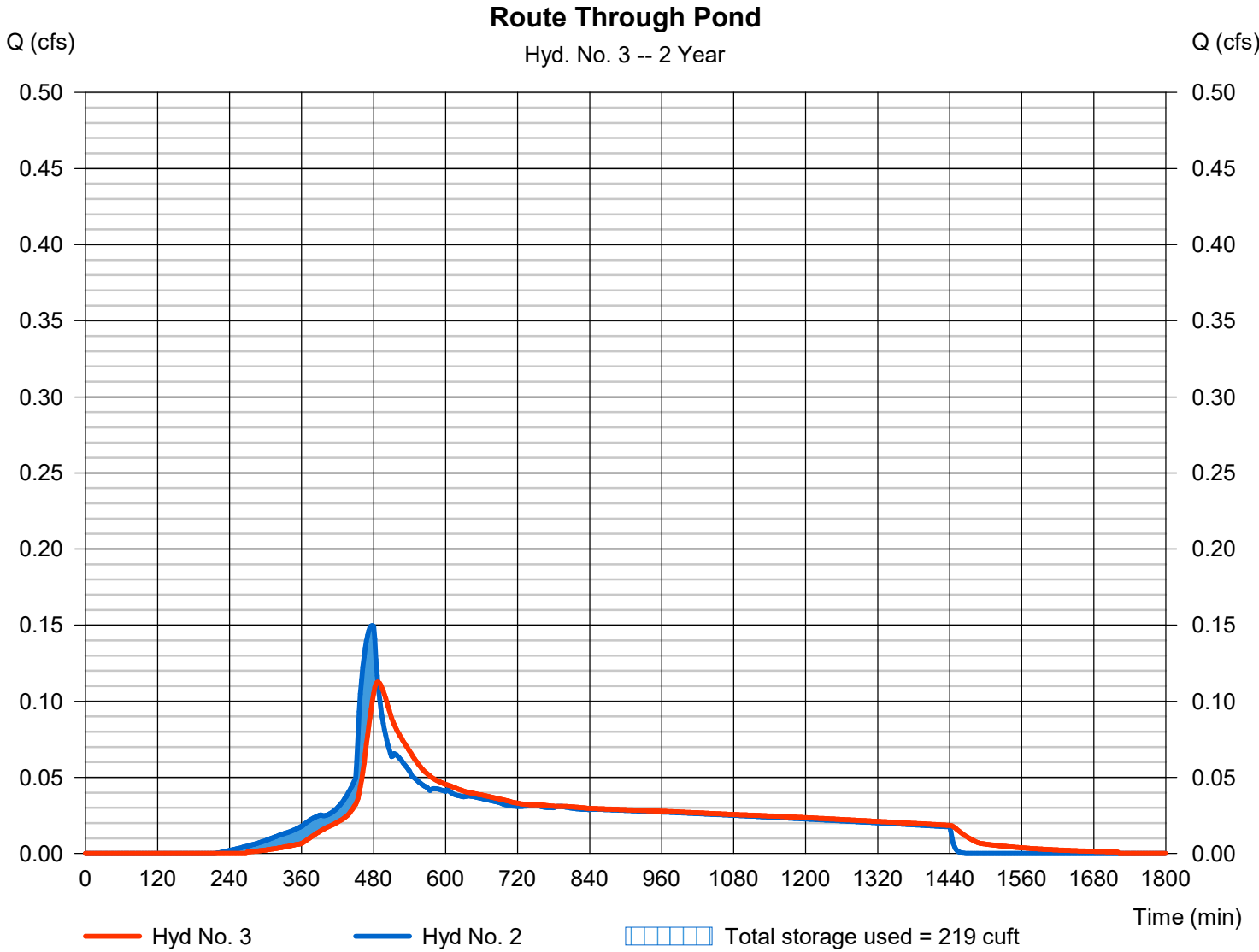
Wednesday, 04 / 29 / 2026

## Hyd. No. 3

### Route Through Pond

Hydrograph type	= Reservoir	Peak discharge	= 0.113 cfs
Storm frequency	= 2 yrs	Time to peak	= 486 min
Time interval	= 2 min	Hyd. volume	= 2,211 cuft
Inflow hyd. No.	= 2 - Developed	Max. Elevation	= 365.73 ft
Reservoir name	= Total Site Pond Area	Max. Storage	= 219 cuft

Storage Indication method used.



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

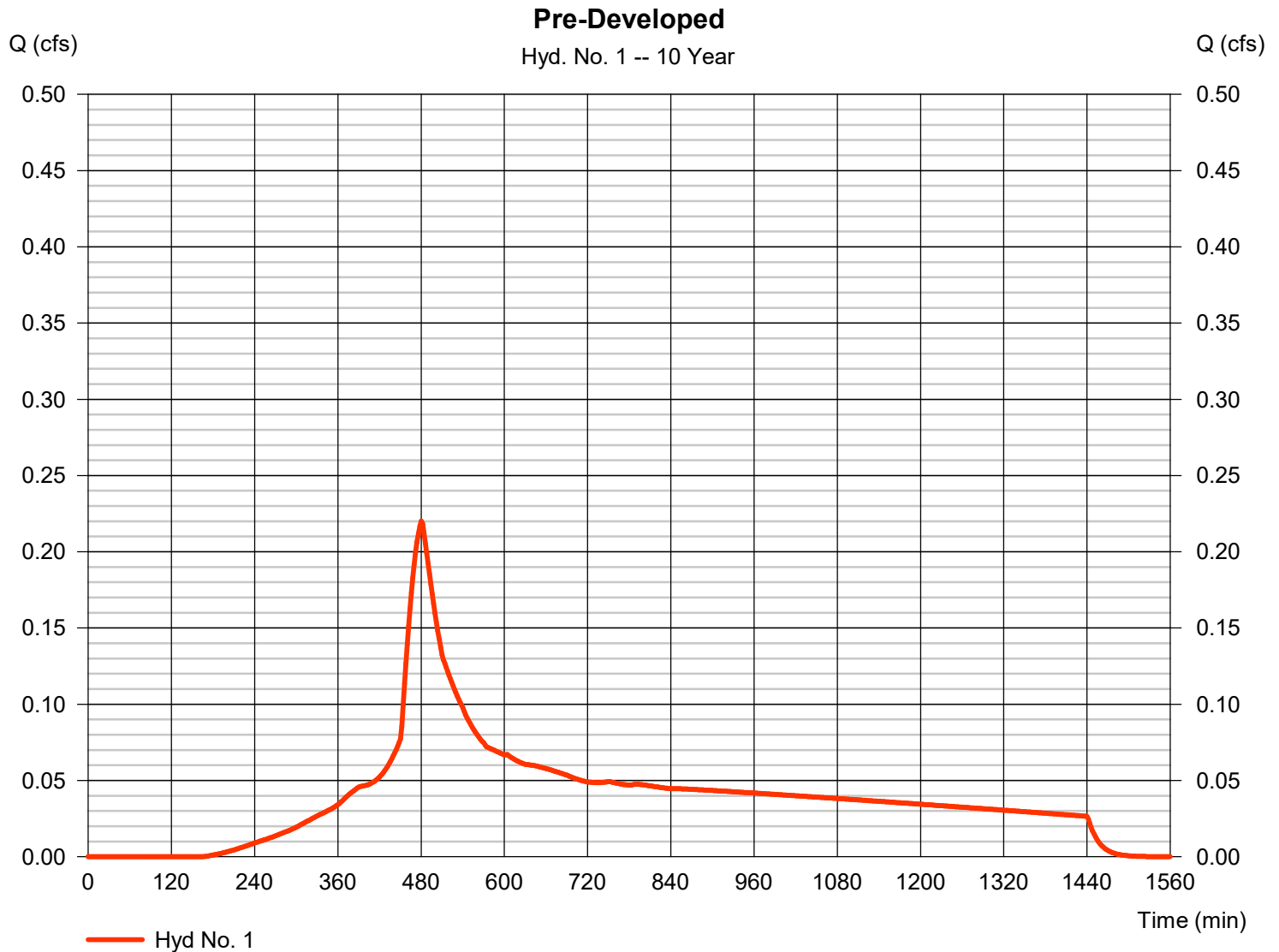
Wednesday, 04 / 29 / 2026

## Hyd. No. 1

Pre-Developed

Hydrograph type	= SBUH Runoff	Peak discharge	= 0.220 cfs
Storm frequency	= 10 yrs	Time to peak	= 480 min
Time interval	= 2 min	Hyd. volume	= 3,607 cuft
Drainage area	= 0.430 ac	Curve number	= 89*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 15.60 min
Total precip.	= 3.45 in	Distribution	= Type IA
Storm duration	= 24 hrs	Shape factor	= n/a

\* Composite (Area/CN) = [(0.340 x 86) + (0.090 x 98)] / 0.430



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

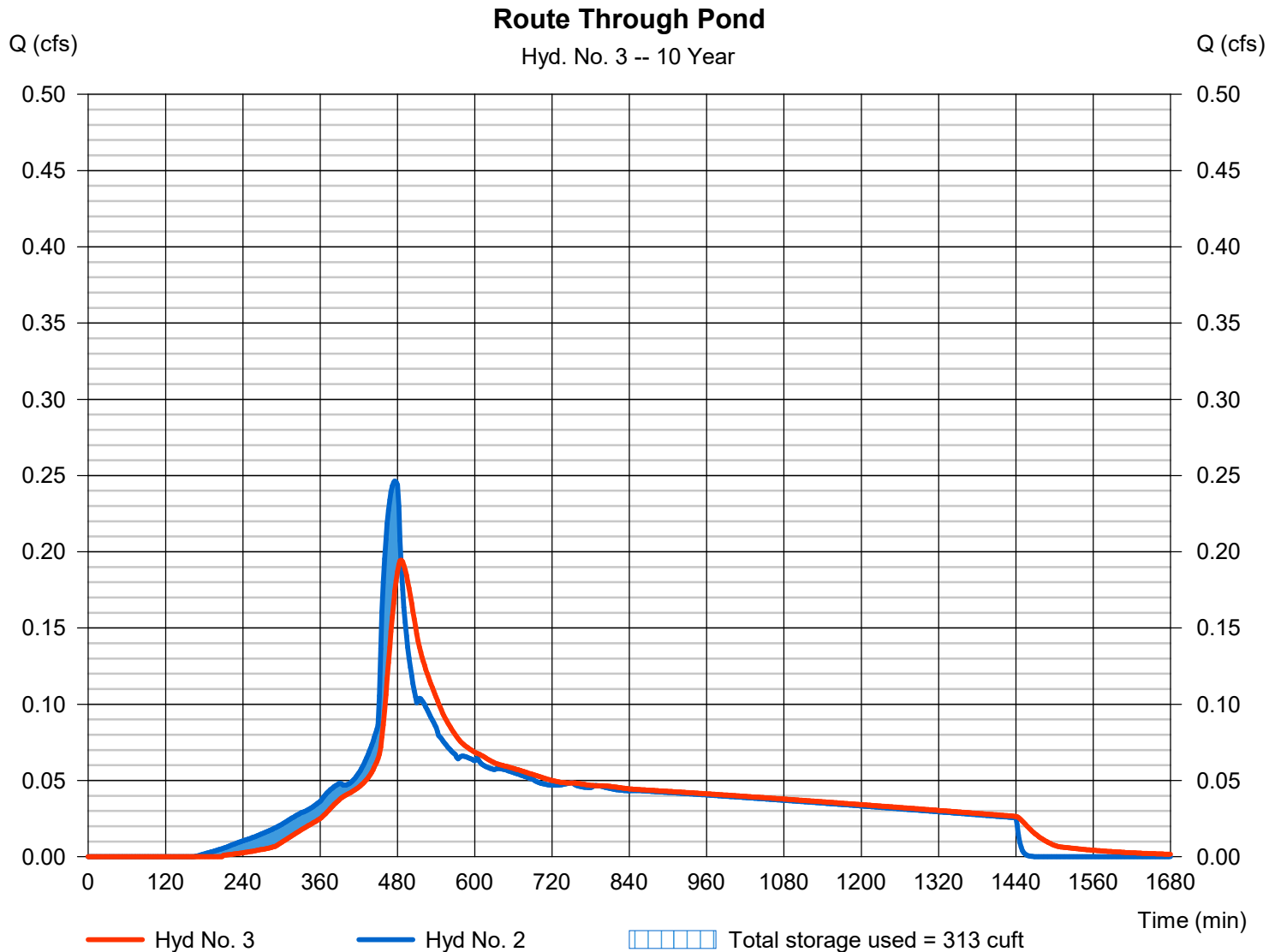
Wednesday, 04 / 29 / 2026

## Hyd. No. 3

### Route Through Pond

Hydrograph type	= Reservoir	Peak discharge	= 0.194 cfs
Storm frequency	= 10 yrs	Time to peak	= 486 min
Time interval	= 2 min	Hyd. volume	= 3,516 cuft
Inflow hyd. No.	= 2 - Developed	Max. Elevation	= 365.82 ft
Reservoir name	= Total Site Pond Area	Max. Storage	= 313 cuft

Storage Indication method used.



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

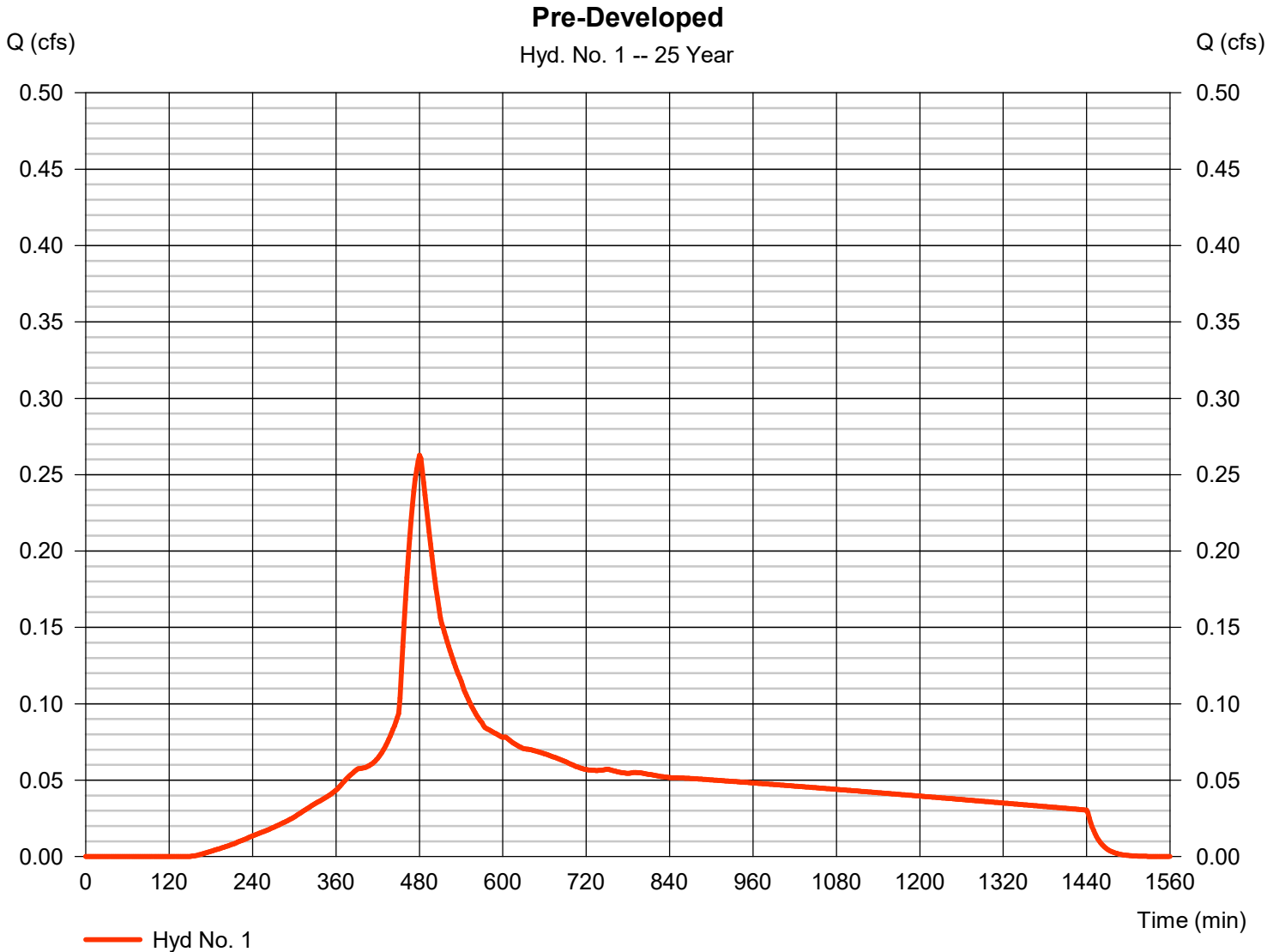
Wednesday, 04 / 29 / 2026

## Hyd. No. 1

Pre-Developed

Hydrograph type	= SBUH Runoff	Peak discharge	= 0.263 cfs
Storm frequency	= 25 yrs	Time to peak	= 480 min
Time interval	= 2 min	Hyd. volume	= 4,260 cuft
Drainage area	= 0.430 ac	Curve number	= 89*
Basin Slope	= 0.0 %	Hydraulic length	= 0 ft
Tc method	= TR55	Time of conc. (Tc)	= 15.60 min
Total precip.	= 3.90 in	Distribution	= Type IA
Storm duration	= 24 hrs	Shape factor	= n/a

\* Composite (Area/CN) = [(0.340 x 86) + (0.090 x 98)] / 0.430



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2024

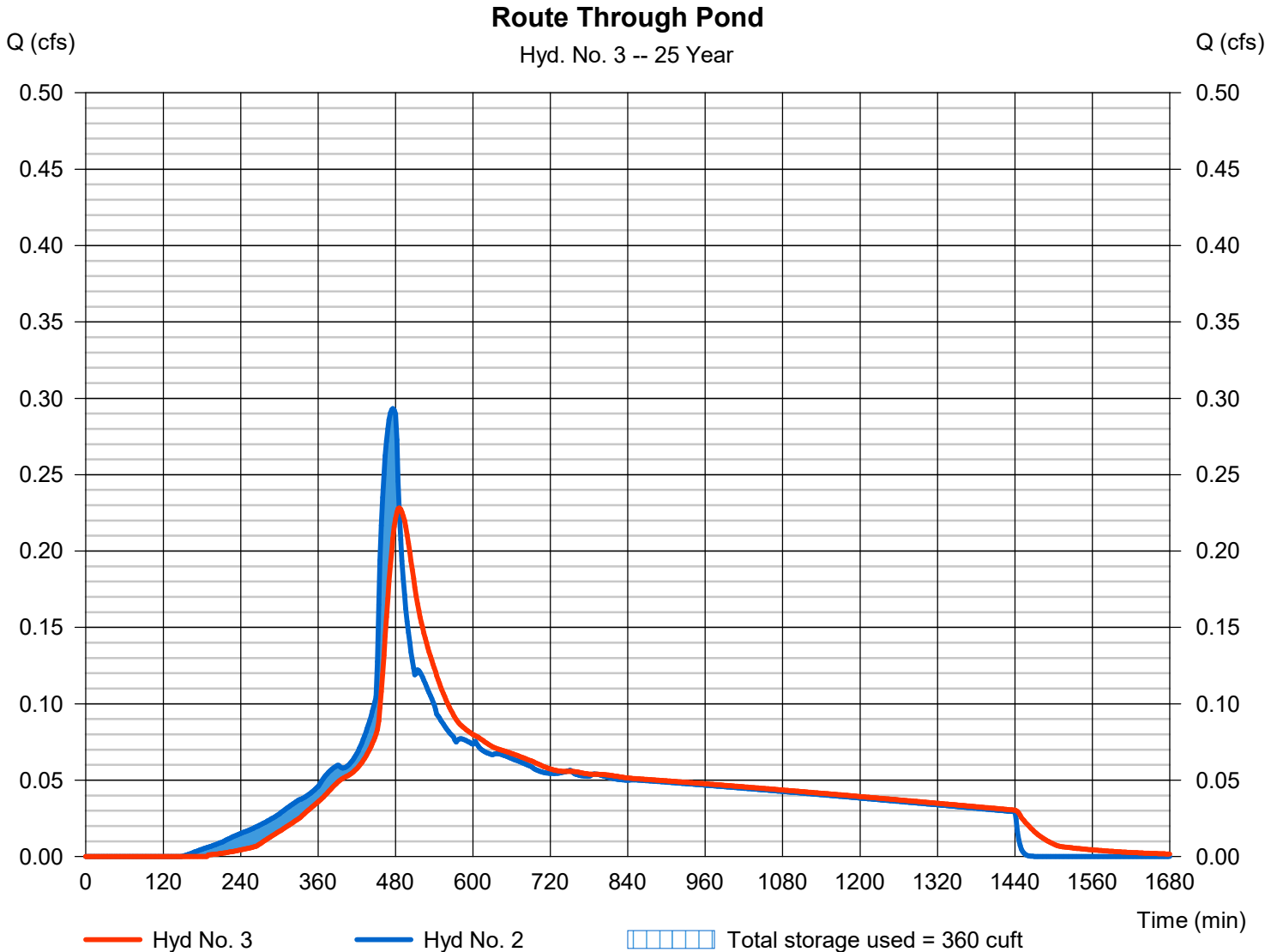
Wednesday, 04 / 29 / 2026

## Hyd. No. 3

### Route Through Pond

Hydrograph type	= Reservoir	Peak discharge	= 0.228 cfs
Storm frequency	= 25 yrs	Time to peak	= 486 min
Time interval	= 2 min	Hyd. volume	= 4,154 cuft
Inflow hyd. No.	= 2 - Developed	Max. Elevation	= 365.87 ft
Reservoir name	= Total Site Pond Area	Max. Storage	= 360 cuft

Storage Indication method used.



APPENDIX C

**CONVEYANCE  
CALCULATIONS**

## Storm Pipe Sizing - Rational Method

Project Name:	Molalla Fire	By:	BTC	Checked:	Checked
Project Number:	2250404	Date:	4/21/2026	Date:	Date

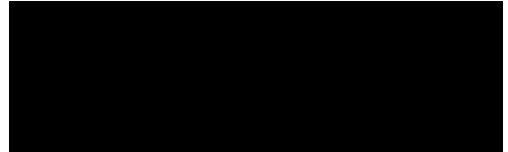
<b>User Entry Variables</b>	
Runoff Coefficient	C = 0.9
Pipe Coefficient of Friction	n = 0.013
Return Period	25 years

<b>Notes/Design Criteria</b>
C = 0.9 for pavement and roof areas City of Portland: 0.013 regardless of pipe material - check jurisdiction specific requirements Check conveyance requirement for jurisdiction (likely either 10 or 25)

<b>Equations</b>			
Rational	$Q = CiA$	Q = Runoff	ft <sup>3</sup> /s
Manning's	$Q_m = \left(\frac{1.49}{n}\right) A(R_h^{2/3})S^{1/2}$	Q <sub>m</sub> = Capacity	ft <sup>3</sup> /s
Hydraulic Radius	$R_h = \frac{A_{pipe}}{P}$	c = Runoff coefficient	N/A
Velocity (full pipe)	$V_f = \frac{Q_m}{A_{pipe}}$	I = Rainfall intensity	in/hr
Incremental Time	$T_i = \frac{L}{v_d} * \frac{min}{60 sec}$	A = Basin Area	ac
		P = Wetted Perimeter	ft
		S = Slope	ft/ft
		A <sub>pipe</sub> = Pipe Area	ft <sup>2</sup>
		L = Length of pipe	ft

Basin Data							Pipe Data			Calculations						
Pipe Link (See Basin Map)	Catchment ID (See Basin Map)	A <sub>i</sub> , Incremental Basin Area (Ac) <sup>1</sup>	A <sub>t</sub> , Total Basin Area (Ac) <sup>2</sup>	T <sub>c</sub> , Time of Concentration (min) <sup>3</sup>	T <sub>t</sub> , Total Time (min) <sup>4</sup>	i <sub>i</sub> , Intensity (in/hr) <sup>5</sup>	S, Pipe Slope (%) <sup>6</sup>	D, Pipe Diameter (in) <sup>7</sup>	L, Length of Pipe Run (ft) <sup>8</sup>	A <sub>pipe</sub> , Pipe Area (ft <sup>2</sup> )	R <sub>h</sub> , Hydraulic Radius <sup>2/3</sup>	Q, Runoff (ft <sup>3</sup> /s)	Q <sub>m</sub> , Capacity (ft <sup>3</sup> /s)	Runoff/Capacity (ratio) <sup>9</sup>	v <sub>d</sub> , Velocity at Design Flow (ft/s)	T <sub>i</sub> , Incremental Time (min)
	East Roof (50%)	0.06	0.06	5.00	5.00	3.40	0.30	6	72	0.20	0.25	0.17	<b>0.31</b>	0.55	1.59	0.76
	CB#1	0.03	0.03	5.00	5.00	3.40	0.30	6	5	0.20	0.25	0.09	<b>0.31</b>	0.30	1.36	0.06
1			0.09	5.00	5.76	3.26	0.30	6	40	0.20	0.25	0.25	<b>0.31</b>	0.81	1.73	0.39
	CB#2	0.03	0.03	5.00	5.00	3.40	0.30	6	5	0.20	0.25	0.09	<b>0.31</b>	0.30	1.36	0.06

Basin Data							Pipe Data			Calculations						
Pipe Link (See Basin Map)	Catchment ID (See Basin Map)	$A_i$ , Incremental Basin Area (Ac) <sup>1</sup>	$A_t$ , Total Basin Area (Ac) <sup>2</sup>	$T_c$ , Time of Concentration (min) <sup>3</sup>	$T_t$ , Total Time (min) <sup>4</sup>	$i$ , Intensity (in/hr) <sup>5</sup>	$S$ , Pipe Slope (%) <sup>6</sup>	$D$ , Pipe Diameter (in) <sup>7</sup>	$L$ , Length of Pipe Run (ft) <sup>8</sup>	$A_{pipe}$ , Pipe Area (ft <sup>2</sup> )	$R_h$ , Hydraulic Radius <sup>2/3</sup>	$Q$ , Runoff (ft <sup>3</sup> /s)	$Q_m$ , Capacity (ft <sup>3</sup> /s)	Runoff/Capacity (ratio) <sup>9</sup>	$v_d$ , Velocity at Design Flow (ft/s)	$T_i$ , Incremental Time (min)
2			0.12	5.00	6.14	3.19	2.00	6	13	0.20	0.25	0.33	<b>0.80</b>	0.42	3.83	0.06
	West Roof (50%)	0.06	0.06	5.00	5.00	3.40	0.30	6	53	<b>0.20</b>	0.25	0.18	0.308	0.596	1.62	0.54
3			0.18	5.00	6.20	3.18	1.50	6	9	<b>0.20</b>	0.25	0.50	0.689	0.728	3.79	0.04



APPENDIX D

**GEOTECHNICAL  
REPORT (RELEVANT  
EXCERPTS)**

**Report of Geotechnical  
Engineering Services**

**Molalla Fire Station 82  
Improvements**

**Molalla, Oregon**

**April 17, 2026**



April 17, 2026

Molalla Rural Fire Protection District #73  
320 North Molalla Avenue  
Molalla, OR 97038

**Re: Report of Geotechnical Engineering Services  
Molalla Fire Station 82 Improvements  
320 North Molalla Avenue  
Molalla, Oregon  
CWE Project: MolalFD-1-01-1**

Columbia West Engineering, Inc. (Columbia West) is pleased to present this geotechnical report for the proposed improvements to Molalla Fire Station 82 in Molalla, Oregon. Our services were conducted in accordance with the Geotechnical Engineering Services Contract between Molalla Rural Fire Protection District #73 and Columbia West dated February 26, 2026.

We appreciate the opportunity to work on the project. Please contact us if you have any questions regarding this report.

Sincerely,

NNP:kat  
Attachments  
Document ID: MolalFD-1-01-1-041726-geor.docx

## EXECUTIVE SUMMARY

This section provides a summary of the geotechnical considerations associated with the proposed improvements to Molalla Fire Station 82 in Molalla, Oregon. Our conclusions and recommendations are based on the subsurface information presented in the report and proposed development information provided by the design team. A detailed discussion of the geotechnical considerations summarized herein is presented in respective sections of the report.

- The proposed new building and improvements to the existing fire station can be supported on conventional spread footings bearing on firm native soil or new structural fill on firm native soil.
- Liquefaction, seismic settlement, and lateral spreading are not design considerations for the project.
- Cobbles, boulders, and construction debris from prior development at the site could be present. These materials (particularly boulders) will result in difficult excavation and trenches may be wider than anticipated, increasing the amount of backfill material required.
- Moisture conditioning will be required to use the on-site material as structural fill. Accordingly, extended dry weather will be required to adequately condition and place the soil as structural fill. It will be difficult, if not impossible, to adequately compact the on-site soil during the rainy season or during prolonged periods of rainfall.
- Dewatering during construction may be required in portions of the site when site cuts are more than 5 feet BGS.
- The on-site soil below existing improvements or at the ground surface will not provide adequate support for construction equipment during the wet construction season and possibly during the dry season. Granular haul roads and staging areas should be used if earthwork has the potential to damage existing subgrade. The earthwork contractor should be responsible for protection of the subgrade and determining appropriate thicknesses of haul roads and staging areas.
- Based on the soil and groundwater conditions and results of testing, infiltration systems are not feasible at the site.

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## **ABBREVIATIONS AND ACRONYMS**

AASHTO	American Association of State Highway and Transportation Officials
AC	asphalt concrete
ACP	asphalt concrete pavement
ASCE	American Society of Civil Engineers
ASTM	ASTM International
BGS	below ground surface
BPOE	Basic Performance Objective for Existing Buildings
BPON	Basic Performance Objective Equivalent to New Buildings
BSE	Basic Safety Earthquake
CSZ	Cascadia Subduction Zone
fps	feet per second
FHWA	Federal Highway Administration
g	gravitational acceleration (32.2 feet/second <sup>2</sup> )
GPS	global positioning system
H:V	horizontal to vertical
IBC	International Building Code
in/hr	inch(es) per hour
km	kilometer(s)
mm/yr	millimeter(s) per year
M <sub>w</sub>	moment magnitude
ORS	Oregon Revised Statute
OSHA	Occupational Safety and Health Administration
OSSC	2024 Oregon Standard Specifications for Construction
PCC	portland cement concrete
pcf	pounds per cubic foot
pci	pounds per cubic inch
PG	performance grade
PNSN	Pacific Northwest Seismic Network
psf	pounds per square foot
psi	pounds per square inch
ReMi	refraction microtremor
SOSSC	State of Oregon Structural Specialty Code
USDA	U.S. Department of Agriculture
USGS	U.S. Geological Survey
V <sub>s100</sub>	average shear wave velocity for the upper 100 feet of the soil profile

## REPORT OF GEOTECHNICAL ENGINEERING SERVICES MOLALLA FIRE STATION 82 IMPROVEMENTS MOLALLA, OREGON

### 1.0 INTRODUCTION

Columbia West is pleased to submit this geotechnical report for the proposed improvements to Molalla Fire Station 82 in Molalla, Oregon. The site is located at 320 North Molalla Avenue. The site is shown relative to surrounding physical features on Figure 1. The existing site conditions are shown on Figure 2. Abbreviations and acronyms used herein are defined immediately following the Table of Contents.

The project includes tenant improvements to the existing building and construction of a new 60-foot by 90-foot building. The new building will house an apparatus bay, office space, multiple bunk rooms, and associated support spaces. Related site improvements will include construction of new utilities and potentially new pavement areas.

We anticipate maximum column and wall loads for the building will be less than 100 kips and 8 kips per lineal foot, respectively, with floor slab loading of 200 psf. Based on the topography at the site, we anticipate cuts and fills will generally be less than a few feet.

### 2.0 PURPOSE AND SCOPE

The purpose of our services was to provide geotechnical engineering recommendations for use in design and construction of the proposed project. Specifically, we completed the following tasks:

- Reviewed information available in Columbia West's files for the site vicinity.
- Coordinated and managed the field exploration program, which included locating utilities, coordinating site access, and scheduling subcontractors and Columbia West field staff.
- Drilled two borings to depths of 14.5 and 12.9 feet BGS.
- Collected soil samples from the explorations for laboratory testing and maintained a log of encountered soil and groundwater conditions in the explorations.
- Completed infiltration testing to evaluate the potential for on-site stormwater systems.
- Completed a laboratory testing program using select soil samples collected from the explorations, including the following:
  - Four moisture content determinations in general accordance with ASTM D2216
  - Three particle-size analyses in general accordance with ASTM D1140
  - One Atterberg limits test in general accordance with ASTM D4318
- Prepared this geotechnical report that summarizes our explorations, laboratory testing, and analyses and provides geotechnical design criteria and construction recommendations for the development, including information relating to the following:
  - Summary of soil and groundwater conditions at the site
  - Laboratory testing results
  - Infiltration test results
  - Liquefaction discussion
  - Recommendations for shallow foundation support, including allowable bearing pressure, modulus of subgrade reaction, and total and differential settlement

- Recommendations for foundations and walls, including passive earth pressure and coefficient
- A site-specific seismic evaluation that includes spectral response accelerations at short and 1-second periods ( $S_s$  and  $S_1$ )
- Recommendations for management of groundwater for design of structures and pavement
- Lateral earth pressure design for below-grade walls
- Recommendations for AC and PCC design and construction based on loading information provided by the design team
- Recommendations for temporary excavations
- Recommendations for cut and fill
- Recommendations for underslab and foundation drainage
- Results of infiltration testing and recommendations.

### **3.0 SITE CONDITIONS**

#### **3.1 GEOLOGY**

The near-surface soil at the site is mapped as Miocene- to Pleistocene-aged Troutdale Formation consisting of claystone, siltstone, sandstone, and conglomerate material (Hampton 1972). The Web Soil Survey indicates the near-surface soil at the site consists of Sawtell silt loam consisting of approximately 5 feet of clay with gravel (USDA 2026).

#### **3.2 SURFACE CONDITIONS**

The approximately 2.25-acre, irregularly shaped fire station property is located at 320 North Molalla Avenue. The site is bounded by residential properties to the north, North Molalla Avenue to the east, a park and the City of Molalla Library to the south, and Kennel Avenue to the west.

The existing fire station is located in the southeast portion of the site. The remainder of the site is occupied by a mixture of gravel-surfaced storage areas for the fire station; a residential structure; an outbuilding; and open, grassy areas. The proposed new building will be located in an area currently occupied by an outbuilding and grass.

Site topography is generally flat. Vegetation at the site is limited to grassy areas and several scattered trees. Existing conditions at the site are shown on Figure 2.

#### **3.3 SUBSURFACE CONDITIONS**

##### **3.3.1 General**

Subsurface conditions at the site were explored by drilling two borings (B-1 and B-2) to depths of 14.5 and 12.9 feet BGS, respectively. The exploration locations are shown on Figure 2. A description of our field exploration program and the exploration logs are presented in Appendix A. A description of our laboratory testing program and the test results are presented in Appendix B.

In addition to Columbia West's recent borings, Geotech Solutions, Inc. completed two drilled borings at the site to depths of 13 and 21 feet BGS. Logs of these borings are presented in Appendix C and their locations are shown on Figure 2.

One ReMi array (A-1) was previously completed at the Molalla Library directly adjacent to the fire station. As described in Appendix D, based on subsurface conditions and geologic mapping, the results of the ReMi array are valid for design of the fire station improvements. ReMi array A-1 was completed at the location shown on Figure 2. The shear wave velocity test results are presented in Appendix D.

A summary of the subsurface conditions is presented below.

### **3.3.2 Soil Conditions**

#### **3.3.2.1 Clay**

Medium stiff to very stiff, brown and orange-tan clay with variable proportions of gravel and sand is present below the ground surface. An approximately 2-inch-thick root zone was observed directly below the ground surface. While not able to be observed directly in the borings, we anticipate a 4- to 8-inch-thick topsoil zone may also be present directly below the ground surface in the clay.

The clay has low plasticity and is moist. The clay extends to a depth of 7.5 feet BGS. Laboratory testing indicates the moisture content of the clay varied between 32 and 43 percent at the time of exploration.

#### **3.3.2.2 Gravel**

Medium dense to very dense clayey gravel is present below the clay. The gravel contains variable proportions of sand and is moist to wet. The gravel extends to the maximum depth explored in both borings. Laboratory testing indicates the moisture content of the gravel varied between 11 and 12 percent at the time of exploration.

### **3.3.3 Groundwater**

Groundwater was observed in borings B-1 and B-2 at a depths of approximately 8.5 and 3 feet BGS, respectively. The groundwater measurement at 3 feet BGS was after infiltration testing was completed, and we anticipate groundwater is not this shallow. Groundwater was observed at a depth of approximately 13 feet BGS in explorations at the Molalla Library to the southwest.

Based on experience, there is a seasonal fluctuation of groundwater levels in the area, and we anticipate groundwater could be within 5 to 8 feet of the ground surface at the end of the wet season and up to 20 feet BGS at the end of the dry season.

## **3.4 INFILTRATION TESTING**

Infiltration testing was completed in one of the borings to assist in evaluating stormwater infiltration facilities for the project. The location of the test was provided by Mackenzie. Because groundwater was observed at a depth of 8.5 feet BGS at the site, infiltration testing was completed at a depth of 3 feet BGS to maintain a minimum 5-foot separation between the bottom of the infiltration system and seasonal high groundwater.

Infiltration testing was conducted in general accordance with the recommendations for the encased falling head method. Table 1 summarizes our infiltration testing results and fines content determination. The exploration logs and laboratory test results are presented in Appendices A and B, respectively.

**Table 1. Infiltration Testing Results**

<b>Location</b>	<b>Depth (feet BGS)</b>	<b>Soil Type</b>	<b>Fines Content<sup>1</sup> (percent)</b>	<b>Unfactored Infiltration Rate (in/hr)</b>
B-2	3	Clay with sand and gravel	48	Negligible

1. Fines content: percent passing U.S. Standard No. 200 sieve

Recommendations for design of infiltration system are provided in Section 4.6.4 (Stormwater Infiltration Systems).

### **3.5 GEOLOGIC HAZARDS**

#### **3.5.1 Seismic Settlement and Lateral Spreading**

Liquefaction is caused by a rapid increase in pore water pressure that reduces the effective stress between soil particles. Granular soil, which relies on interparticle friction for strength, undergoes a loss of strength until the excess pore pressures dissipate. In general, loose, saturated sand soil with low silt and clay content is the most susceptible to liquefaction. Silty soil with low plasticity can be susceptible to strain softening under relatively higher levels of ground shaking. Strain-softened soil has volumetric strains much smaller than liquefiable soil due to matrix effects.

Lateral spreading is a liquefaction-related seismic hazard and occurs on gently sloping or flat sites underlain by liquefiable sediment adjacent to an open face, such as a riverbank. Liquefied soil adjacent to an open face can flow toward the open face, resulting in lateral ground displacement.

Based on the soil and groundwater conditions and results of geophysical testing at the site, it is our opinion that liquefaction and lateral spreading are not design consideration for the project.

#### **3.5.2 Other Geologic Hazards**

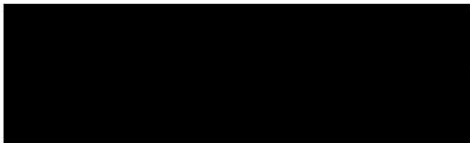
A discussion of other geologic hazards that could affect the site are discussed in the site-specific seismic hazard evaluation presented in Appendix E.

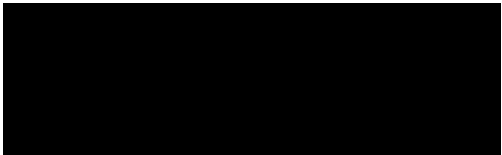
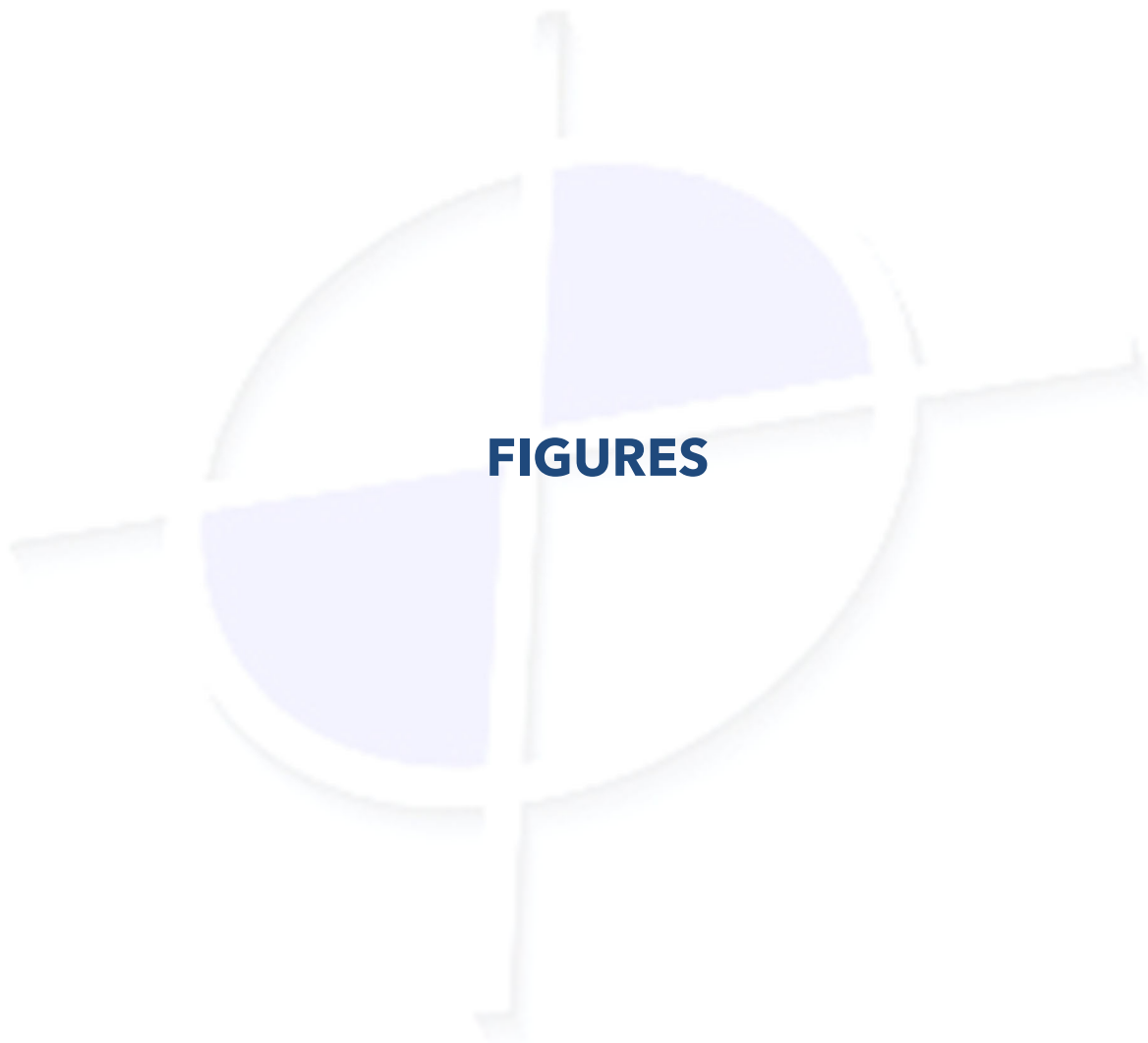
## **4.0 DESIGN**

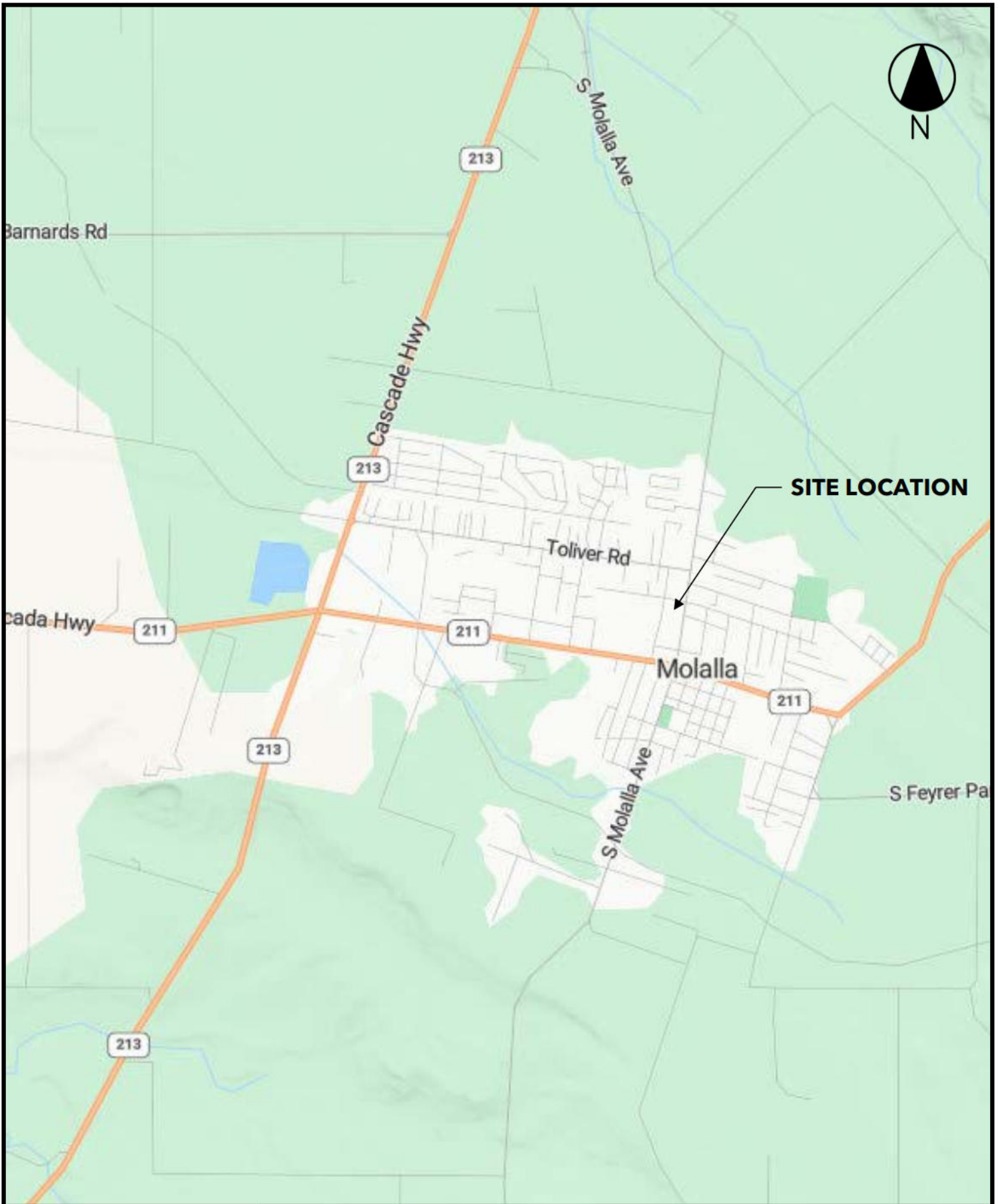
### **4.1 FOUNDATION SUPPORT**

#### **4.1.1 General**

Structural loads were unknown at the time this report was prepared; however, we anticipate maximum column and wall loads for the new building and existing station will be less than 100 kips and 8 kips per lineal foot, respectively. Based on the subsurface conditions at the site, foundations for structures associated with development can be supported on conventional spread footings on firm native soil.












PROJECT NO.:  
MOLALFD-1-01-1  
APRIL 2026

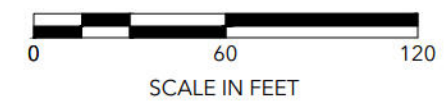
VICINITY MAP  
MOLALLA FIRE STATION 82 IMPROVEMENTS  
MOLALLA, OREGON

FIGURE  
1

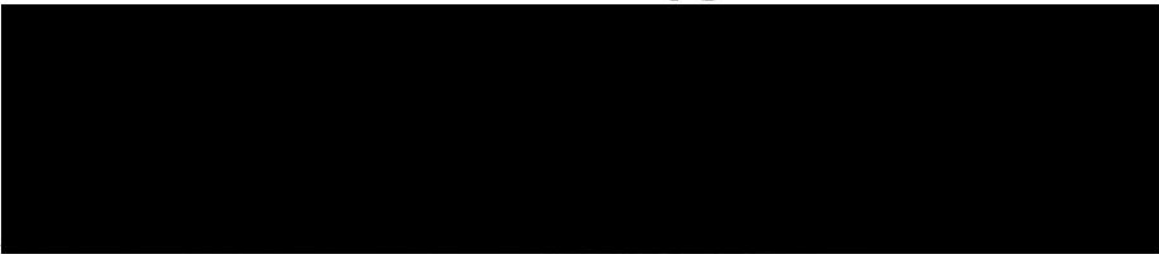


**LEGEND**

-  SITE BOUNDARY
-  BORING (COLUMBIA WEST 2026)
-  BORING (GEOTECH SOLUTIONS 2017)
-  APPROXIMATE NEW BUILDING LOCATION
-  PRIOR REMI ARRAY
- IR** UNFACTORED INFILTRATION RATE



- NOTES:
1. AERIAL PHOTO SOURCED FROM GOOGLE EARTH.
  2. EXPLORATION LOCATIONS ARE APPROXIMATE AND NOT SURVEYED.
  3. REFER TO REPORT TEXT FOR EXPLORATION DESCRIPTIONS.



**OWNERSHIP INFORMATION**

Owner: Molalla Fire Dist #73  
CoOwner:  
Site: 320 N Molalla Ave Molalla OR 97038  
Mail: PO Box 655 Molalla OR 97038

Parcel #: 01097805  
Ref Parcel #: 52E09CB08400  
TRS: 05S / 02E / 09 / SW  
County: Clackamas

**PROPERTY DESCRIPTION**

Map Grid: 837-E1  
Census Tract: 023901 Block: 1062  
Neighborhood: MOLA - Molalla  
School Dist: 35 Molalla River  
Impr Type:  
Subdiv/Plat: The Shaver Place  
Land Use: 201 - Commercial land improved  
Std Land Use: CMSC - Commercial Miscellaneous  
Zoning: Molalla-PSP - Public/Semi Public  
Lat/Lon: 45.15026 / -122.57702  
Watershed: Rock Creek  
Legal: 360 THE SHAVER PLACE LT 4 BLK 2|Y|184,433

**ASSESSMENT AND TAXATION**

Market Land: \$166,896.00  
Market Impr: \$2,268,030.00  
Market Total: \$2,434,926.00 (0)  
% Improved: 93.00%  
Assessed Total: \$1,546,178.00 (2019)  
Levy Code: 035-039  
Tax: \$0.00 (2019)  
Millage Rate: 16.5002  
Exemption:  
Exemption Type: Fire District Property

**PROPERTY CHARACTERISTICS**

Bedrooms:	Total SqFt:	Year Built: 1967
Baths, Total:	First Floor:	Eff Year Built:
Baths, Full:	Second Floor:	Lot Size Ac: 0.46 Acres
Baths, Half:	Basement Fin:	Lot Size SF: 19,972 SqFt
Total Units:	Basement Unfin:	Lot Width:
# Stories:	Basement Total:	Lot Depth:
# Fireplaces:	Attic Fin:	Roof Material:
Cooling:	Attic Unfin:	Roof Shape:
Heating:	Attic Total:	Ext Walls:
Building Style:	Garage:	Const Type:

**SALES AND LOAN INFORMATION**

Owner	Date	Doc #	Sale Price	Deed Type	Loan Amt	Loan Type
WALTER C RIDDELL	08/15/2016	677-104		S		
				Deed		Conv/Unk

Sentry Dynamics, Inc. and its customers make no representations, warranties or conditions, express or implied, as to the accuracy or completeness of information contained in this report.

8600



After recording return to:  
Molalla Rural Fire Protection District  
PO Box 655  
Molalla, OR 97038

Until a change is requested all tax  
statements shall be sent to the  
following address:  
Molalla Rural Fire Protection District  
PO Box 655  
Molalla, OR 97038

File No.: 7072-2113190 (se)  
Date: June 21, 2013

THIS SPACE RESERVED FOR RECORDER'S USE

Clackamas County Official Records Sherry Hall, County Clerk	<b>2013-062705</b>
	08/30/2013 01:42:54 PM
D-D                      Cnt=1 Stn=1 BARBARA	
\$15.00 \$16.00 \$10.00 \$17.00	<b>\$58.00</b>

FIRST AMERICAN 2113190 SS

### STATUTORY WARRANTY DEED

**Mildred A. Robinson, as trustee of the Mildred A. Robinson Revocable Living Trust u/t/a dated October 14, 2009, Grantor, conveys and warrants to Molalla Rural Fire Protection District #73, Grantee, the following described real property free of liens and encumbrances, except as specifically set forth herein:**

**LEGAL DESCRIPTION:** Real property in the County of Clackamas, State of Oregon, described as follows:

**Lot 2, Block 2, THE SHAVER PLACE, in the City of Molalla, County of Clackamas and State of Oregon.**

**Subject to:**

1. Covenants, conditions, restrictions and/or easements, if any, affecting title, which may appear in the public record, including those shown on any recorded plat or survey.

The true consideration for this conveyance is **\$230,000.00**. (Here comply with requirements of ORS 93.030)

411 8100 9300

WFG Title 15005789 LP 88

File No. 15005789

Clackamas County Official Records Sherry Hall, County Clerk	<b>2015-081466</b>
	12/11/2015 10:29:34 AM
D-D                      Cnt=1 Stn=5 KANNA	
\$5.00 \$16.00 \$10.00 \$22.00	<b>\$53.00</b>

<b>Grantor</b>
US Bank National Association as Trustee for RAMP 2005-EFC5 c/o Ocwen Loan Servicing, LLC 1661 Worthington Road, Suite 100 West Palm Beach, FL 33409
<b>Grantee</b>
Molalla Rural Fire Protection District #73 P.O. Box 855 N Molalla Ave Molalla, OR 97038
<b>After recording return to</b>
Molalla Rural Fire Protection District #73 P.O. Box 855 N Molalla Ave Molalla, OR 97038
<b>Until requested, all tax statements shall be sent to</b>
Molalla Rural Fire Protection District #73 P.O. Box 855 N Molalla Ave Molalla, OR 97038 Tax Acct No(s): 01097896

Reserved for Recorder's Use

### STATUTORY SPECIAL WARRANTY DEED

**[REDACTED]** Grantor, conveys and specially warrants to Molalla Rural Fire Protection District #73, Grantee, the following described real property free of encumbrances created or suffered by the Grantor except as specifically set forth herein:

**The South one-half of Lot 11, Block 2, THE SHAVER PLACE, in the City of Molalla, County of Clackamas and State of Oregon.**

This property is free of encumbrances created or suffered by the Grantor, EXCEPT: NONE

The true consideration for this conveyance is \$111,000.00. (Here comply with requirements of ORS 93.030.)

**BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.338 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010. THIS INSTRUMENT DOES NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY THAT THE UNIT OF LAND BEING TRANSFERRED IS A LAWFULLY ESTABLISHED LOT OR PARCEL, AS DEFINED IN ORS 92.010 OR 215.010, TO VERIFY THE APPROVED USES OF THE LOT OR PARCEL, TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES AS DEFINED IN ORS 30.920, AND TO INQUIRE ABOUT THE RIGHTS OF**



**Bronson W Rueda, County Assessor  
Department of Assessment and Taxation**

**Development Services Building  
150 Beaver Creek Road, Oregon City, OR 97045**

October 20, 2025

MOLALLA FIRE DIST #73  
PO BOX 655  
MOLALLA OR 97038

**RECEIVED**  
OCT 22 2025  
BY: *me* .....

Dear Exempt Property Owner:

The following information describes your property which is exempt from taxation by the Oregon Revised Statute referenced. This letter is your notice of value on the exempt property for the 2025-2026 tax year. If you have any questions, please feel free to contact Meka Olsen at 503/655-8671, extension 7638.

**2025 Value Notice on Exempt Property**

<i>Account</i>	<i>Map</i>	<i>Acres</i>	Section 20 Township 4S Range 2E Quarter DA TAX LOT 01000	<i>Assessed Value</i>	<b>859,928</b>
01033429	42E20DA01000			<i>Market Land</i>	<b>180,785</b>
MOLALLA FIRE DIST #73				<i>Market Building</i>	<b>1,233,570</b>
				<i>Market Total</i>	<b>1,414,355</b>
27689 S HWY 213			<i>Fire District Property</i>		
MULINO					
<i>Account</i>	<i>Map</i>	<i>Acres</i>	360 THE SHAVER PLACE LT 4 BLK 2	<i>Assessed Value</i>	<b>2,361,590</b>
01097805	52E09CB08400			<i>Market Land</i>	<b>220,674</b>
MOLALLA FIRE DIST #73				<i>Market Building</i>	<b>3,663,520</b>
				<i>Market Total</i>	<b>3,884,194</b>
320 N MOLALLA AVE			<i>Fire District Property</i>		
MOLALLA					
<i>Account</i>	<i>Map</i>	<i>Acres</i>	360 THE SHAVER PLACE LT 3 BLK 2	<i>Assessed Value</i>	<b>1,078,570</b>
01097814	52E09CB08500			<i>Market Land</i>	<b>220,674</b>
MOLALLA FIRE DIST #73				<i>Market Building</i>	<b>1,553,290</b>
				<i>Market Total</i>	<b>1,773,964</b>
NO SITUS			<i>Fire District Property</i>		
ADDRESS					
<i>Account</i>	<i>Map</i>	<i>Acres</i>	360 THE SHAVER PLACE SOUTH 1/2 LT 11 BLK 2	<i>Assessed Value</i>	<b>94,424</b>
01097896	52E09CB09300			<i>Market Land</i>	<b>175,836</b>
MOLALLA FIRE DIST #73				<i>Market Building</i>	<b>0</b>
				<i>Market Total</i>	<b>175,836</b>
411 KENNEL AVE			<i>Fire District Property</i>		
MOLALLA					
<i>Account</i>	<i>Map</i>	<i>Acres</i>	Section 03 Township 6S Range 2E TAX LOT 01405	<i>Assessed Value</i>	<b>1,151,772</b>
01350022	62E03 01405			<i>Market Land</i>	<b>187,476</b>
MOLALLA FIRE DIST #73				<i>Market Building</i>	<b>1,957,350</b>
				<i>Market Total</i>	<b>2,144,826</b>
36715 S SAWTELL RD			<i>Fire District Property</i>		
MOLALLA					



**Bronson W Rueda, County Assessor**  
**Department of Assessment and Taxation**

**Development Services Building**  
**150 Beaver Creek Road, Oregon City, OR 97045**

October 20, 2025

MOLALLA RURAL FIRE PROTECTION DIST #73  
PO BOX 655  
MOLALLA OR 97038

**RECEIVED**  
**OCT 22 2025**  
*Wm*  
**BY: .....**

Dear Exempt Property Owner:

The following information describes your property which is exempt from taxation by the Oregon Revised Statute referenced. This letter is your notice of value on the exempt property for the 2025-2026 tax year. If you have any questions, please feel free to contact Meka Olsen at 503/655-8671, extension 7638.

**2025 Value Notice on Exempt Property**

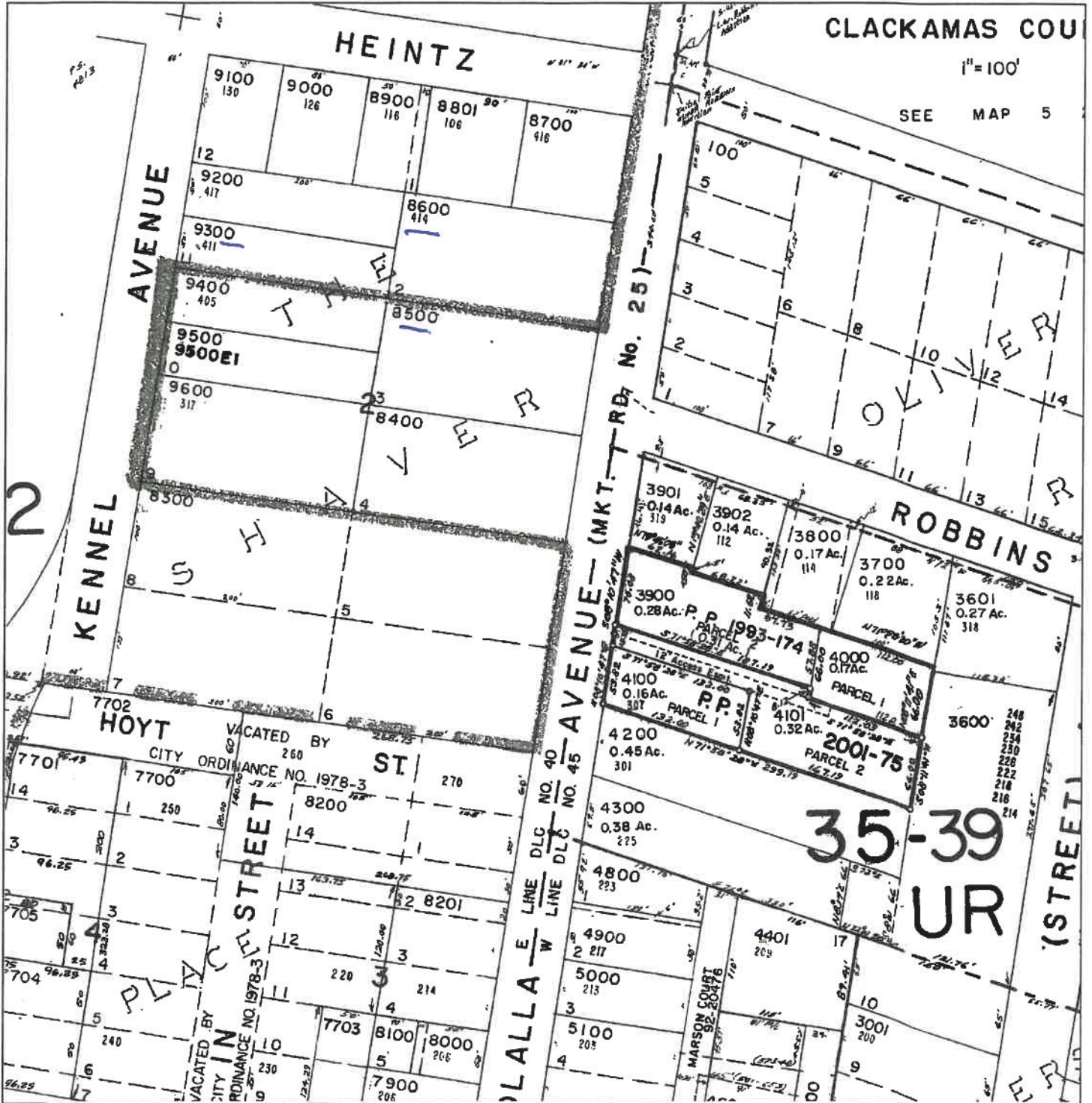
Account	Map	Acres	360 THE SHAVER PLACE LT 2 BLK 2	Assessed Value	159,293
01097823	52E09CB08600			Market Land	206,965
MOLALLA RURAL FIRE PROTECTION DIST #73				Market Building	55,030
				Market Total	261,995
414 N MOLALLA AVE		<i>Fire District Property</i>			
MOLALLA					

Account	Map	Acres	Subdivision THE SHAVER PLACE BLK 2	Assessed Value	102,793
01097912	52E09CB09500			Market Land	175,531
MOLALLA RURAL FIRE PROTECTION DIST #73			PT LT 10	Market Building	15,890
			360	Market Total	191,421
321 KENNEL AVE		<i>Fire District Property</i>			
MOLALLA					

Account	Map	Acres	360 THE SHAVER PLACE LT 9 BLK 2	Assessed Value	261,599
01097921	52E09CB09600			Market Land	231,729
MOLALLA RURAL FIRE PROTECTION DIST #73				Market Building	255,420
				Market Total	487,149
317 KENNEL AVE		<i>Fire District Property</i>			
MOLALLA					



Parcel #: 01097896 / 52E09CB09300



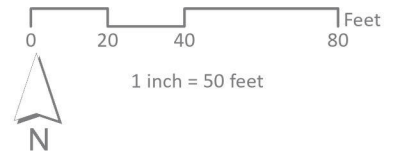
This map is a copy of public record and is provided solely for informational purposes. WFG National Title assumes no liability for variations, if any, in dimensions, area or location of the premises or the location of improvements.

# Molalla Fire Station 82

Molalla, Oregon

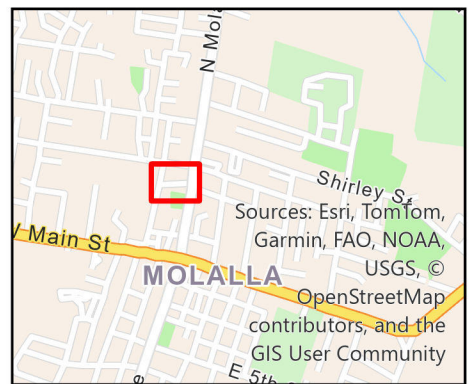
## LEGEND

-  Subject Site Boundary
-  Tax Lots



SOURCE DATA: Metro RUS Lite Base Data, Jan 2026  
GEOGRAPHIC PROJECTION: NAD 83 HARN, Oregon North Lambert Conformal Conic

Date: 3/23/2026 Map Created By: CREINOEHL  
File: Molalla Fire Project No: 2250404.00



Source: Esri, Vantor, Earthstar Geographics, and the GIS User Community, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community, Esri, USDA FSA



# MOLALLA FIRE STATION 82 - PRE-ENGINEERED METAL BUILDING

LAND USE REVIEW : 05/01/26



VICINITY MAP  
NOT TO SCALE



SITE AREA MAP  
NOT TO SCALE

Molalla RFPD

Project  
**Molalla Fire Station  
82 - PEMB**  
320 N Molalla Avenue  
Molalla, OR 97038

Mechanical/Electrical

CLIENT	PROJECT MANAGER	ARCHITECTURAL	CIVIL ENGINEERING

--	--	--	--

### INDEX OF DRAWINGS

#### GENERAL

G0.01 TITLE SHEET AND DRAWING INDEX

#### CIVIL DRAWINGS

C1.10 SITE PLAN  
C1.20 GRADING PLAN  
C1.30 UTILITY PLAN

#### LANDSCAPE DRAWINGS

L1.10 LANDSCAPE PLAN

#### ARCHITECTURAL DRAWINGS

A1.10 PEMB BUILDING PLANS  
A2.10 BUILDING ELEVATIONS  
A3.10 BUILDING AND WALL SECTIONS

### DRAWING CRITERIA

ALL DRAWINGS ARE IDENTIFIED BY TWO DIGITS AS FOLLOWS:

- A. CATEGORY LETTER REFERRING TO THE DISCIPLINE OR MAJOR DIVISION.
  - G. GENERAL
  - C. CIVIL
  - A. ARCHITECTURAL
  - S. STRUCTURAL
  - M. MECHANICAL
  - P. PLUMBING
- B. SUB-CATEGORY NUMBER REFERRING TO TYPE OF DRAWING OR GROUPING.
  - 0. TYPICAL CONDITIONS
  - 1. PLANS
  - 2. EXTERIOR ELEVATIONS
  - 3. BUILDING / WALL SECTIONS
  - 4. ENLARGED PLANS AND INTERIOR ELEVATIONS
  - 5. DETAILS

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OR REPRODUCED IN ANY MANNER  
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#### REVISION SCHEDULE

Delta	Issued As	Issue Date

SHEET TITLE:  
**TITLE SHEET  
AND DRAWING  
INDEX**

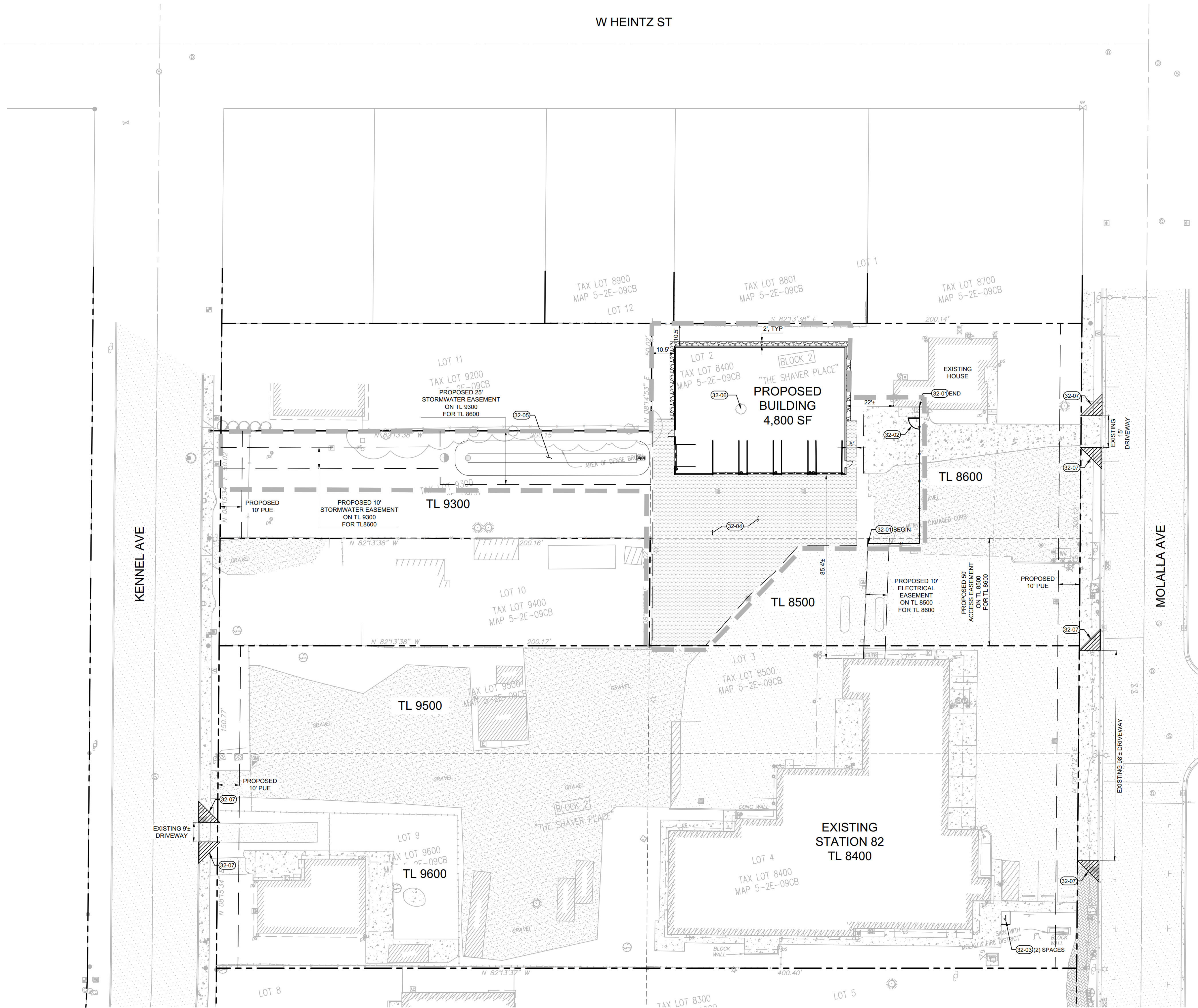
SHEET

# G0.01

JOB NO. **2250404.00**

LAND USE REVIEW 05/01/26

W HEINTZ ST



LEGEND

	EXISTING	PROPOSED
SITE BOUNDARY	---	---
LOT LINE	---	---
LIMITS OF DISTURBANCE	---	---
ACCESS EASEMENT	---	---
STORMWATER EASEMENT	---	---
ELECTRICAL EASEMENT	---	---
ASPHALT PAVING	[Pattern]	[Pattern]
GRAVEL	[Pattern]	[Pattern]

KEYNOTES

- 32-01 SLATTED CHAINLINK FENCE - MATCH EXISTING
- 32-02 MAN GATE
- 32-03 BIKE RACK (STAPLE BY HUNT CO. OR SIMILAR, POWDER COAT FINISH, SURFACE MOUNT) ON 4' x 6' CONCRETE PAD
- 32-04 ASPHALT PAVING - 3.5" AC OVER 12" AGGREGATE BASE OVER COMPACTED SUBGRADE PER "REPORT OF GEOTECHNICAL ENGINEERING SERVICES" BY COLUMBIA WEST
- 32-05 FILTRATION RAIN GARDEN - 18" GROWING MEDIUM OVER 3" OF 3/4" - 1/2" OPEN GRADED AGGREGATE OVER 18" 1/2" TO 3/4" WASHED DRAIN ROCK WITH 4" PERFORATED DRAIN PIPE 30 MIL PVC WATERPROOF LINER
- 32-06 EXISTING TREE TO BE REMOVED
- 32-07 VISION CLEARANCE TRIANGLE PER MOLALLA MUNICIPAL CODE SECTION 17-3.3.030.G

SITE DATA

	AREA (SF)	AREA (AC)	COVERAGE
PROPERTY AREA	100,126	2.30	
AREA OF DISTURBANCE	13,110	0.30	13.1%
IMPERVIOUS AREA	19,595	0.45	19.6%
BUILDING AREA	52,681	1.21	52.6%
TOTAL IMPERVIOUS AREA	72,276	1.66	72.2%
LANDSCAPE AREA	27,850	0.64	27.8%

Molalla RFPD #73

Molalla RFPD #73  
Fire Station 82  
320 N Molalla Avenue  
Molalla, OR 97038

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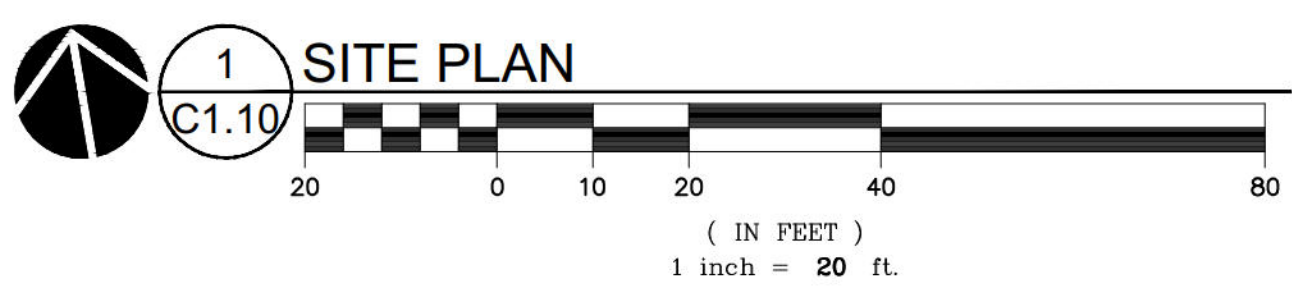
REVISION SCHEDULE		
Delta	Issued As	Issue Date

SHEET TITLE:  
SITE PLAN

SHEET

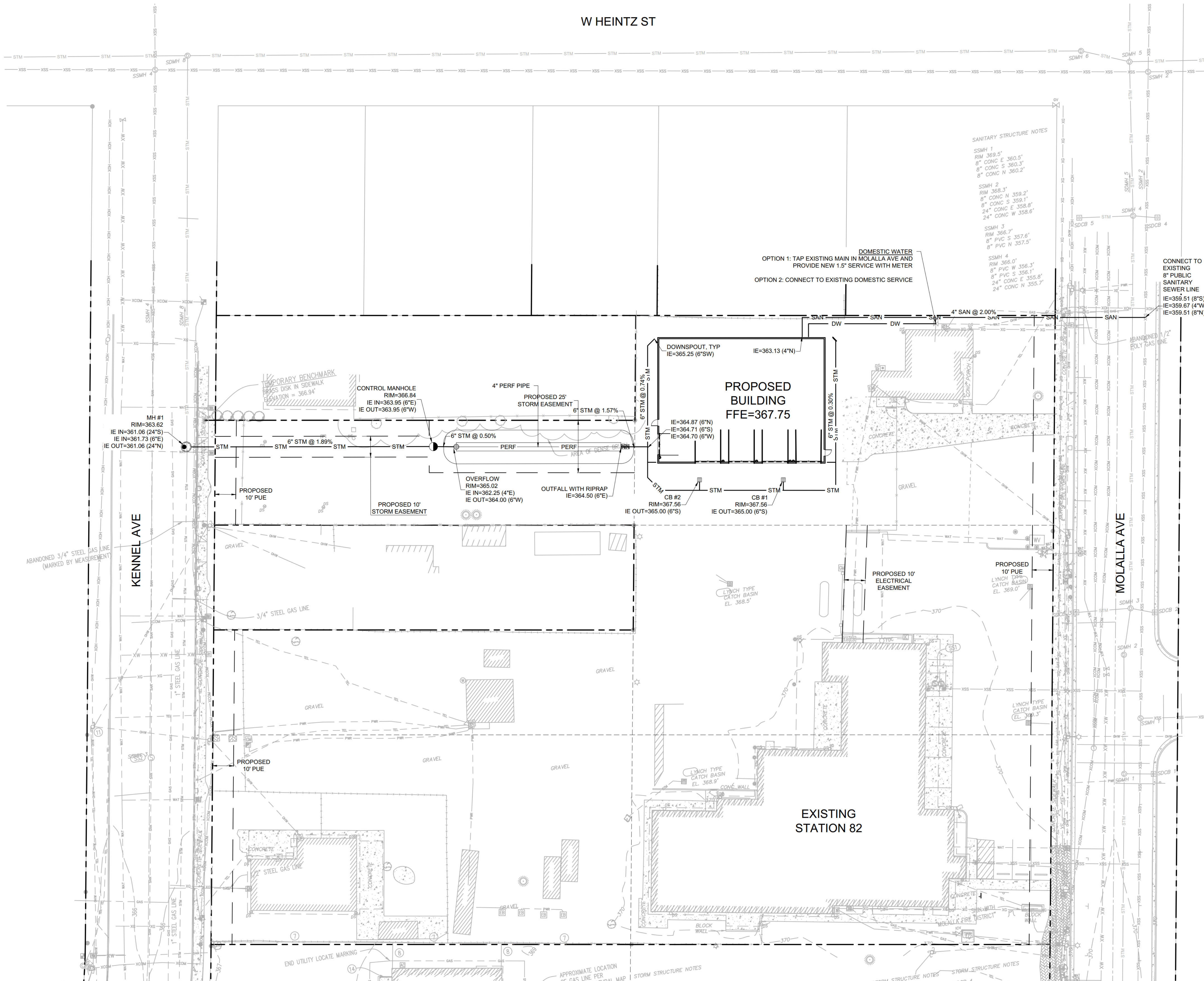
C1.10

JOB NO. 2250404.00





W HEINTZ ST



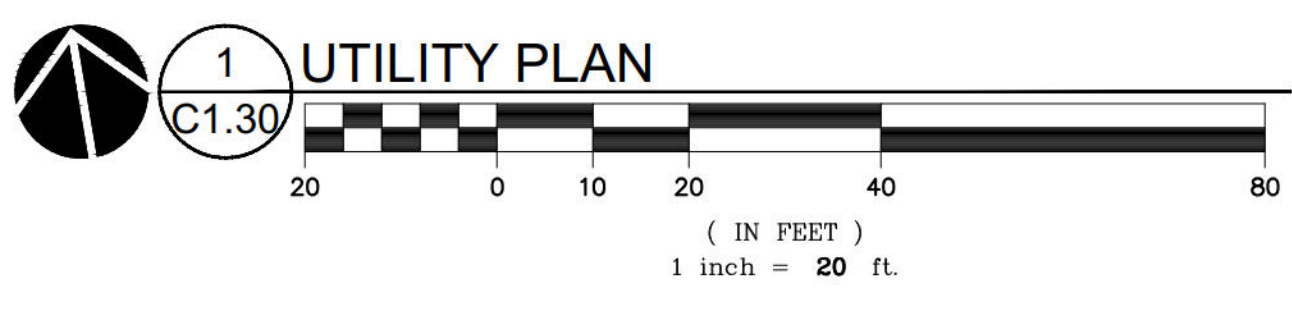
Molalla RFPD #73

Molalla RFPD #73  
Fire Station 82  
320 N Molalla Avenue  
Molalla, OR 97038

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REVISION SCHEDULE		
Delta	Issued As	Issue Date

SHEET TITLE:  
**UTILITY PLAN**



SHEET

**C1.30**

JOB NO. 2250404.00

PLANT SCHEDULE + LEGEND

SYMBOL	BOTANICAL / COMMON NAME	SIZE	SPACING
<b>SHRUBS</b>			
⊙	CHOISYA TERNATA 'SUNDANCE' SUNDANCE MEXICAN MOCK ORANGE	2 GAL.	48" o.c.
⊙	MAHONIA AQUIFOLIUM 'COMPACTA' COMPACT OREGON GRAPE	2 GAL.	48" o.c.
⊙	PHILADELPHUS LEWISII WILD MOCK ORANGE	2 GAL.	48" o.c.
⊙	RIBES SANGUINEUM 'KING EDWARD VII' RED FLOWERING CURRANT	2 GAL.	48" o.c.
<b>GROUND COVERS</b>			
⊙	GAULTHERIA SHALLON SALAL	1 GAL.	24" o.c.

- STORMWATER PLANTING AREA WITH SHRUBS (2 GAL.) AND GROUND COVER (1 GAL.) IN 18" MIN. STORMWATER SOIL
- EXISTING LANDSCAPE - REPAIR TO PRE-EXISTING CONDITIONS, SEED TO LIMITS OF DISTURBANCE

LANDSCAPE NOTES

GENERAL

- CONTRACTOR SHALL CONFIRM ALL DIMENSIONS AND EXISTING CONDITIONS PRIOR TO COMMENCING WORK AND PROMPTLY NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES BETWEEN ACTUAL SITE CONDITIONS AND THE CONTRACT DOCUMENTS.
- CALL BEFORE YOU DIG. CONTRACTOR SHALL VERIFY INVERT ELEVATIONS OF ALL UNDERGROUND UTILITIES AND NOTIFY LANDSCAPE ARCHITECT IF THERE ARE ANY DISCREPANCIES WITH PLANTING ROOT ZONES. TO REQUEST LOCATES FOR PROPOSED EXCAVATION CALL 1-800-333-2344 (OR 811) IN OREGON.
- NOTIFY THE OWNER OR OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES OR CONFLICTS WITH EXISTING CONDITIONS PRIOR TO COMMENCEMENT OF ANY WORK.
- CONTRACTOR IS RESPONSIBLE FOR DETERMINING MEANS AND METHODS FOR CONSTRUCTION. THESE DRAWINGS MAY INDICATE A LIMIT OF PROPOSED IMPROVEMENTS, LIMITS OF DEMOLITION, ETC FOR DELINEATION OF EXPECTED EXTENTS OF DISTURBANCE. HOWEVER FINAL IMPACT SHALL BE DETERMINED IN THE FIELD. SHOULD LIMITS OF DISTURBANCE EXCEED BOUNDARIES DEFINED IN DRAWINGS, CONTRACTOR SHALL CONTACT THE OWNER'S REPRESENTATIVE AND LANDSCAPE ARCHITECT FOR RESOLUTION.
- DAMAGE TO EXISTING CONCRETE CURB, ASPHALT PAVING, OR OTHER STRUCTURE SHALL BE REPAIRED OR REPLACED TO PRE CONSTRUCTION CONDITIONS.
- CONTRACTOR SHALL COORDINATE WITH THE OWNER ANY DISRUPTION TO VEHICULAR CIRCULATION PRIOR TO COMMENCEMENT OF ANY WORK.
- THE DRAWINGS AND SPECIFICATIONS ARE DESIGNED TO BE COMPLEMENTARY TO ONE ANOTHER AND IMPLIED TO CORRESPOND WITH ONE ANOTHER. ANY DISCREPANCIES SHOULD BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT FOR RESOLUTION IMMEDIATELY.

PLANT PROTECTION AND REMOVAL

- ALL EXISTING TREES, PLANTS, AND ROOTS SHALL BE PROTECTED FROM DAMAGE FROM ANY CONSTRUCTION PREPARATION, REMOVAL OR INSTALLATION ACTIVITIES WITHIN AND ADJACENT TO PROJECT LIMITS.
- LOCATION OF EXISTING TREES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO COMMENCEMENT OF WORK. TREES INDICATED TO BE REMOVED SHALL ALSO INCLUDE COMPLETE REMOVAL OF STUMP AND ROOTS AND FILLING IN DEPRESSION WITH SUITABLE SOIL FILL.
- IF DISTURBANCE IS NECESSARY AROUND EXISTING TREES, CONTRACTOR SHALL PROTECT THE CROWN AND ALL WORK WITHIN THE TREE CRITICAL ROOT ZONE SHALL BE LIMITED TO THE USE OF HAND TOOLS AND MANUAL EQUIPMENT ONLY.
- EXISTING AREAS PROPOSED FOR NEW PLANT MATERIAL SHALL BE CLEARED AND LEGALLY DISPOSED UNLESS SO NOTED.

PLANTING

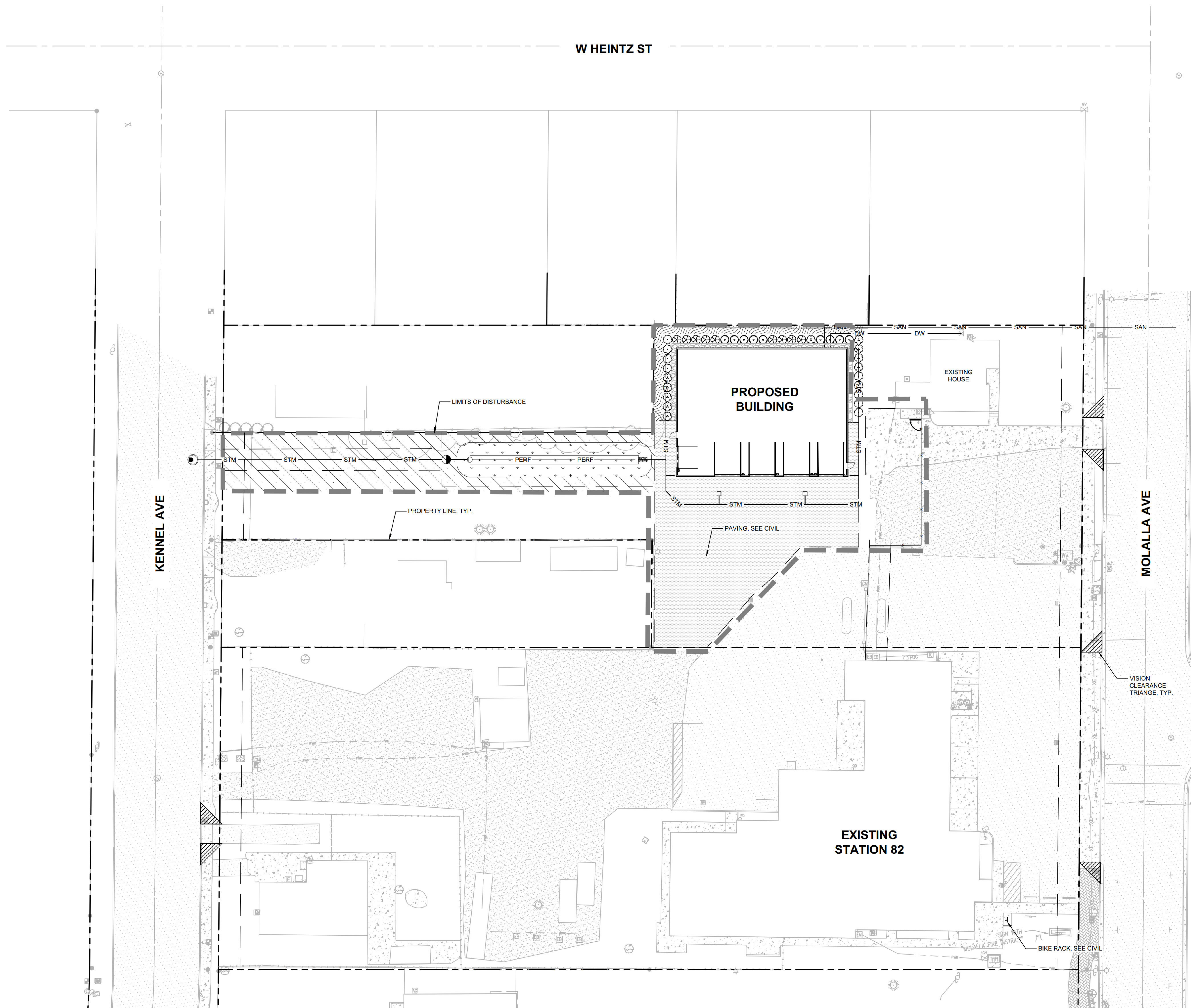
- ALL PLANT MATERIAL SHALL BE HEALTHY NURSERY STOCK, WELL BRANCHED AND ROOTED, FULL FOLIAGE, FREE FROM INSECTS, DISEASES, WEEDS, WEED ROT, INJURIES AND DEFECTS WITH NO LESS THAN MINIMUMS SPECIFIED IN AMERICAN STANDARDS FOR NURSERY STOCK, ANSI Z60.1-2004.
- DO NOT PLANT TREES ABOVE WATERLINES, UTILITIES, OR OTHER UNDERGROUND PIPING.
- REPLACE, REPAIR AND RESTORE DISTURBED LANDSCAPE AREAS DUE TO GRADING, TRENCHING OR OTHER REASONS TO PRE-CONSTRUCTION CONDITION AND PROVIDE MATERIAL APPROVED BY THE OWNER AND OWNER'S REPRESENTATIVE.
- A SOILS ANALYSIS, BY AN INDEPENDENT SOILS TESTING LABORATORY RECOGNIZED BY THE STATE DEPARTMENT OF AGRICULTURE, SHALL BE USED TO RECOMMEND AN APPROPRIATE PLANTING SOIL AND/OR SPECIFIED SOIL AMENDMENTS.
- TOPSOIL SHALL BE AMENDED AS RECOMMENDED BY AN INDEPENDENT SOILS TESTING LABORATORY AND AS OUTLINED IN THE SPECIFICATION.
- ALL LANDSCAPED AREAS SHALL BE COVERED BY A LAYER OF ORGANIC MULCH TO A MINIMUM DEPTH OF 2-INCHES.

IRRIGATION

- UNLESS OTHERWISE INDICATED, ALL NEW LANDSCAPE AREAS TO BE IRRIGATED WITH A FULLY AUTOMATIC UNDERGROUND IRRIGATION SYSTEM. PROVIDE LOOP SYSTEM FOR OPTIMUM EFFICIENCY.
- CONTRACTOR TO EVALUATE AND CONNECT TO EXISTING IN-GROUND IRRIGATION SYSTEM.
- CONTRACTOR SHALL SUBMIT SHOP DRAWINGS (IRRIGATION PLANS) TO LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION. DRAWINGS TO INDICATE HEAD TYPE, GALLONS PER MINUTE, LATERAL LINES, AND BE AT MINIMUM SCALE OF 1"=20'
- CONTRACTOR TO DETERMINE STATIC WATER PRESSURE AT THE P.O.C. PRIOR TO PREPARING SHOP DRAWINGS.
- CONTRACTOR SHALL ESTABLISH MINIMUM PRESSURE AND MAXIMUM DEMAND REQUIREMENTS FOR IRRIGATION SYSTEM DESIGN, AND PROVIDE INFORMATION IN AN IRRIGATION SCHEDULE.
- IRRIGATION SYSTEM AS DESIGNED AND INSTALLED SHALL PERFORM WITHIN THE TOLERANCES AND SPECIFICATIONS OF THE SPECIFIED MANUFACTURERS.
- SYSTEM SHALL BE DESIGNED TO SUPPLY MANUFACTURER'S SPECIFIED MINIMUM OPERATING PRESSURE TO FARTHEST EMITTER FROM WATER METER.
- SYSTEM SHALL PROVIDE HEAD TO HEAD COVERAGE WITHOUT OVERSPRAY ONTO BUILDING, FENCES, SIDEWALKS, PARKING AREAS, OR OTHER NON-VEGETATED SURFACES.
- ALL IRRIGATION PIPE MATERIAL AND INSTALLATION SHALL CONFORM TO APPLICABLE CODE FOR PIPING AND COMPONENT REQUIREMENTS.
- PROVIDE SLEEVING AT ALL AREAS WHERE PIPE TRAVELS UNDER CONCRETE OR HARD SURFACING.
- VALVES SHALL BE WIRED AND INSTALLED PER MANUFACTURER'S RECOMMENDED INSTALLATION PROCEDURES AND CONNECTED TO THE IRRIGATION CONTROLLER.
- CONTRACTOR TO UTILIZE EXISTING IRRIGATION CONTROLLER.
- ZONE THE FOLLOWING AREAS SEPARATELY: TEMPORARY AREAS, PERMANENT LANDSCAPE AREAS, STORMWATER PLANTING.
- QUICK COUPLERS TO BE PLACED EVERY 150 LINEAR FEET MAX.
- IRRIGATION SHALL BE WINTERIZED THROUGH LOW PRESSURE, HIGH VOLUME AIR BLOWOUT CONNECTION THROUGH QUICK COUPLER.
- THE SYSTEM SHALL BE GRAVITY DRAINED. THE CONTRACTOR SHALL PROVIDE APPROPRIATE MANUAL DRAINS AT LOW POINTS.

STORMWATER FACILITY PLANTING

- THE DESIGN FOR PLANTINGS SHALL MINIMIZE THE NEED FOR HERBICIDES, FERTILIZERS, PESTICIDES, OR SOIL AMENDMENTS AT ANY TIME BEFORE, DURING, AND AFTER CONSTRUCTION AND ON A LONG TERM BASIS.
- PLANTS ARE SELECTED AND PLANTED TO MINIMIZE THE NEED FOR MOWING, PRUNING, AND IRRIGATION ONCE ESTABLISHED.
- PLANTS ARE SELECTED TO BE LOW MAINTENANCE, HARDY, AND DROUGHT TOLERANT TO REDUCE FUTURE MAINTENANCE REQUIREMENTS.
- VEGETATION SHALL BE INSTALLED SUCH THAT 100 PERCENT VEGETATIVE COVER IS ACHIEVED THROUGH A MIX OF HERBACEOUS PLANTS, GROUND COVER, AND SHRUBS AT THE END OF THE WARRANTY PERIOD.
- WOODY VEGETATION SHALL BE USED TO PROVIDE SHADE OVER STANDING WATER AND TO PROVIDE STRUCTURAL DIVERSITY WITHIN THE POND. SHRUBS AND TREES SHALL BE SITED TO PROMOTE LONG-TERM HEALTH AND SURVIVAL, MINIMIZE MAINTENANCE, AND PROTECT LINES OF SIGHT. SHRUBS AND TREES SHALL BE LOCATED TO ALLOW FOR MAINTENANCE ACCESS TO THE TREATMENT AREA.
- WOODY VEGETATION SHALL NOT BE PLANTED WITHIN 20 FEET OF INLET AND OUTLET STRUCTURES.
- PLANTING SHOULD OCCUR WHEN PLANTS ARE DORMANT, DEPENDING ON WHEN STORMWATER WILL BE ROUTED TO THE STORMWATER FACILITY. FOR BEST RESULTS, PLANT BETWEEN FEBRUARY 1 AND MAY 1 OR BETWEEN OCTOBER 1 AND DECEMBER 1. PLANTS MUST BE HEALTHY AND VIGOROUS.
- ALL PLANTS REQUIRE TEMPORARY IRRIGATION THROUGH ESTABLISHMENT PERIOD.



Molalla RFPD #73

Molalla RFPD #73  
Fire Station 82  
320 N Molalla Avenue  
Molalla, OR 97038

Project

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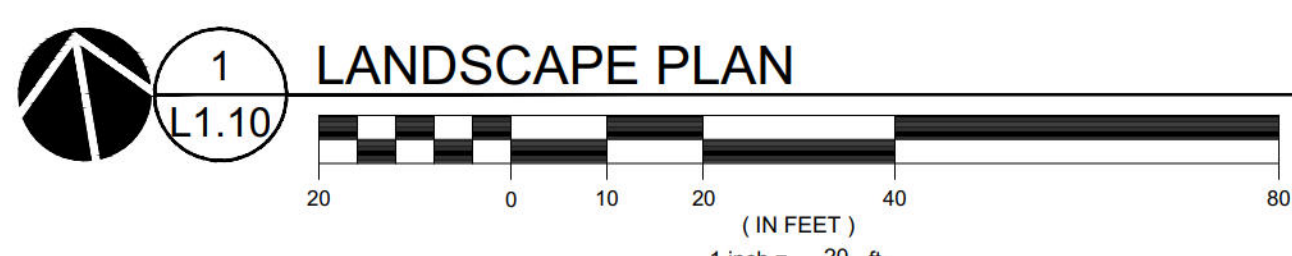
REVISION SCHEDULE		
Delta	Issued As	Issue Date

SHEET TITLE:  
**LANDSCAPE PLAN**

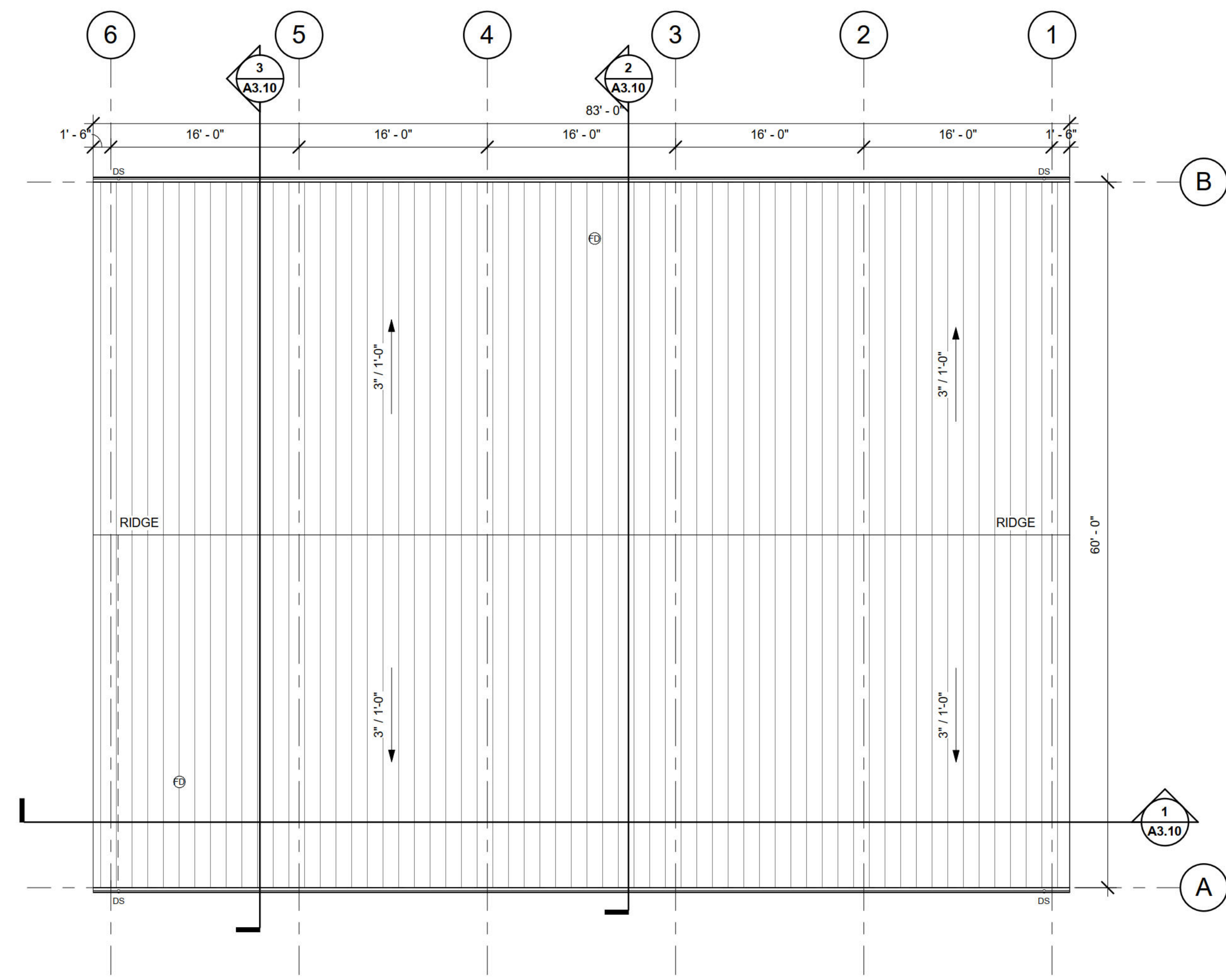
SHEET

**L1.10**

JOB NO. **2250404.00**



1 LANDSCAPE PLAN  
L1.10



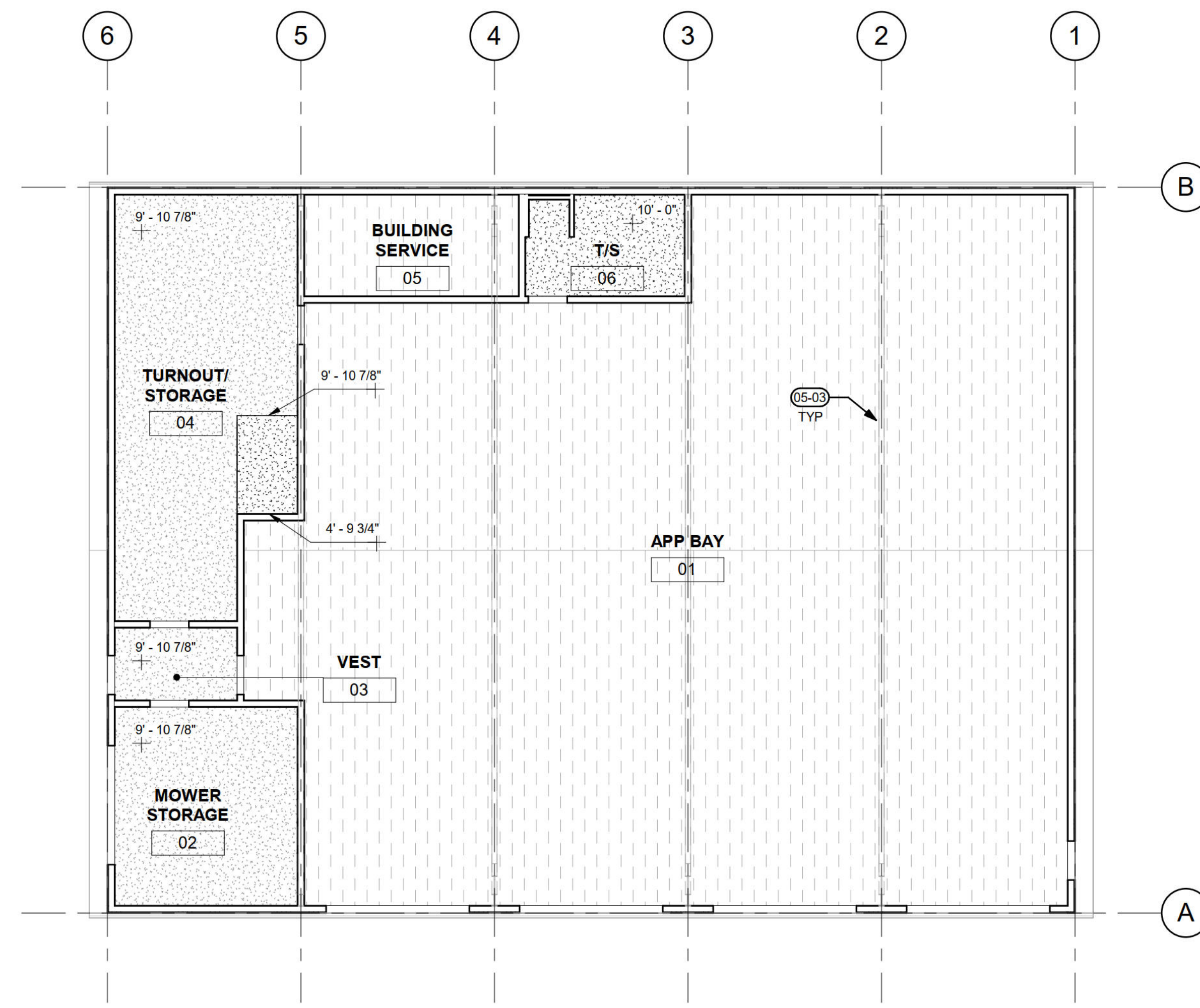
1 ROOF PLAN  
A1.10 / 1/8" = 1'-0"

GENERAL NOTES - ROOF PLANS

- A. CONTRACTOR TO PROVIDE COVERS, ENCLOSURES, AND/OR SEALANTS AT ALL ROOF PENETRATIONS, PIPES, CURBS, DUCTS, AND CONNECTIONS.
- B. SEE DETAIL [X(A5.XX)] FOR PIPE PENETRATIONS.

LEGEND - ROOF PLAN

- PEMB - METAL ROOF, SEE DETAIL
- GUTTER
- DOWNSPOUT, FINISH TO MATCH METAL SIDING



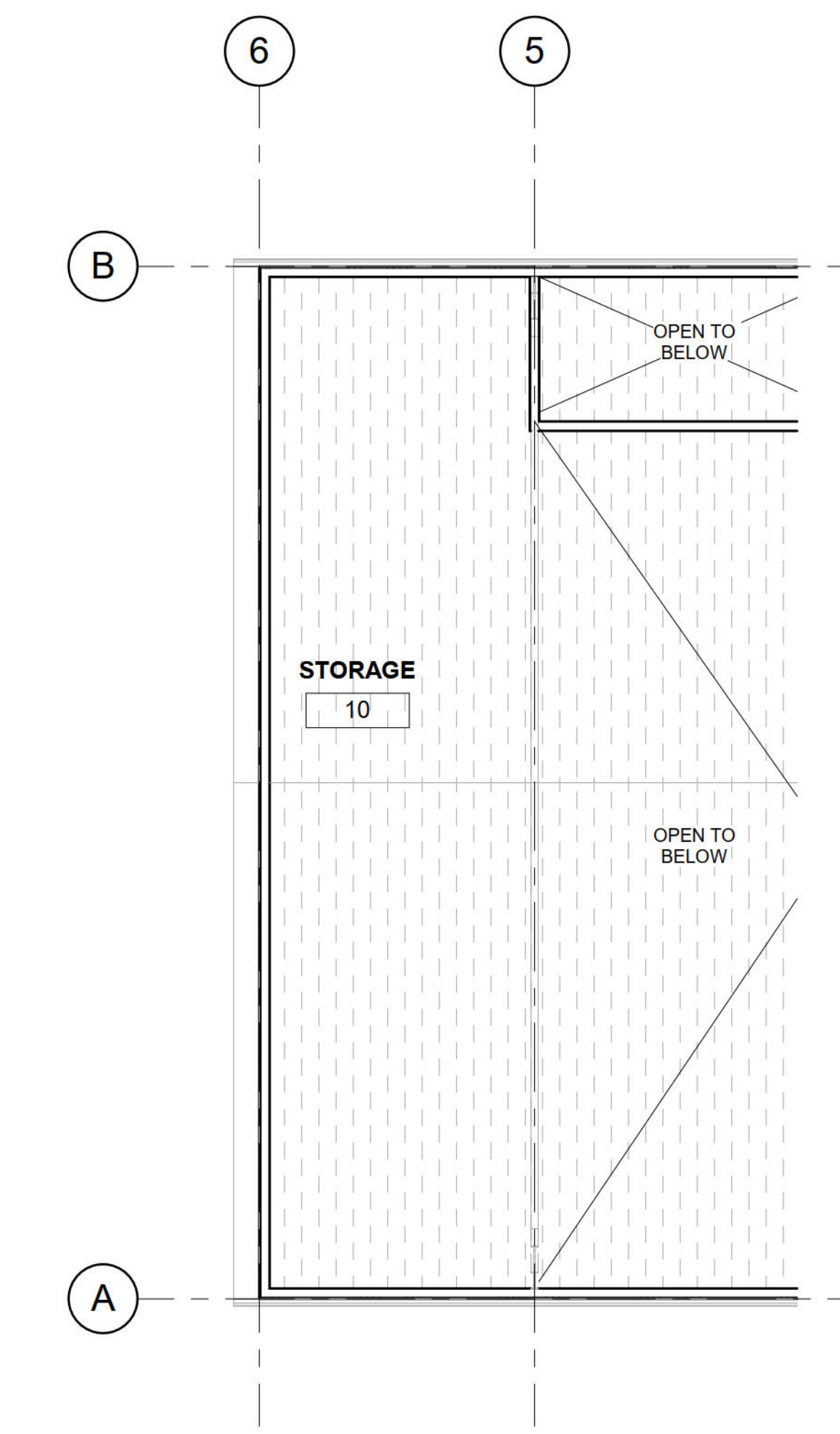
2 REFLECTED CEILING PLAN  
A1.10 / 1/8" = 1'-0"

GENERAL NOTES - REFLECTED CEILING PLANS

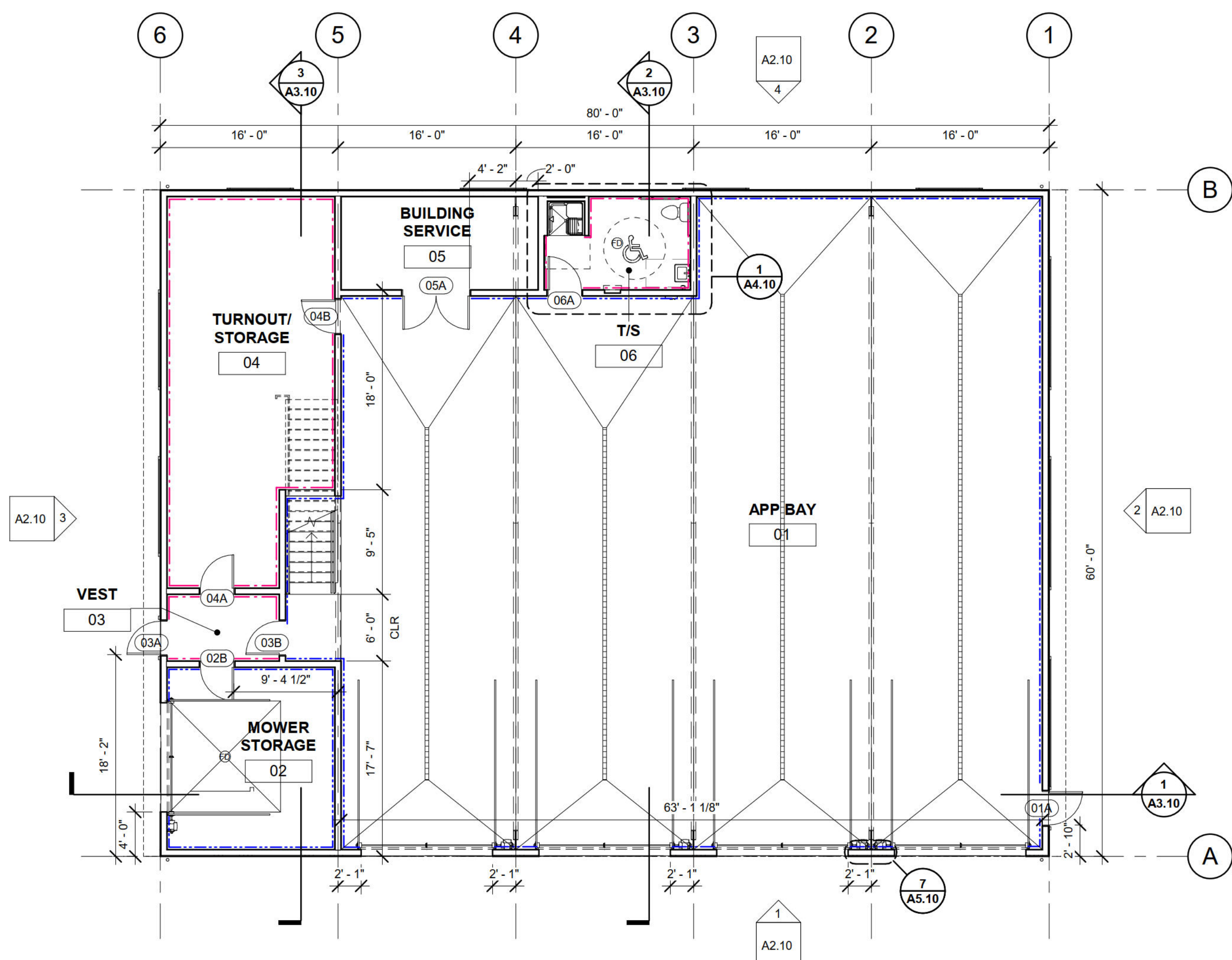
- A. AT AREAS OPEN TO STRUCTURE, PAINT METAL DECK, STRUCTURE, EXPOSED CONDUIT, MECHANICAL DUCT WORK, AND FIRE SPRINKLER COMPONENTS (EXCEPT HEADS), UNLESS OTHERWISE NOTED.
- B. FULLY CONCEALED SPRINKLER HEADS REQUIRED AT ALL GYPSUM BOARD CEILING ASSEMBLIES, WITH POWDERCOATED HEAD COVER PLATES FINISHED TO MATCH ADJACENT CEILING PAINT COLOR.

LEGEND - REFLECTED CEILING PLAN

- GYPSUM BOARD CEILING
- WHITE SKRIM / VAPOR BARRIER, SEE R1 / A0.03



3 REFLECTED CEILING PLAN - MEZZANINE  
A1.10 / 1/8" = 1'-0"



4 FIRST FLOOR PLAN  
A1.10 / 1/8" = 1'-0"

KEYNOTES

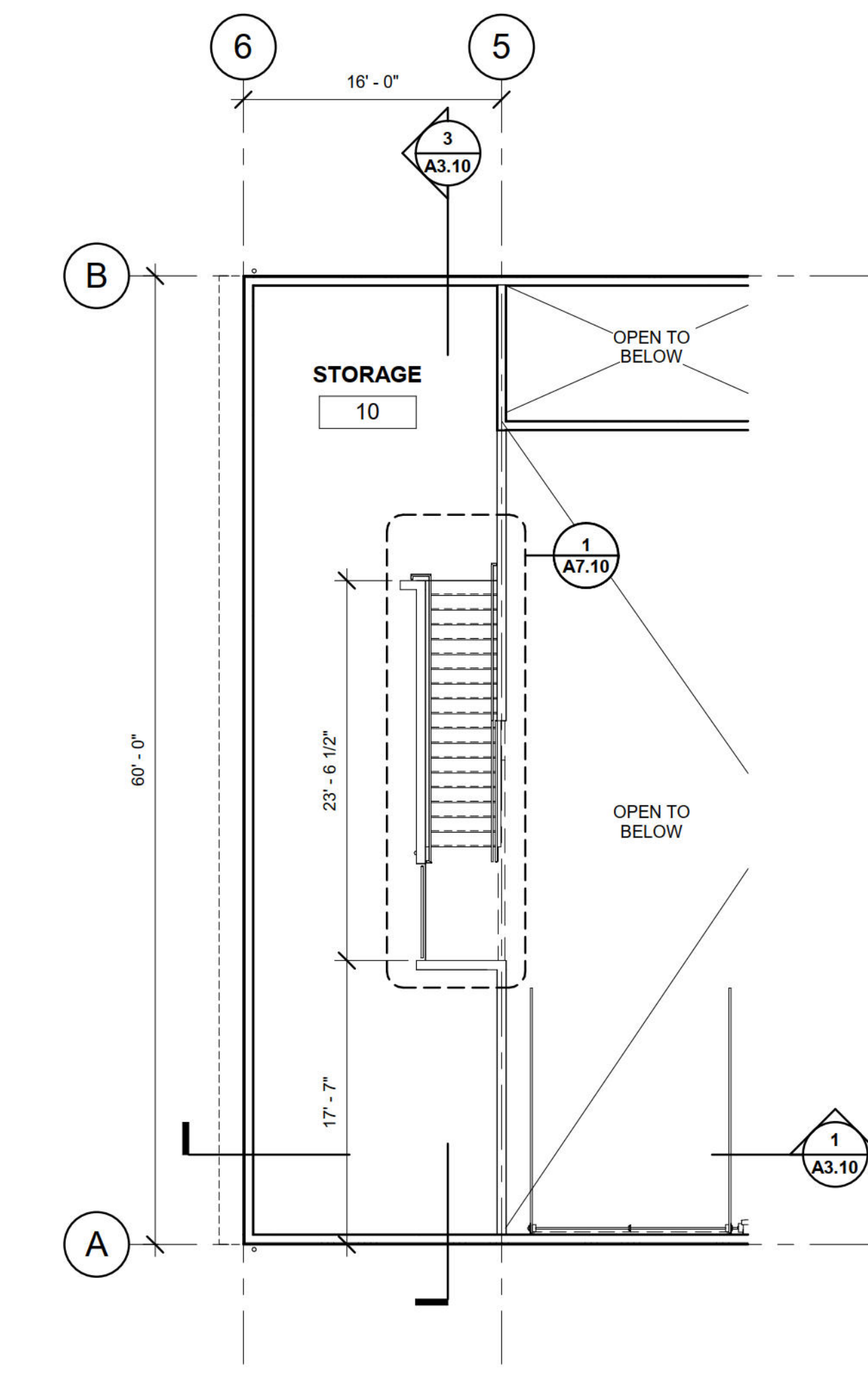
- 05-03 PEMB FRAME, PER PEMB MFR

GENERAL NOTES - PLANS

- A. SEE ARCHITECTURAL GENERAL NOTES ON G0.03 FOR ADDITIONAL INFORMATION
- B. SEE A0.03 FOR WALL TYPES

LEGEND - FLOOR PLAN

- PRE-ENGINEERED METAL BUILDING EXTERIOR ASSEMBLY, SEE SHEET A0.03
- INTERIOR PARTITION, SEE SHEET A0.03
- FRP WITH PVC WALL BASE, SEE 1 / A0.04
- FRP WITH RUBBER WALL BASE, SEE 1 / A0.04
- TRENCH DRAIN, SLOPE SLAB TO DRAIN; 1/4" / 12" MIN
- FLOOR DRAIN, SLOPE SLAB TO DRAIN; 1/4" / 12" MIN



5 MEZZANINE PLAN  
A1.10 / 1/8" = 1'-0"

Molalla RFPD

Project  
Molalla Fire Station  
82 - PEMB  
320 N Molalla Avenue  
Molalla, OR 97038

Mechanical/Electrical

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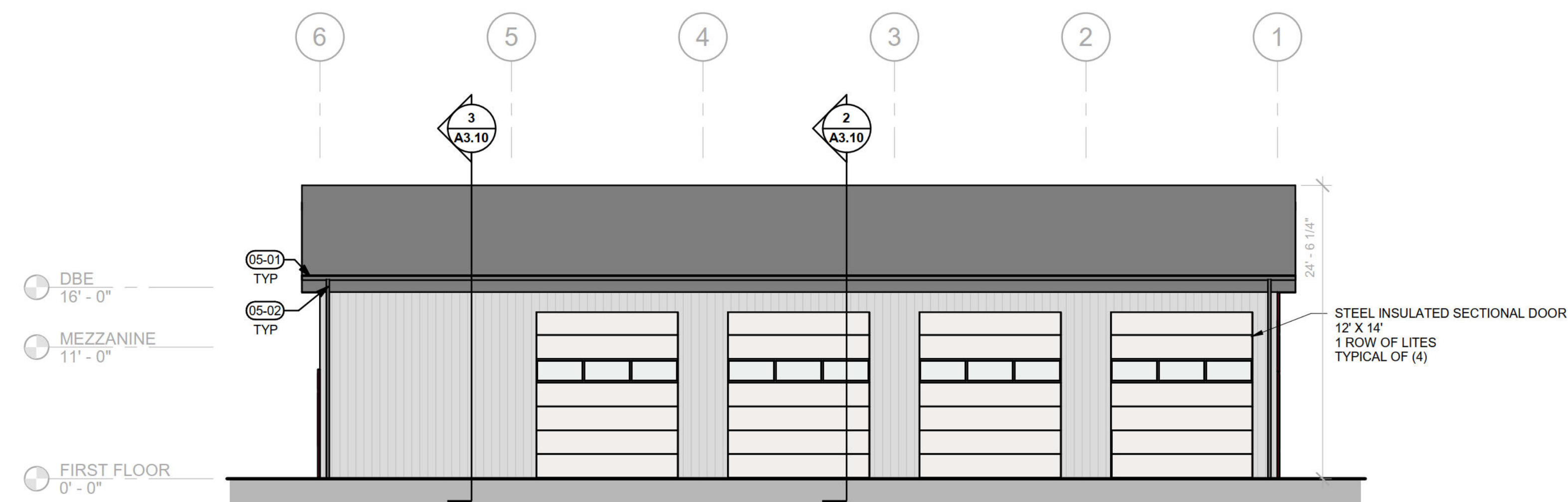
SHEET TITLE:  
PEMB  
BUILDING  
PLANS

SHEET

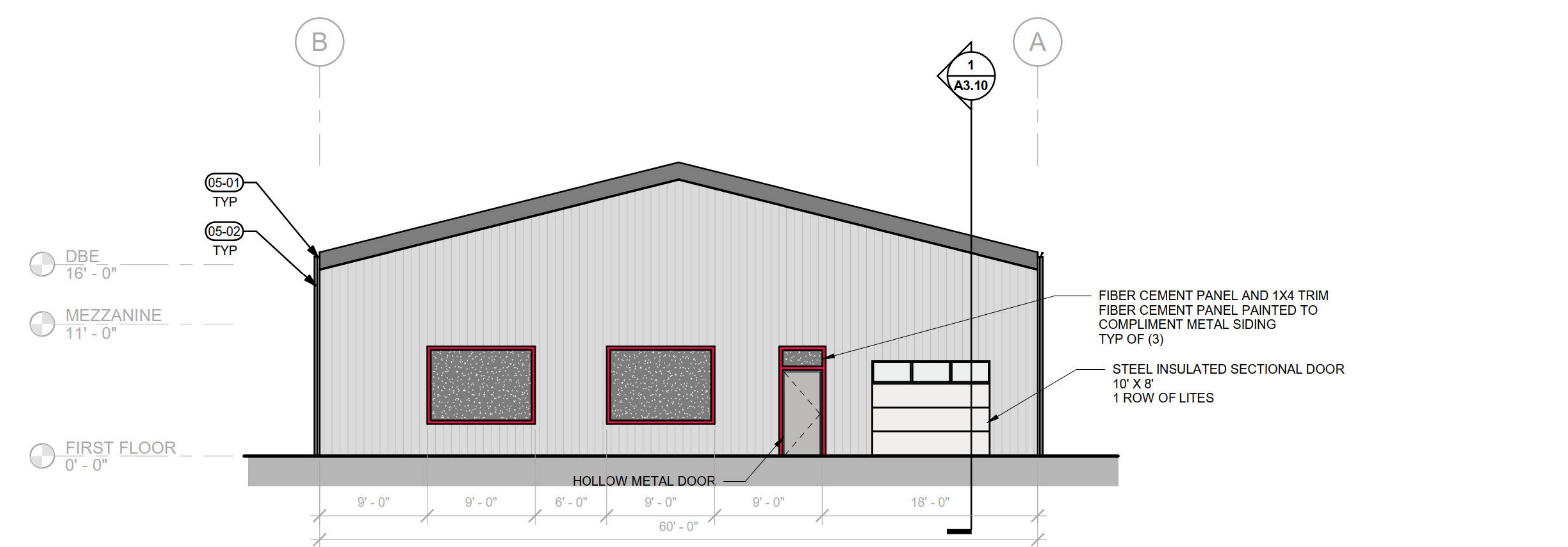
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JOB NO. 2250404.00

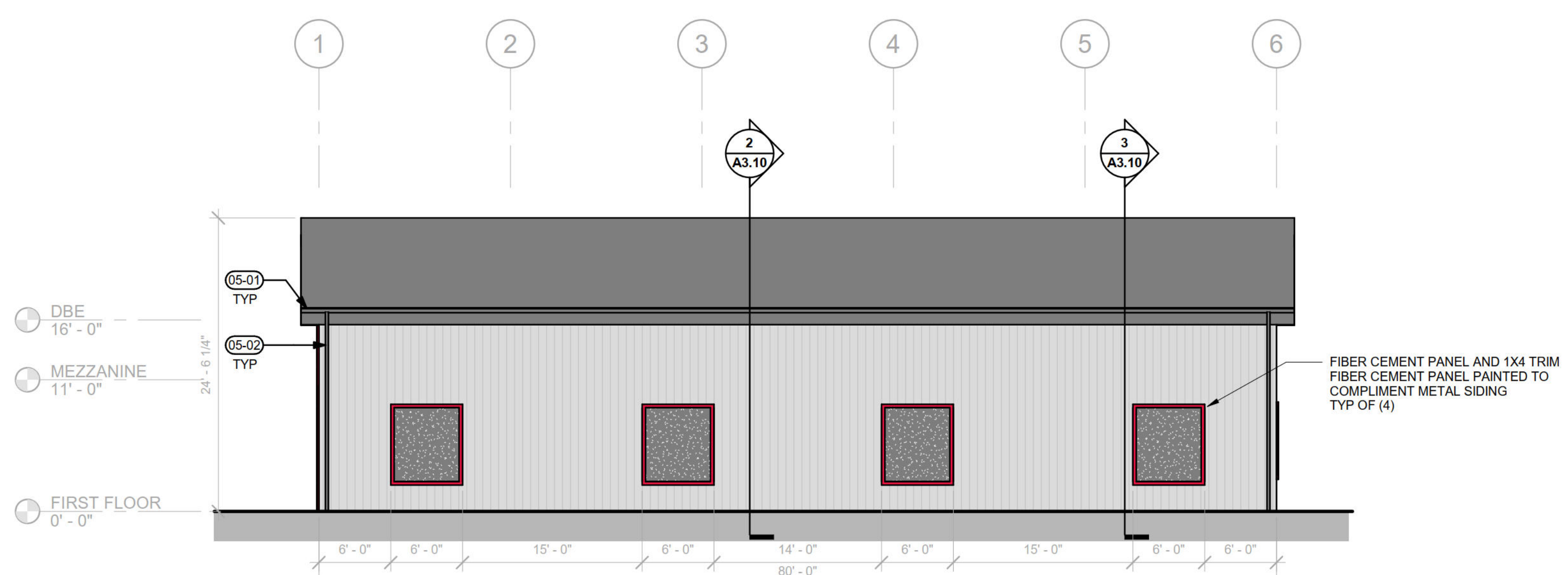
LAND USE REVIEW 05/01/26



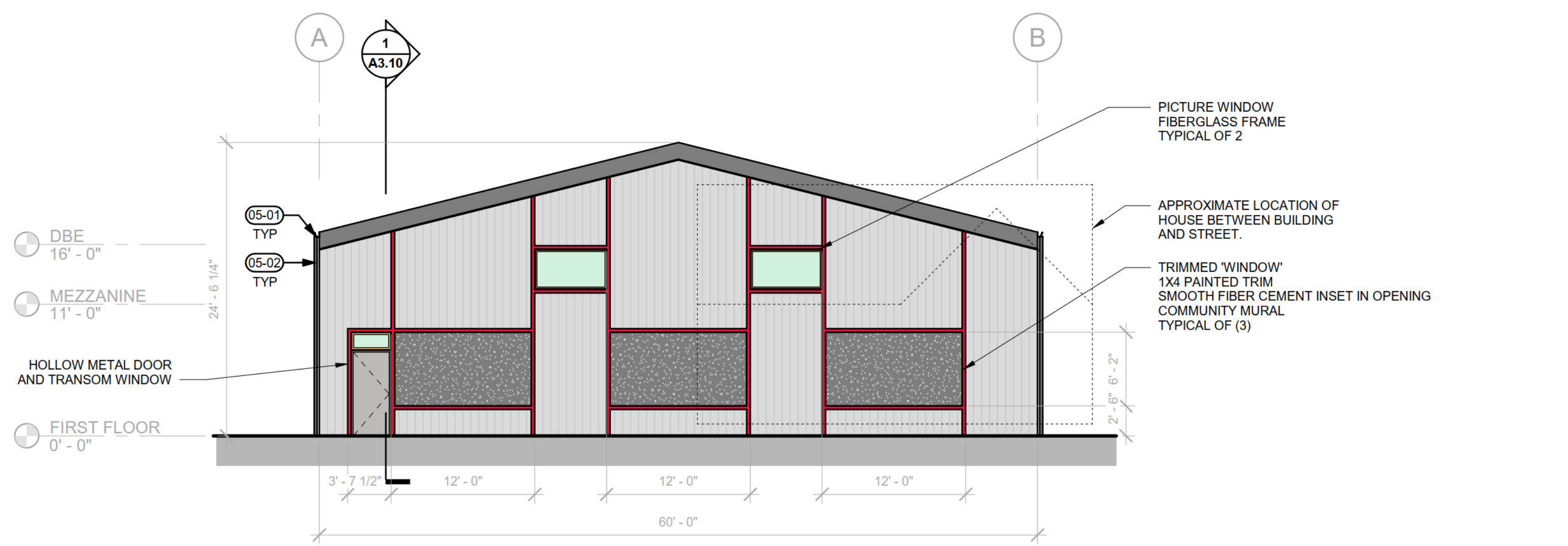
1 SOUTH ELEVATION  
A2.10 1/8" = 1'-0"



3 WEST ELEVATION  
A2.10 1/8" = 1'-0"



4 NORTH ELEVATION  
A2.10 1/8" = 1'-0"



2 EAST ELEVATION  
A2.10 1/8" = 1'-0"

LEGEND - BUILDING ELEVATIONS

- METAL ROOFING, TAYLOR METAL, STERLING GREY
- RIBBED VERTICAL METAL SIDING, TAYLOR METAL, GLACIER WHITE
- TRIM, FIBER CEMENT 1X4, SHERWIN WILLIAMS, CORAL BELLS (RED)
- WINDOW, SMOOTH FIBER CEMENT PANEL, STREET FACING - MURAL, OTHER SIDES - COLOR TO MATCH METAL ROOF

KEYNOTES

- 05-01 GUTTER, FINISH TO MATCH METAL SIDING
- 05-02 DOWNSPOUT, FINISH TO MATCH METAL SIDING. PROVIDE SPLASHBLOCK AT BASE

Molalla RFPD

Project  
**Molalla Fire Station 82 - PEMB**  
320 N Molalla Avenue  
Molalla, OR 97038

Mechanical/Electrical

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REVISION SCHEDULE		
Delta	Issued As	Issue Date

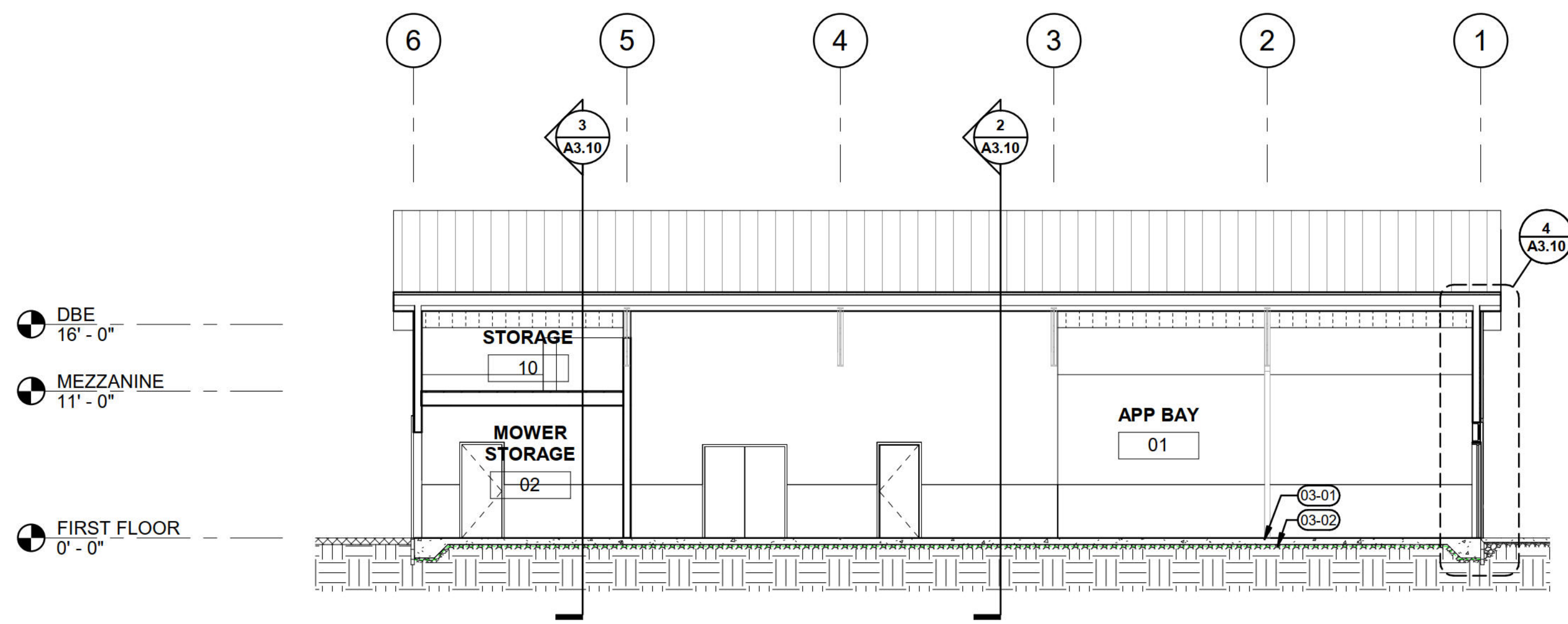
SHEET TITLE:  
**BUILDING ELEVATIONS**

SHEET

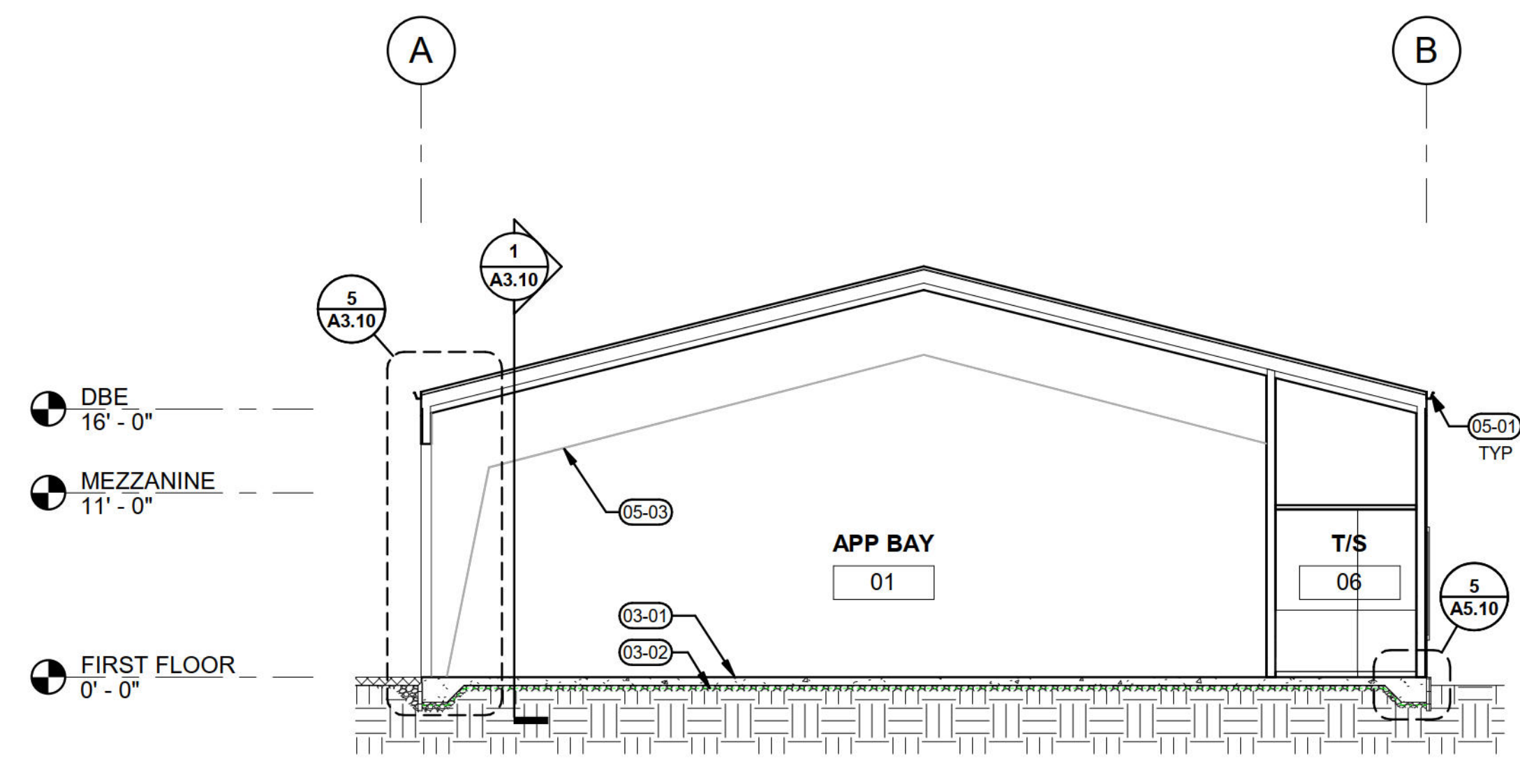
**A2.10**

JOB NO. **2250404.00**

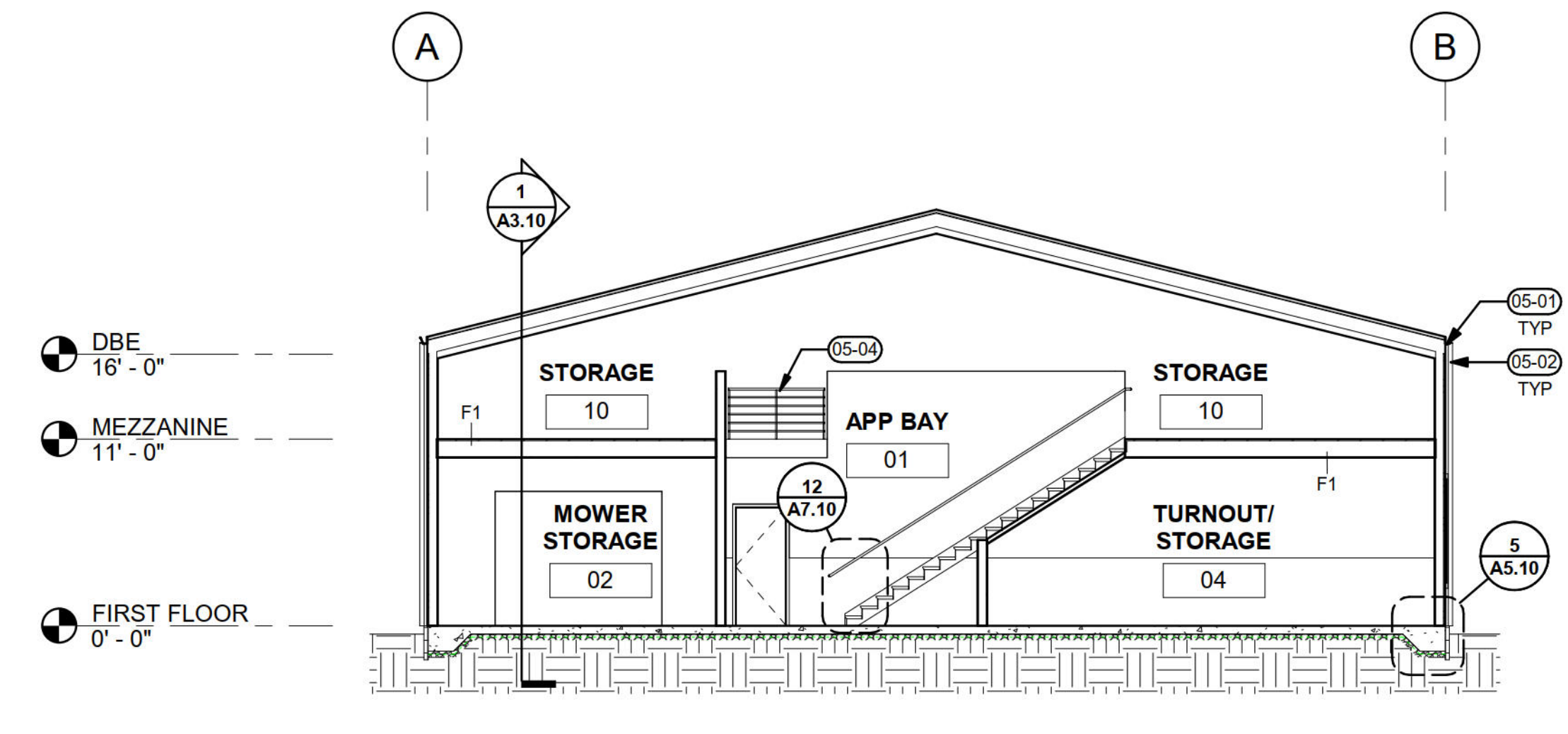
REVISION SCHEDULE		
Delta	Issued As	Issue Date



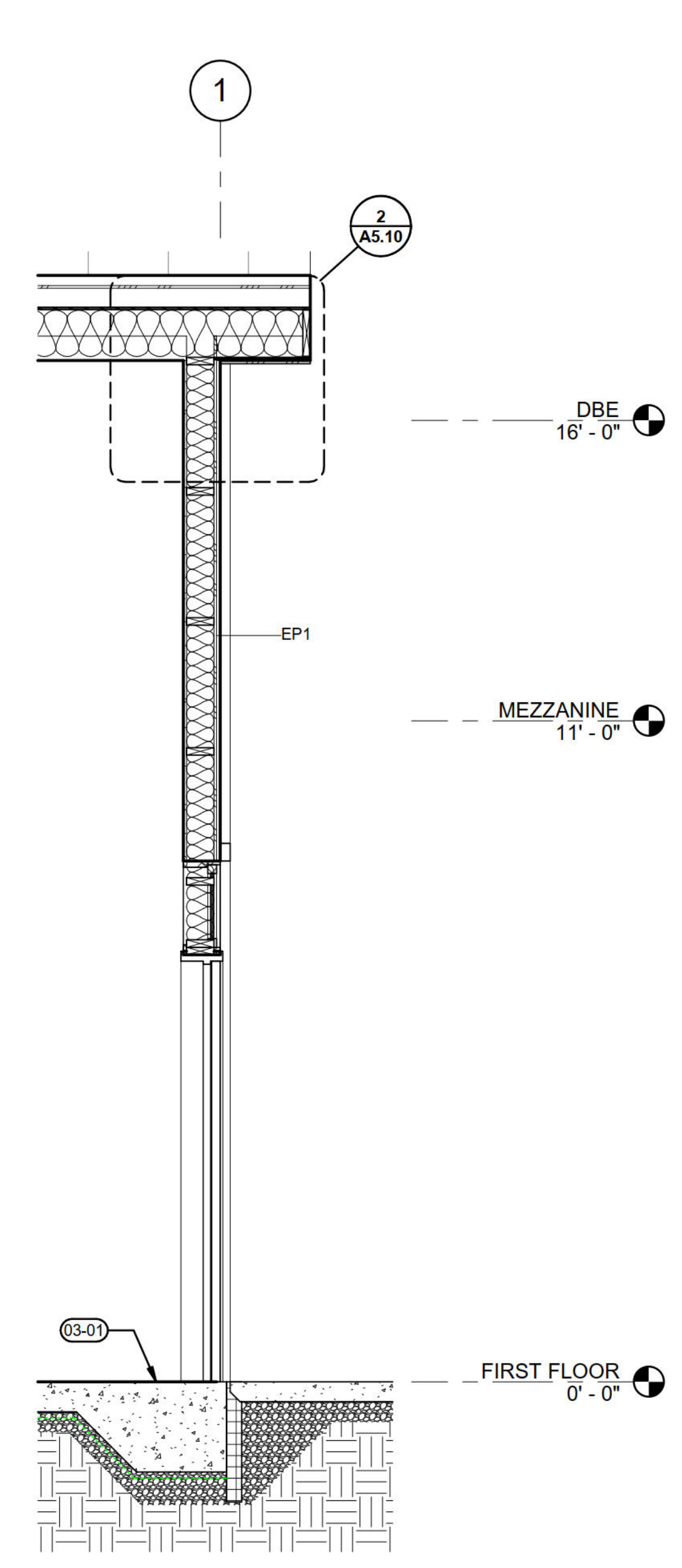
1 NORTH/SOUTH SECTION - APPARATUS BAY  
 A3.10 1/8" = 1'-0"



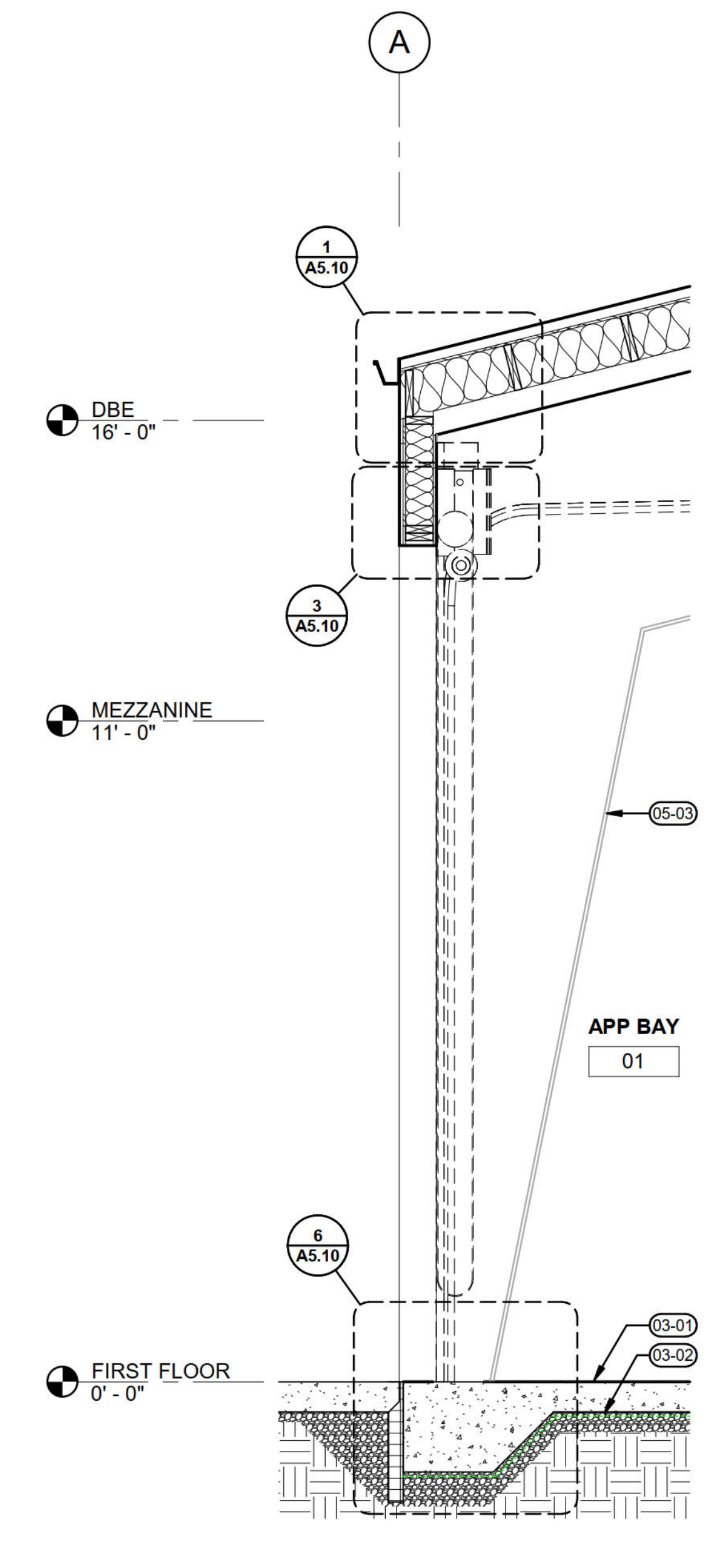
2 EAST/WEST SECTION - APPARATUS BAY  
 A3.10 1/8" = 1'-0"



3 EAST/WEST SECTION - MEZZANINE STAIR  
 A3.10 1/8" = 1'-0"



4 WALL SECTION @ PERSONNEL DOOR  
 A3.10 1/2" = 1'-0"



5 WALL SECTION @ OVERHEAD DOOR  
 A3.10 1/2" = 1'-0"

- KEYNOTES**
- 03-01 CONCRETE SLAB, PER PEMB MFR
  - 03-02 UNDERSLAB VAPOR BARRIER
  - 05-01 GUTTER, FINISH TO MATCH METAL SIDING
  - 05-02 DOWNSPOUT, FINISH TO MATCH METAL SIDING. PROVIDE SPLASHBLOCK AT BASE
  - 05-03 PEMB FRAME, PER PEMB MFR
  - 05-04 REMOVABLE GUARDRAIL

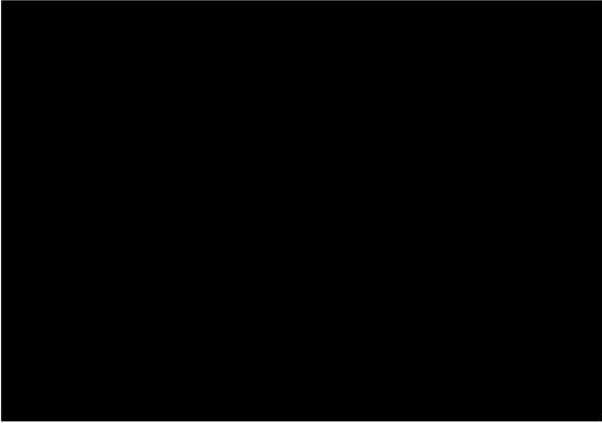


## MEMORANDUM

DATE: April 28, 2026

TO: [REDACTED]

FROM: Reah Flisakowski, PE | DKS Associates  
Hallie Turk, EI, RSP<sub>1</sub> | DKS Associates



SUBJECT: Molalla Fire Station Remodel - Transportation Analysis Letter DKS P#26490-000

DKS has prepared a Transportation Analysis Letter (TAL) to evaluate the potential transportation impacts associated with the proposed Molalla Fire Station 82 Remodel. Fire Station 82 is located at 320 N Molalla Avenue in Molalla, Oregon. The project will construct a new 6,000-square foot storage and maintenance building that will replace a small existing residential garage.

The purpose of this TAL is to provide the estimated vehicle trip generation for the proposed development and to evaluate the site’s compliance with applicable City standards.

### TRIP GENERATION

Trip generation is the method used to estimate the number of vehicles added to site roadways and the adjacent roadway network by a development during a specified period (i.e., a.m. and p.m. peak hours). Typically, trip generation is estimated using trip generation rates from the Institute of Transportation Engineers (ITE) Trip Generation Manual, which utilizes national land use data.

The proposed 6,000-square foot storage and maintenance building will consist of a prefabricated metal building and include a bathroom and equipment storage areas. The proposed building is intended to expand the fire station’s storage and maintenance capabilities. The proposed development will not accommodate additional employees or result in additional emergency response demand from the station. The development is not expected to generate any additional vehicle trips to the site, as shown in Table 1.

TABLE 1: PROJECT TRIP GENERATION

LAND USE	SIZE	AM PEAK TRIPS			PM PEAK TRIPS			WEEKDAY
		IN	OUT	TOTAL	IN	OUT	TOTAL	
STORAGE AND MAINTENANCE FACILITY	6,000 sq ft	0	0	0	0	0	0	0

## TIA REQUIREMENTS

---

According to the Molalla Code of Ordinances, a Traffic Impact Analysis (TIA) is required for developments that meet the following criteria<sup>1</sup>:

1. Developments that are expected to generate 25 or more peak hour trips. Based on the trip generation previously shown in Table 1, the development is not expected to generate new trips during any time period. The development does not meet this criterion.
2. Developments that are expected to impact intersections that currently fail to meet the City's level of service standards or intersections that operate near the level of service standards. The development does not meet this criterion.
3. Developments that are expected to significantly impact adjacent high-accident locations, areas that contain an identified safety concern, or near school zones. The development does not meet this criterion.
4. Developments that generate an increase in use of adjacent streets by heavy vehicles. The development does not meet this criterion.

Therefore, the development does not require a TIA, and a TAL is sufficient to support the development application.

## SITE PLAN REVIEW

This section reviews the site plan's compliance with standards outlined in the Molalla Transportation System Plan (TSP).

## SITE DESCRIPTION

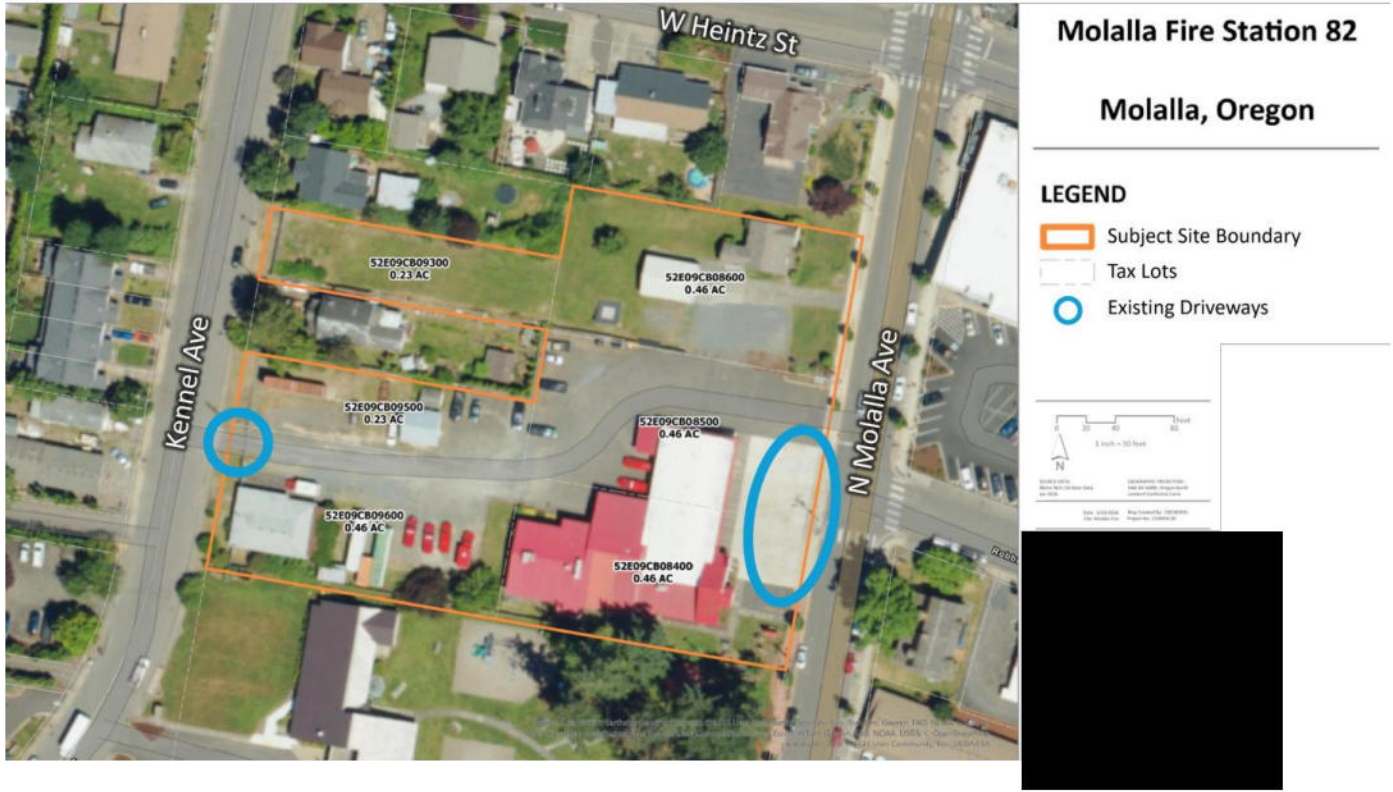
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The Molalla Fire Station 82 remodeling project will construct a new 6,000-square foot storage and maintenance facility, which will be a prefabricated metal building that includes a bathroom, mezzanine, and designated equipment storage area. The new facility will replace a small existing garage associated with the single-family home on the property that is owned by the fire station. The single-family home has a driveway access to Molalla Avenue located north of Robbins Street that will remain operational.

The project site is shown in Figure 1.

---

<sup>1</sup> City of Molalla Code of Ordinances, Section 17-3.6.020 Transportation Standards. Effective June 30, 2022.



**FIGURE 1: PROJECT SITE**

## ACCESS SPACING AND DRIVEWAY WIDTH

Access spacing<sup>2</sup> and driveway width<sup>3</sup> standards were evaluated based on standards provided in the City of Molalla TSP. The development is proposing to use the existing site accesses with no proposed changes. The site operates today with two driveways on Molalla Avenue and one gated driveway on Kennel Avenue.

- Kennel Avenue is classified as a neighborhood street. Private access driveways on neighborhood collectors must be spaced over 100 feet apart. Industrial driveways must be a minimum of 30 feet wide and a maximum of 40 feet wide.
  - There are multiple single-family driveways on both sides of Kennel Avenue that are located within 100 feet of the site access. These driveways serve very low volumes and have no reported safety or operational issues. The site driveway is gated and used infrequently to access the west portion of the site.
  - The existing Kennel Avenue driveway meets driveway spacing standards.
- Molalla Avenue is classified as an arterial facility. Private access driveways on arterials must be spaced over 150 feet apart. Industrial driveways must be a minimum of 30 feet wide and a maximum of 40 feet wide.
  - The existing north site driveway is located approximately 100 feet south of the driveway to the single-family home on the site property. The existing south site driveway is located approximately 110 feet north of a retail driveway on the east side of Molalla Avenue. These existing conditions do not meet access spacing standards however there are no reported safety or operational issues.
  - There are two driveways on Molalla Avenue, both serve emergency response vehicles stored in a five-bay garage. The north driveway is located at the west leg of the signalized Molalla Avenue/Robbins Street intersection. The southern Molalla Avenue driveway is located south of the Robbins Street intersection. The driveways are separated by an ADA ramp at a marked pedestrian crossing on Molalla Avenue. The existing Molalla Avenue driveways meet the driveway width standards.

The existing site driveways on Molalla Avenue and Kennel Avenue do not meet access spacing standards. The proposed development will not add any vehicle trips to the driveways, therefore no safety or operational impacts are anticipated and no mitigations are recommended.

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<sup>2</sup> Table 10: Minimum Intersection Spacing Standards, City of Molalla Transportation System Plan. Adopted September 26, 2018.

<sup>3</sup> Table 11: Private Access Driveway Width Standards, City of Molalla Transportation System Plan. Adopted September 26, 2018.

## INTERSECTION SIGHT DISTANCE

Kennel Avenue and Molalla Avenue both have a statutory speed limit of 25 mph, which corresponds to a design speed of 30 mph. For a design speed of 30 mph, sight distance standards outlined by the American Association of State Highway and Transportation Officials (AASHTO) require 335 feet of intersection sight distance in either direction.<sup>4</sup> This standard accounts for the sight distance needed to make a left turn maneuver from each of the driveways.

The preliminary sight distance triangles are illustrated in Figure 2.



**FIGURE 2: PRELIMINARY SIGHT DISTANCE AT SITE ACCESS DRIVEWAYS**

Based on preliminary sight distance evaluation at the existing site access driveways, the sight distance requirements are met and there are no safety concerns associated with sight distance at the driveways.

<sup>4</sup> Section 9.5. A Policy on Geometric Design of Highways and Streets, American Association of Highway and Transportation Officials (AASHTO), 7<sup>th</sup> Edition. Published 2018.

## TRANSPORTATION SYSTEM PLAN COMPLIANCE

The site frontages on Kennel Avenue and Molalla Avenue were evaluated for compliance with street design standards, including right-of-way width, lane widths, bike lanes or on-street parking, and sidewalks. The standards are provided in the TSP which includes requirements from the Downtown Master Plan<sup>5</sup>.

- Kennel Avenue along the site frontage meets all neighborhood street cross-section requirements.
- Molalla Avenue along the site frontage meets all arterial street cross-section requirements.

Overall, no additional frontage improvements are required to meet street design standards.

## SUMMARY

The key findings of the TAL for the Molalla Fire Station 82 remodel are summarized below.

- The project will construct a new 6,000-square foot storage and maintenance facility at Fire Station 82 at 320 N Molalla Avenue. The new facility will consist of a prefabricated metal building that will replace a small existing residential garage.
- The proposed development is not expected to generate any new weekday a.m. peak hour trips, p.m. peak hour trips, or daily trips. Per the trip generation, the proposed development does not require a TIA based on the City of Molalla standards.
- Since both site driveways are existing with no reported safety or operational issues and the proposed development will not add any vehicle trips to the site, no mitigations are recommended to meet City access spacing standards.
- Based on preliminary sight distance evaluation at the existing site access driveways, the sight distance requirements are met and there are no safety concerns.

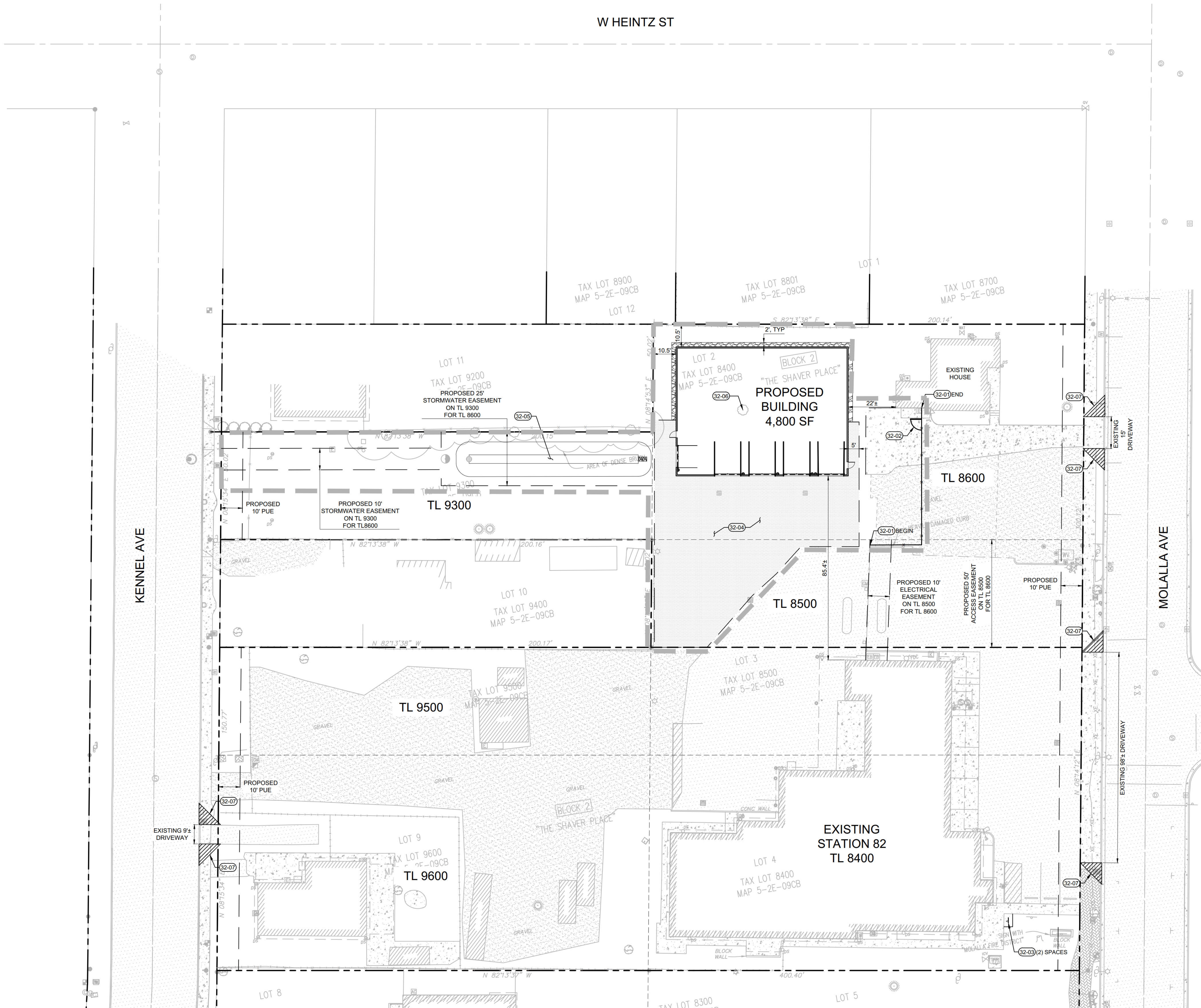
## ATTACHMENTS

- A. Site Plan

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<sup>5</sup> Downtown Molalla Development and OR 211 Streetscape Plan, City of Molalla. June 29, 2007.

W HEINTZ ST



LEGEND

	EXISTING	PROPOSED
SITE BOUNDARY	---	---
LOT LINE	---	---
LIMITS OF DISTURBANCE	---	---
ACCESS EASEMENT	---	---
STORMWATER EASEMENT	---	---
ELECTRICAL EASEMENT	---	---
ASPHALT PAVING	[Pattern]	[Pattern]
GRAVEL	[Pattern]	[Pattern]

KEYNOTES

- 32-01 SLATTED CHAINLINK FENCE - MATCH EXISTING
- 32-02 MAN GATE
- 32-03 BIKE RACK (STAPLE BY HUNT CO. OR SIMILAR, POWDER COAT FINISH, SURFACE MOUNT) ON 4' x 6' CONCRETE PAD
- 32-04 ASPHALT PAVING - 3.5" AC OVER 12" AGGREGATE BASE OVER COMPACTED SUBGRADE PER "REPORT OF GEOTECHNICAL ENGINEERING SERVICES" BY COLUMBIA WEST
- 32-05 FILTRATION RAIN GARDEN - 18" GROWING MEDIUM OVER 3" OF 3/4" - 1/2" OPEN GRADED AGGREGATE OVER 18" 1/2" TO 3/4" WASHED DRAIN ROCK WITH 4" PERFORATED DRAIN PIPE 30 MIL PVC WATERPROOF LINER
- 32-06 EXISTING TREE TO BE REMOVED
- 32-07 VISION CLEARANCE TRIANGLE PER MOLALLA MUNICIPAL CODE SECTION 17-3.3.030.G

SITE DATA

	AREA (SF)	AREA (AC)	COVERAGE
PROPERTY AREA	100,126	2.30	
AREA OF DISTURBANCE	13,110	0.30	13.1%
IMPERVIOUS AREA	19,595	0.45	19.6%
BUILDING AREA	52,681	1.21	52.6%
TOTAL IMPERVIOUS AREA	72,276	1.66	72.2%
LANDSCAPE AREA	27,850	0.64	27.8%

Molalla RFPD #73

Molalla RFPD #73  
Fire Station 82  
320 N Molalla Avenue  
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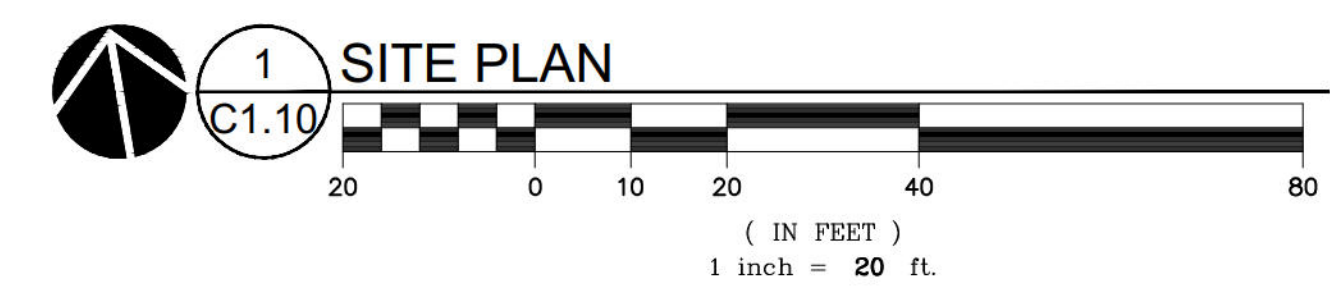
REVISION SCHEDULE		
Delta	Issued As	Issue Date

SHEET TITLE:  
SITE PLAN

SHEET

C1.10

JOB NO. 2250404.00



**Exhibit C:**  
*Public Works Comments*



**Community Development Department**

315 Kennel Ave/PO Box 248

Molalla, OR 97038

Phone 503.759.0205

[www.cityofmolalla.com](http://www.cityofmolalla.com)

---

June 2, 2026

TO: Mac Corthell, Community Development Director  
Dan Zinder, Planning Director  
Claressa Davis, Associate Planner.

FROM: Sam Miller, Engineering Section Manager

**RE: SDR03 & VAR02 - 2026 Molalla Fire - 320 N Molalla Ave**

Based on a review of the materials submitted, Staff has prepared the following comments. These comments are applicable to the subject application; any subsequent modifications may require amendments and/or additions. These conditions do not necessarily include requirements already set forth in the municipal code, but does not alleviate the applicant from following them.

**1. CONDITIONS REQUIRING RESOLUTION PRIOR TO BUILDING PERMIT SUBMITTAL**

- a. No building permit may be issued until all required public facility improvements are in place and approved by the City Engineer, or otherwise bonded, in conformance with the provisions of the Molalla Municipal Code (MMC) and the City of Molalla Standard Specifications for Public Works (MSSPW).
- b. Applicant shall submit a performance bond, irrevocable stand-by letter of credit, or a cash deposit that meets the requirements in MSSPW section 1.15.9 for public improvements, in a form and substance satisfactory to the City.
- c. Applicant shall conform to all emergency access, building, and fire code standards, as applicable at the time of building permit submittal.

**2. CONDITIONS REQUIRING RESOLUTION PRIOR TO FINAL OCCUPANCY**

- a. All public facility improvements shall be completed and accepted by the City of Molalla prior to issuance of final occupancy.
- b. **Streets:**
  - i. The applicant has submitted a Traffic Impact Analysis (TIA) as part of the Site Development Review (SDR) for the proposed Molalla Fire Station storage development. This analysis has been reviewed and accepted by the city. DKS

Associates prepared a Traffic Analysis Letter (TAL) included with the application. The TAL assesses the impacts of the proposed development and confirms compliance with city standards. The new storage building will enhance the fire station's storage and maintenance capabilities without increasing the number of employees or adding emergency vehicle trips to the site.

- ii. **N Molalla Ave:** N Molalla Ave is classified as an arterial street under the jurisdiction of the City of Molalla. The current right-of-way (ROW) width is 60 feet, and the paved width is approximately 38 feet. Arterial street with parking (W/PK) typically requires a right-of-way of 60 feet and a pavement width of 40 feet. Frontage improvements were completed in 2017 as part of Public Improvement project 16-01. The existing access to tax lot 8600 will need to be reassessed for access and circulation, along with spacing standards in accordance with the Transportation Master Plan and Public Works Standard.
- iii. **Kennel Ave:** Kennel Avenue is classified as a neighborhood Street under the jurisdiction of the City of Molalla. The current right-of-way (ROW) is 50 feet, with an approximate pavement width of 38 feet. No additional street dedication is required, as the existing ROW complies with the City's Transportation System Plan (TSP).
- iv. All Street improvements, including the extension or widening of existing streets and public access ways, must comply with Section 17-3.6.020 and be constructed in accordance with the City of Molalla Public Works Design Standards. All necessary public facility improvements must be completed and approved by the City Engineer or otherwise secured with a bond, in line with the provisions of this Code and the Public Works Design Standards.
- v. **Access & Circulation:** All lots created or reconfigured shall have adequate vehicle access and parking, as may be required, pursuant to Chapter **17-3.3**. Access to public streets shall be constructed in such a manner as to eliminate turning conflicts. Access spacing shall conform to the Transportation Systems Plan. The proposed width of accesses shall meet Molalla Standard Specifications for Public Works Construction.
- vi. **Easements:** Applicant shall be required to dedicate a 10-foot-wide public utility easement fronting the public right-of-way if one does not exist. Applicant shall provide proof of existing dedication.
- vii. **Transportation SDC's** – In accordance with MMC 13.14 this design review does not increase the impacts to the public improvement facility and is therefore exempt from transportation SDC charges.

**c. Sanitary Sewer:**

- i. The properties are currently served by two existing 4-inch sewer service lateral connections from the City sanitary main line located on Kennel and N Molalla Ave. The applicant is proposing a new sewer lateral that will connect the new storage building to the City sewer main on Molalla Ave. All new development must receive approval for sewer improvements from the City Engineer. Permits for these improvements will only be issued once all sanitary sewer plans comply with city standards, as well as Oregon state plumbing laws and the regulations set by the Oregon Department of Environmental Quality (DEQ). System Development Charges (SDC) credit will be granted for the existing services; however, an SDC will apply if the overall improvements lead to increased usage or additional impact on the City sewer system.
- ii. Sewer SDC's - In accordance with MMC 13.14 this design review does increase the impact to the public improvement facility and is therefore not exempt from Sewer SDC charges. SDC's shall be calculated in accordance with the SDC methodology.

**d. Water:**

- i. The properties are currently served by a 2-inch water meter service connection from the City water main located on N. Molalla Ave. The applicant is proposing a new water service lateral that will connect the new storage building to the City water main on Molalla Ave. Any new development must receive approval for water improvements from the City Engineer. Permits for these improvements shall not be issued until all water plans comply with city standards. A System Development Charge (SDC) credit will be granted for the existing service; however, an SDC will apply if the overall improvements result in additional usage or an increased impact on the City water system.
- ii. Should Fire Department regulations require additional fire flow that results in looping the water line through the site, then the applicant's engineer shall coordinate with Public Works for the extension of the public waterline. Public waterline shall be sized appropriately to meet fire flows, and meet City of Molalla Public Works Standards, and all public water lines and hydrants shall be within a public utility easement approved by the Public Works Department.
- iii. Water SDC's - In accordance with MMC 13.14 this design review does increase the impact to the public improvement facility and is therefore not exempt from Water SDC charges. SDC's shall be calculated in accordance with the SDC methodology.

**e. Storm:**

- i. **Onsite Stormwater Condition:** Applicant will be required to submit design and construction requirements for stormwater and surface water management at the time of Public Works Permit for the onsite private storm system for detention and water quality prior to discharge into a public facility. Design shall be in accordance with Section 3 of the Molalla Standard Specifications for Public Works Construction and Stormwater Master Plan. On-site private storm system shall comply with state plumbing code requirements. The detention and flow control facilities shall be reviewed, permitted, and inspected by Public Works. The onsite storm conveyance system shall be reviewed and inspected by Clackamas County Building under a plumbing permit. in Accordance with MMC 13.13 Surface Water Management.
- ii. The development must provide adequate surface water drainage facilities to reduce the risk of flood damage and enhance water quality. Improvements for water quality or quantity control may be required as per Chapter 17-3.6. The Applicant is also required to record a stormwater crossover easement between adjacent lots, which should be recorded with the County prior to final occupancy. A Storm Water Report has been submitted with the application, complying with the City of Molalla Public Works Standards.
- iii. Stormwater SDC's - *In accordance with MMC 13.14 this design review does increase the impacts to the public improvement facility and is therefore not exempt from stormwater SDC charges. SDC's shall be calculated in accordance with the SDC methodology.*

f. **Utilities:**

- i. Underground utilities: All utilities for the project shall be served by underground services. No overhead crossings of public right of way shall be approved by the City.
- ii. Connection to Water, Sewer, Storm: Prior to connection to the listed utilities, the applicant must gain approval from the City of Molalla Engineering Section.

**GENERAL DESIGN REQUIREMENTS & POLICIES**

- A. **Conformance Required.** *Improvements installed by the developer, either as a requirement of these regulations or at the developer's option, shall conform to the requirements of this chapter, approved construction plans, and to improvement standards and specifications adopted by the City.*
- B. **Adopted Installation Standards.** *The City of Molalla has adopted Public Works Design Standards for public improvements and private utility installation within the public right-of-way.*
- C. **Commencement.** *Work in a public right-of-way shall not begin until all applicable agency permits have been approved and issued.*

- D. **Resumption.** *If work is discontinued for more than six months, it shall not be resumed until the Public Works Director is notified in writing and grants approval of an extension.*
- E. **City Inspection.** *Improvements shall be constructed under the inspection of the City Engineer. The City Engineer may approve minor changes in typical sections and details if unusual conditions arising during construction warrant such changes in the public interest, except that substantive changes to the approved design shall be subject to review under Chapter [17-4.5](#) Modifications to Approved Plans and Conditions of Approval. Any survey monuments that are disturbed before all improvements are completed by the developer or subdivider shall be replaced at the developer or subdivider's expense prior to final acceptance of the improvements.*
- F. **Engineer's Certification and As-Built Plans.** *In accordance with the current version of the Public Works Design Standards, a registered civil engineer shall provide written certification in a form required by the City that all improvements, workmanship, and materials meet current and standard engineering and construction practices, conform to approved plans and conditions of approval, and are of high grade, prior to City's acceptance of the public improvements, or any portion thereof, for operation and maintenance. The developer's engineer shall also provide two sets of "as-built" plans, one paper set and one electronic set for permanent filing with the City. If required by the City, the developer or subdivider shall provide a warranty bond pursuant to Section [17-3.6.100](#). (Ord. 2017-08 §1*
- G. **Commercial Development Projects,** *No building permit may be issued until all required public facility improvements are in place and approved by the City Engineer, or otherwise bonded, in conformance with the provision of the Code and the Public Works Design Standards in accordance with MMC 17-3.6 Public Facilities. All public facilities shall be completed and accepted by the Public Works Department prior to issuance of final occupancy.*
- H. **Materials Submitted,** *The storm drain, domestic water, and sanitary sewer facilities will be obtained from main line connections and/or extensions. Separate engineering drawings reflecting the installation of these public utilities will be required.*
- I. **Construction and/or Connection,** *No construction of, or connection to, any existing or proposed public utility/improvements will be permitted until all plans are approved by Staff, all fees have been paid, all necessary permits, bonding, right-of-way, and easements have been obtained and approved by staff, and Staff is notified a minimum of 24 hours in advance.*
- J. **Revisions/Modifications,** *Staff reserves the right to require revisions/modifications to the public improvement construction plans and completed street improvements, if additional modifications or expansion of the sight distance onto adjacent streets is required.*
- K. **Civil Review,** *All public utility/improvement plans submitted for review shall be based upon a 22"x 34" format and shall be prepared in accordance with the City of Molalla Public Works Standards as described in Section 1 of the Molalla Standard Specifications for Public Works Construction.*
- L. **Monuments,** *All survey monuments on the subject site or that may be subject to disturbance within the construction area, or the construction of any off-site improvements, shall be adequately referenced and protected prior to commencement of any construction activity. If the survey monuments are disturbed, moved, relocated or destroyed as a result of any construction, the project shall, at its cost, retain the services of a registered professional land surveyor in the State of Oregon to restore the monument to its original condition and file the necessary surveys as required by Oregon State law. A copy of any recorded survey shall be submitted to Staff.*
- M. **Existing Wells,** *The applicant shall contact the Oregon Water Resources Department and inform them of any existing wells located on the subject site. Any existing well shall be limited to irrigation purposes only. Proper separation, in conformance with applicable State standards, shall be maintained between irrigation systems,*

*public water systems, and public sanitary systems. Should the project abandon any existing wells, they shall be properly abandoned in conformance with State standards and supply the City with a copy of the final document.*

- N. **Utilities**, *All utilities will be stubbed out to the far end of each street for future extension. The project shall utilize existing water, sewer, and storm water 'stub-outs' wherever possible. Water for domestic and fire protection shall be looped through the proposed site. Any 'stub-outs' determined to be not needed for the proposed development or any future development of the subject property shall be abandoned in accordance with the Molalla Standard Specifications for Public Works Construction.*
- O. **Public Improvements**, *All public improvement designs shall meet the requirements of the Molalla Standard Specifications for Public Works Construction as amended by the Public Works Director.*
- P. **General Easements** – *A 10-foot-wide public utility easement shall be dedicated to the City adjacent to all public right-of-way and no structures are allowed to encroach into the easement. Applicant shall be required to submit a legal description and exhibit map for review and sign City easements. Once completed, applicant will be required to record easements with the County Recorder's Office and return the original document to the City prior to final occupancy.*
- Q. **General Erosion Control** – *The applicant shall install, operate, and maintain adequate erosion control measures in conformance with the standards adopted by the City of Molalla and DEQ during the construction of any public/private utility and building improvements until such time as approved permanent vegetative materials have been installed. Applicant or Applicant's Contractor shall be responsible for all erosion control requirements under the 1200-C permit and shall coordinate directly with DEQ for questions related to 1200-C permit compliance.*



## CITY OF MOLALLA

### Staff Report

**Agenda Category: GENERAL BUSINESS**

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**Agenda Date: Wednesday, July 1, 2026**

**Submitted by: Dan Zinder, Planning Manager**

**Approved by: Dan Huff, City Manager**

**SUBJECT: Zone Change from Light Industrial (M-1) to Public/Semi-Public (PSP) for Taxlot 52E07A 00711**

**ATTACHMENTS:**

[ORD 2026-03 Yaw Zone Change Staff Report.pdf](#)



# CITY OF MOLALLA

117 N. Molalla Avenue  
PO Box 248  
Molalla, OR 97038

## Staff Report Agenda Category: General Business

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July 1, 2026,

From: Claressa Davis, Associate Planner  
Approved by: Dan Zinder, Planning Manager

**SUBJECT:** Recommend the rezone of a Publicly Owned Property to PSP

**FISCAL IMPACT:** N/A.

**RECOMMENDATION/RECOMMEND MOTION:** PC to discuss and provide recommendation on rezoning a publicly owned property to Public and Semi Public.

**BACKGROUND:** City Staff submits Planning File ZC01-2026/ORD2026-04 to Planning Commission for discussion. The proposal is to change the Comprehensive Plan Zoning Map and Zoning Map designations for one (1) publicly owned property to the Public and Semi Public Zone. The property does not have an address, but is identified as Taxlot 52E07A 00711, and is within City limits along OR-213.



The City recently acquired this property, and intends to change the zoning to more accurately reflect the property's future use. The zone change will:

- More accurately reflect what the property is used for,
- Remove the property from the City's industrial land calculations,
- Streamline the development process for the site; and,
- Potentially provide more sound legal arguments for recreational immunity.

The property is identified in the adopted 2025 Parks, Recreation, and Trails System Plan as the westernmost portion of a future trail along Bear Creek. Adoption of this ordinance would facilitate the development of this property as a public trail.

**EXHIBITS SUMMARY:**

- Exhibit A – Ordinance Language
- Exhibit B – Deed
- Exhibit C – Plat
- Exhibit D – Findings of Fact



**ORDINANCE NO. 2026-04**

**AN ORDINANCE OF THE CITY OF MOLALLA, OREGON  
Changing The Comprehensive Plan Zoning Map And Zoning Map For one  
Publicly Owned Property Located in City Limits to the Public and Semi Public Zone**

WHEREAS, the City recently acquired the property identified as Taxlot 52E07A 00711, which is currently zoned Light Industrial; and

WHEREAS, the City wishes to more accurately reflect what the property is used for, remove the property from the City’s industrial land calculations, streamline the development process for the site, and potentially provide more sound legal arguments for recreational immunity; and

WHEREAS, on 7/1/2026 the Planning Commission voted [WHAT] the proposed amendments to City Council; and

WHEREAS, the Molalla City Council conducted a public hearing on 8/26/2026. After the conclusion of the public hearing Council determined the zone change is consistent with all applicable legal requirements of state law, and City ordinances related to zoning changes and voted to approve the application.

**Now, Therefore, the City of Molalla ordains:**

- Section 1.** To amend and replace. The sections of the Comprehensive Plan and Zoning Map identified in Exhibits B & C are hereby amended to the Public and Semi Public Zone.
- Section 2:** The findings related to the zone changes, and supporting documentation, attached as Exhibits A-D, are incorporated herein by reference and adopted.
- Section 3. Effective Date.** This Ordinance shall take effect 30 days after enactment.

The First Reading was held on \_\_\_\_\_, and moved to a Second Reading by \_\_\_\_\_ vote of the City Council.

The Second Reading was held on \_\_\_\_\_ and adopted by the City Council on \_\_\_\_\_.

Signed this \_\_\_\_ day of \_\_\_\_\_ 2026.

ATTEST

\_\_\_\_\_  
Scott Keyser, Mayor

\_\_\_\_\_  
Christie Teets, CMC  
City Recorder

RECORDING REQUESTED BY:



10151 SE Sunnyside Road, Ste 300  
Clackamas, OR 97015

Clackamas County Official Records Catherine McMullen, County Clerk	<b>2026-013242</b> 04/01/2026 11:41:02 AM
D-D \$25.00 \$16.00 \$10.00 \$62.00	Cnt=1 Stn=42 JOSE \$113.00

AFTER RECORDING RETURN TO:

Order No.: 472526001251-JF  
City of Molalla  
PO Box 248  
Molalla, OR 97038

SEND TAX STATEMENTS TO:

City of Molalla  
PO Box 248  
Molalla, OR 97038

APN/Parcel ID(s): 05041712  
Tax/Map ID(s): 52E07A 00711  
Parcel 4 of Partition Plat 2026-001, Molalla, OR  
97038

SPACE ABOVE THIS LINE FOR RECORDER'S USE

STATUTORY WARRANTY DEED

**Kathleen Yaw and Craig Yaw, as tenants by the entirety**, Grantor, conveys and warrants to **City of Molalla**, Grantee, the following described real property, free and clear of encumbrances except as specifically set forth below, situated in the County of Clackamas, State of Oregon:

Parcel 4, PARTITION PLAT NO. 2026-001, recorded January 28, 2026, Recording No. 2026-003407, in the City of Molalla, County of Clackamas and State of Oregon.

THE TRUE AND ACTUAL CONSIDERATION FOR THIS CONVEYANCE IS NO DOLLARS AND NO/100 DOLLARS **(\$0.00)**. Contemporaneous written acknowledgment of Donation. (See ORS 93.030).

Subject to:

SEE EXHIBIT "A" ATTACHED HERETO AND MADE A PART HEREOF

**BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010. THIS INSTRUMENT DOES NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY THAT THE UNIT OF LAND BEING TRANSFERRED IS A LAWFULLY ESTABLISHED LOT OR PARCEL, AS DEFINED IN ORS 92.010 OR 215.010, TO VERIFY THE APPROVED USES OF THE LOT OR PARCEL, TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES, AS DEFINED IN ORS 30.930, AND TO INQUIRE ABOUT THE RIGHTS OF NEIGHBORING PROPERTY OWNERS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010.**

CHICAGO TITLE 472526001251

STATUTORY WARRANTY DEED  
(continued)

IN WITNESS WHEREOF, the undersigned have executed this document on the date(s) set forth below.

Dated: 3/30/2026

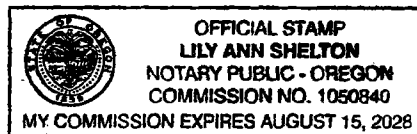
Kathleen Yaw  
Kathleen Yaw

Craig Yaw  
Craig Yaw

State of Oregon  
County of Wallowa

This instrument was acknowledged before me on 3/30/2026 - by Craig Yaw and Kathleen Yaw.

Lily Ann Shelton  
Notary Public - State of Oregon  
My Commission Expires: August 15, 2028



**EXHIBIT "A"**  
Exceptions

**Subject to:**

1. Any adverse claim based on the assertion that any portion of the subject land has been removed from or brought within the subject land's boundaries by the process of accretion or reliction or any change in the location of Bear Creek.

Any adverse claim based on the assertion that any portion of the subject land has been created by artificial means or has accreted to such portions so created, or based on the provisions of ORS 274.905 through 274.940.

Rights of the public, riparian owners and governmental bodies as to the use of the waters of Bear Creek and the natural flow thereof on and across that portion of the subject land lying below the high water line of said waterway.

2. Easement(s) for the purpose(s) shown below and rights incidental thereto, as granted in a document:

Granted to: City of Molalla  
Purpose: Sanitary sewer  
Recording Date: June 30, 1988  
Recording No: 88-026307  
Affects: 15 feet in width across Westerly corner and Southerly lot line, also delineated on Plat Map

3. Easement(s) for the purpose(s) shown below and rights incidental thereto, as granted in a document:

Granted to: Molalla Telephone Company  
Purpose: Underground telephone and communications facilities  
Recording Date: August 15, 2007  
Recording No: 2007-070823  
Affects: Reference is hereby made to said document for full particulars

4. Conditions and restrictions as es City of Molalla:

Purpose: Public Improvements  
Ordinance No. / File No.: 2010-01  
Recording Date: April 13, 2010  
Recording No.: 2010-022085

5. Covenants, conditions, restrictions and easements but omitting any covenants or restrictions, if any, including but not limited to those based upon race, color, religion, sex, sexual orientation, familial status, marital status, disability, handicap, national origin, ancestry, source of income, gender, gender identity, gender expression, medical condition or genetic information, as set forth in applicable state or federal laws, except to the extent that said covenant or restriction is permitted by applicable law, as set forth in the document

Recording Date: November 6, 2018  
Recording No: 2018-067934

## EXHIBIT "A"

### Exceptions

6. Easement(s) for the purpose(s) shown below and rights incidental thereto, as granted in a document:

Granted to: City of Molalla  
Purpose: Public utility  
Recording Date: November 16, 2018  
Recording No: 2018-069809  
Affects: Reference is hereby made to said document for full particulars, also delineated on Plat Map

7. Restrictions, but omitting restrictions, if any, based upon race, color, religion, sex, sexual orientation, familial status, marital status, disability, handicap, national origin, ancestry, or source of income, as set forth in applicable state or federal laws, except to the extent that said restriction is permitted by applicable law, as shown on that certain plat

Name of Plat: Partition Plat No. 2018-116  
Recording Date: November 27, 2018  
Recording No: 2018-071613

8. Easement(s) for the purpose(s) shown below and rights incidental thereto as delineated or as offered for dedication, on the map of said tract/plat;

Purpose: Public utility  
Affects: 10 feet in width along Westerly lot line, also delineated on Partition Plat Map 2026-001

9. Easement(s) for the purpose(s) shown below and rights incidental thereto as delineated or as offered for dedication, on the map of said tract/plat;

Purpose: Storm drainage for the benefit of the City of Molalla  
Affects: Westerly and Southerly portion of lot, as shown on plat, also delineated on Partition Plat Map 2026-001

10. Easement(s) for the purpose(s) shown below and rights incidental thereto, as granted in a document:

Granted to: State of Oregon, by and through its Department of Transportation  
Purpose: Permanent slope easement and temporary construction easement  
Recording Date: May 6, 2022  
Recording No: 2022-026631 and 2002-026632  
Affects: Westerly portion of property - See documents for specifics, also delineated on Partition Plat Map 2026-001

11. Development Agreement including the terms and provisions thereof,

Recording Date: May 14, 2024  
Recording No: 2024-016831  
Between: City of Molalla, a municipal corporation of the State of Oregon  
And: Craig and Kathleen Yaw

**EXHIBIT "A"**  
Exceptions

12. Restrictions, but omitting restrictions, if any, based upon race, color, religion, sex, sexual orientation, familial status, marital status, disability, handicap, national origin, ancestry, or source of income, as set forth in applicable state or federal laws, except to the extent that said restriction is permitted by applicable law, as shown on that certain plat

Name of Plat: Partition Plat No. 2026-001  
Recording Date: January 28, 2026  
Recording No: 2026-003407

**PARTITION PLAT No. 2025-<sup>6</sup>001**  
 A REPLAT OF PARCEL 2 OF PARTITION PLAT NO. 2018-116 IN THE HUGH GORDON D.L.C. NO. 40, IN THE NE 1/4 OF SECTION 7, T. 5 S., R. 2 E., W.M., CITY OF MOLALLA, CLACKAMAS COUNTY, OREGON  
 CITY OF MOLALLA PLANNING FILE NO. MP02-2020  
 OCTOBER 30, 2025 SHEET 1 OF 3

**SURVEY AND DEED REFERENCES**

R1: PARTITION PLAT NO. 2018-116 BY TONY BROOKS  
 R2: SN 2022-263  
 D1: DOCUMENT NO. 2022-026632  
 D2: DOCUMENT NO. 88-026307

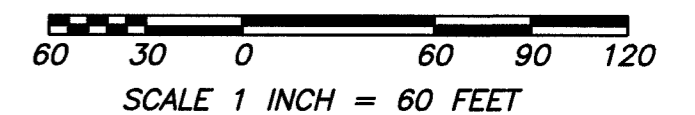
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C2	28.00'	44°46'06"	21.88'	S 48°28'49" E	21.33'
C3	28.00'	44°47'14"	21.89'	N 86°44'31" E	21.33'
C4	183.00'	11°18'10"	36.10'	S 76°30'57" E	36.04'

**LEGEND:**

- ▲ FOUND 5/8" IRON ROD W/YPC "AG GEO NW LLC" FROM P.P. NO. 2018-116
- MONUMENT FOUND AS NOTED
- SET 5/8" IRON ROD W/YPC "AG GEO NW LLC" SET OCTOBER 30, 2025
- IR IRON ROD
- YPC YELLOW PLASTIC CAP
- SN SURVEY NUMBER
- P.P. PARTITION PLAT, CLACKAMAS COUNTY PLAT RECORDS
- ( ) RECORD DATA
- R1 SURVEY REFERENCE
- D1 DEED REFERENCE
- DOC. NO. DOCUMENT NUMBER
- P.U.E. PUBLIC UTILITY EASEMENT
- N.T.S. NOT TO SCALE
- CL CENTERLINE

REGISTERED PROFESSIONAL LAND SURVEYOR  
*Tony Brooks*  
 OREGON  
 JAN. 16, 1996  
 TONY J. BROOKS  
 2736

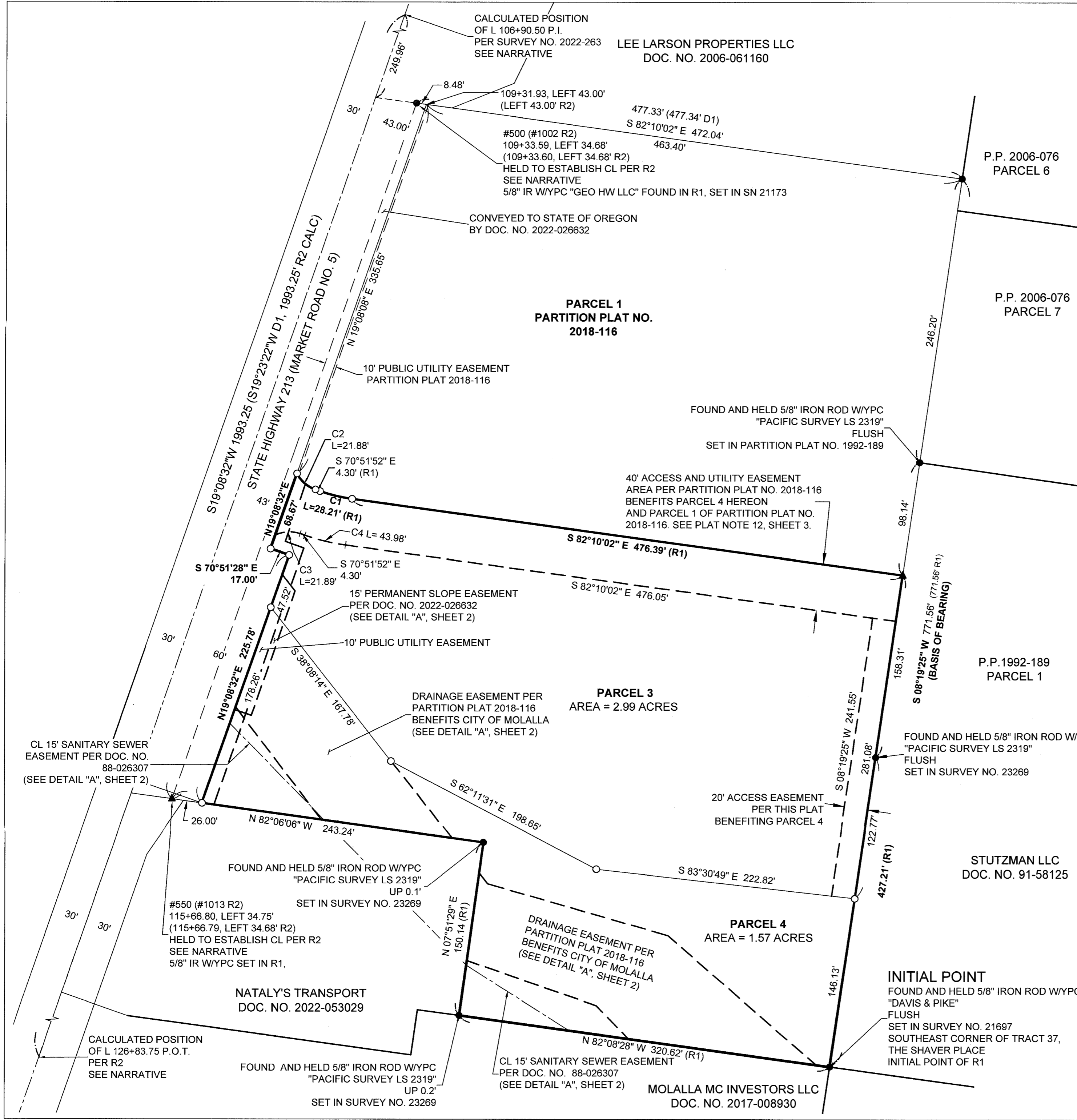
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 SIGNED: *11-6-25*



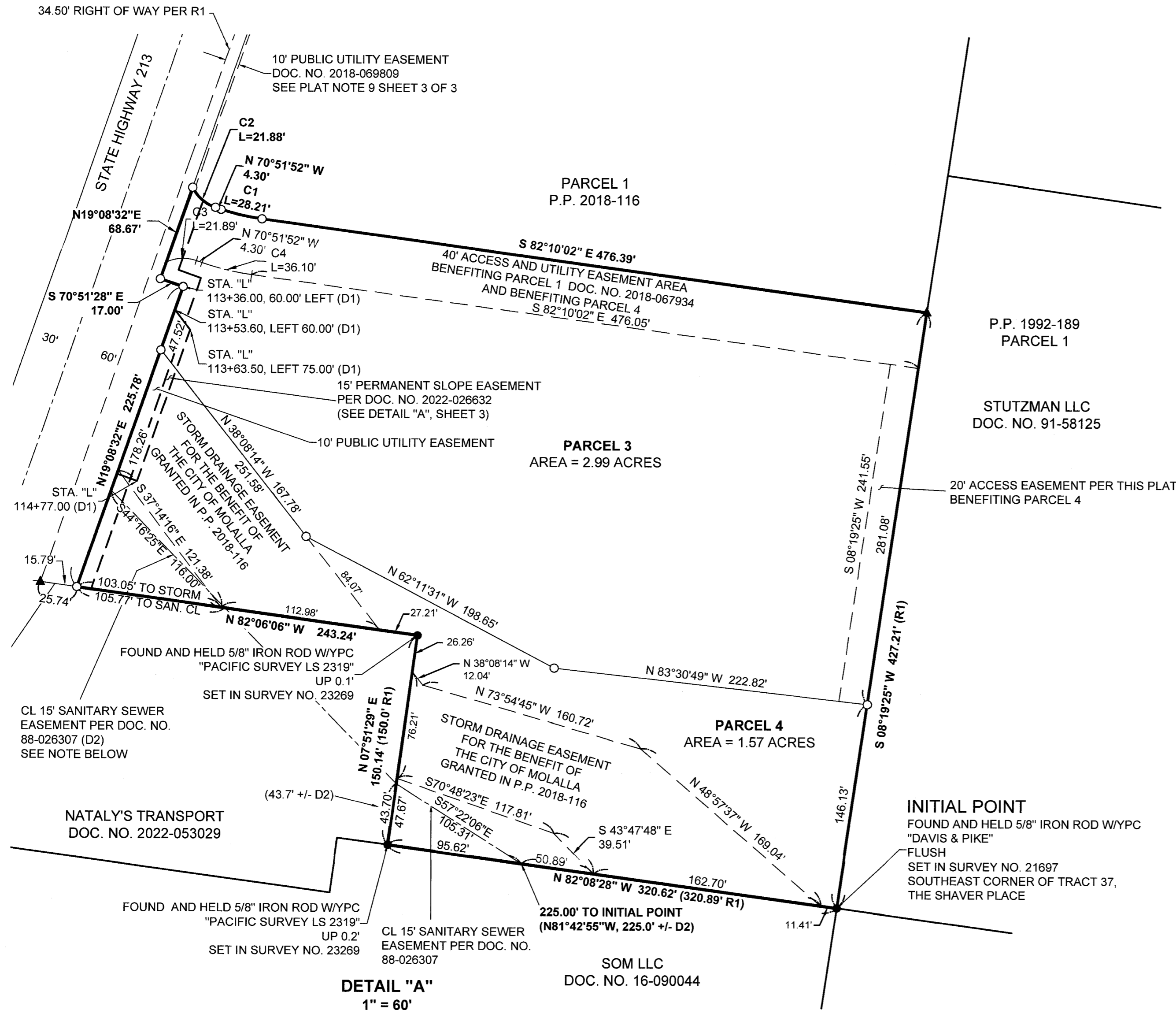
Client: 18-013 CRAIG & KATHLEEN YAW



Ag Geospatial NW  
 Ag spatial Data Management  
 Land Surveying & GIS  
 30532 S McCown Road, Molalla, OR 97038  
 503-329-8008 cell/text  
 info@AgGeoNW.com www.AgGeoNW.com

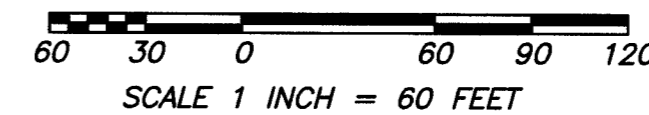
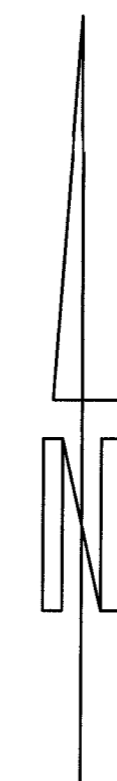


**PARTITION PLAT No. 2025-<sup>6</sup>001**  
 A REPLAT OF PARCEL 2 OF PARTITION PLAT NO. 2018-116 IN THE HUGH GORDON D.L.C. NO. 40, IN THE NE 1/4 OF SECTION 7, T. 5 S., R. 2 E., W.M., CITY OF MOLALLA, CLACKAMAS COUNTY, OREGON  
 CITY OF MOLALLA PLANNING FILE NO. MP02-2020  
 OCTOBER 30, 2025 SHEET 2 OF 3



- LEGEND:**
- ▲ FOUND 5/8" IRON ROD W/PC "AG GEO NW LLC" FROM P.P. NO. 2018-116
  - MONUMENT FOUND AS NOTED
  - SET 5/8" IRON ROD W/PC "AG GEO NW LLC" SET OCTOBER 30, 2025
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  - N.T.S. NOT TO SCALE
  - CL CENTERLINE

CURVE TABLE					
CURVE	RADIUS	DELTA	ARC DIST.	CHORD BEARING	CHORD DIST.
C1	143.00'	11°18'10"	28.21'	S 76°30'57" E	28.16'
C2	28.00'	44°46'06"	21.88'	S 48°28'49" E	21.33'
C3	28.00'	44°47'14"	21.89'	N 86°44'31" E	21.33'
C4	183.00'	11°18'10"	36.10'	S 76°30'57" E	36.04'



REGISTERED PROFESSIONAL LAND SURVEYOR  
*Tony Brooks*  
 OREGON  
 JAN. 16, 1996  
 TONY J. BROOKS  
 2736

RENEWS: 12/31/25  
 SIGNED: 11-6-25

Client: 18-013 CRAIG & KATHLEEN YAW



30532 S McCown Road, Molalla, OR 97038  
 503-329-8008 cell/text

info@AgGeoNW.com www.AgGeoNW.com

**SURVEYOR'S CERTIFICATE:**

I, TONY J. BROOKS, A LICENSED PROFESSIONAL LAND SURVEYOR IN THE STATE OF OREGON, HEREBY CERTIFY THAT I HAVE CORRECTLY SURVEYED AND MARKED WITH PROPER MONUMENTS PARCELS 3 AND 4 AS SHOWN ON THE ATTACHED PARTITION MAP.

PARCEL 2 OF PARTITION PLAT NO. 2018-116 IN THE PLAT RECORDS OF CLACKAMAS COUNTY, IN THE HUGH GORDON D.L.C. NO. 40, IN THE NORTHEAST QUARTER OF SECTION 7, TOWNSHIP 5 SOUTH, RANGE 2 EAST OF THE WILLAMETTE MERIDIAN, CITY OF MOLALLA, CLACKAMAS COUNTY, OREGON, THE SAID TRACT BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INITIAL POINT, THE SAID POINT BEING A FOUND 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "DAVIS & PIKE" MARKING THE SOUTHEAST CORNER OF PARCEL 2 OF PARTITION PLAT NO. 2018-116 AND THE INITIAL POINT OF SAID PLAT; THENCE N 82°08'28" W ALONG THE SOUTH LINE OF SAID PARCEL 2 A DISTANCE OF 320.62 FEET TO A FOUND 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "PACIFIC SURVEY LS 2319"; THENCE N 07°51'29" E ALONG THE MOST SOUTHERLY WEST LINE OF SAID PARCEL 2 A DISTANCE OF 150.14 FEET TO A FOUND 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "PACIFIC SURVEY LS 2319"; THENCE N 82°06'06" W ALONG THE MOST WESTERLY SOUTH LINE OF SAID PARCEL 2 A DISTANCE OF 243.24 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" ON THE EASTERLY RIGHT-OF-WAY LINE OF STATE HIGHWAY 213 (MARKET ROAD NO. 5), WHICH POINT MEASURES 60.00 FEET EAST OF CENTERLINE STATION 115+61.78 WHEN MEASURED AT RIGHT ANGLES AS PER WARRANTY DEED TO THE STATE OF OREGON RECORDED AS DOCUMENT NO. 2022-026632, DEED RECORDS OF CLACKAMAS COUNTY; THENCE N 19°08'32" E ALONG SAID EASTERLY RIGHT-OF-WAY LINE A DISTANCE OF 225.78 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" MARKING A POINT 60.00 FEET OPPOSITE CENTERLINE STATION "L" 113+36.00; THENCE N 70°51'28" W A DISTANCE OF 17.00 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" MARKING A POINT 43.00 FEET OPPOSITE CENTERLINE STATION "L" 113+36.00; THENCE PROCEEDING PARALLEL WITH AND 43.00 FEET EASTERLY OF SAID CENTERLINE WHEN MEASURED AT RIGHT ANGLES, N 19°08'32" E A DISTANCE OF 68.67 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" OPPOSITE CENTERLINE STATION 112+67.33, THE SOUTHERLY LINE OF PARCEL 1 OF SAID PARTITION PLAT NO. 2018-116, AND A POINT ON A 28.00-FOOT RADIUS CURVE TO THE LEFT (NON-TANGENT AT THIS POINT); THENCE A DISTANCE OF 21.88 FEET ALONG THE ARC OF SAID CURVE, THROUGH A CENTRAL ANGLE OF 44°46'06" (CHORD BEARS S 48°28'49" E A DISTANCE OF 21.33 FEET) TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" AND THE POINT OF TANGENCY; THENCE S 70°51'52" E A DISTANCE OF 4.30 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" AND THE BEGINNING OF A 143.00-FOOT-RADIUS TANGENT CURVE TO THE LEFT; THENCE 28.21 FEET ALONG THE ARC OF SAID CURVE, THROUGH A CENTRAL ANGLE OF 11°18'10" (CHORD BEARS S 76°30' 57" E A DISTANCE OF 28.16 FEET TO A SET 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" AND THE POINT OF TANGENCY; THENCE S 82°10'02" E A DISTANCE OF 476.39 FEET TO A FOUND 5/8-INCH IRON ROD WITH A YELLOW PLASTIC CAP STAMPED "AG GEO NW LLC" MARKING THE SOUTHEAST CORNER OF SAID PARCEL 1 AND THE NORTHEAST CORNER OF SAID PARCEL 2 OF PARTITION PLAT NO. 2018-116; THENCE S 08°19'25" W ALONG THE EAST LINE OF SAID PARCEL 2 A DISTANCE OF 427.21 FEET TO THE INITIAL POINT.

CONTAINING 4.56 ACRES, MORE OR LESS

**DECLARATION:**

KNOW ALL PEOPLE BY THESE PRESENTS THAT KATHLEEN YAW AND CRAIG YAW, ARE THE OWNERS OF THE LAND REPRESENTED ON THE ATTACHED MAP AND DESCRIBED IN THE ACCOMPANYING SURVEYORS CERTIFICATE AND HAVE CAUSED THIS PARTITION PLAT TO BE PREPARED AND THE PROPERTY PARTITIONED INTO PARCELS IN ACCORDANCE WITH THE PROVISIONS OF ORS CHAPTER 92 AS SHOW ON THE ATTACHED MAP WITH EASEMENTS AND RESTRICTIONS AS SHOWN OR NOTED. THE DECLARANTS MAKE NO CLAIM TO PROPERTY LYING BEYOND THE PLAT BOUNDARY AS MONUMENTED AND AS DESCRIBED IN THE SURVEYOR'S CERTIFICATE.

BY: Kathleen Yaw November 12, 2025  
KATHLEEN YAW  
BY: Craig Yaw November 12, 2025  
CRAIG YAW

**ACKNOWLEDGMENTS:**

STATE OF OREGON }  
COUNTY OF Wallowa } SS

KNOW ALL PEOPLE BY THESE PRESENT BEFORE ME, A NOTARY IN AND FOR SAID STATE AND COUNTY, PERSONALLY APPEARED KATHLEEN YAW, WHO BEING FIRST DULY SWORN DID SAY SHE IS THE IDENTICAL PERSON NAMED IN THE FOREGOING DECLARATION AND THAT SHE EXECUTED THE SAME FREELY AND VOLUNTARILY.

BY: Danaleigh Roberts November 12th, 2025  
NOTARY SIGNATURE COMMISSION NO. 1039155

BY: Danaleigh Roberts MY COMMISSION EXPIRES: July 31st, 2027  
NOTARY PUBLIC-OREGON

STATE OF OREGON }  
COUNTY OF CLACKAMAS } SS

KNOW ALL PEOPLE BY THESE PRESENT BEFORE ME, A NOTARY IN AND FOR SAID STATE AND COUNTY, PERSONALLY APPEARED CRAIG YAW, WHO BEING FIRST DULY SWORN DID SAY HE IS THE IDENTICAL PERSON NAMED IN THE FOREGOING DECLARATION AND THAT HE EXECUTED THE SAME FREELY AND VOLUNTARILY.

BY: Katie Ellen Matson December 9, 2025  
NOTARY SIGNATURE COMMISSION NO. 1054495

BY: Katie Ellen Matson MY COMMISSION EXPIRES: December 18, 2028  
NOTARY PUBLIC-OREGON

**NARRATIVE:**

THE PURPOSE OF THIS SURVEY IS TO PARTITION PARCEL 2 OF PARTITION PLAT NO. 2018-116 INTO TWO PARCELS. PLANNING APPROVAL IS BY CITY OF MOLALLA PLANNING DEPARTMENT IN FILE NO. MP02-2020. VESTING DEED FOR SAID PARCEL 2 WAS RECORDED AS DOCUMENT NO. 2023-023026, CLACKAMAS COUNTY DEED RECORDS.

BOUNDARY RESOLUTION: I RECOVERED AND HELD THE MONUMENTS SHOWN ON THE MAP AS FOUND. SEVERAL OF THE MONUMENTS SET IN PARTITION PLAT NO. 2018-116 WERE DESTROYED BY CONSTRUCTION. I RESET THEM IN THEIR RECORD POSITIONS.

ADDITIONAL RIGHT-OF-WAY AND A PERMANENT SLOPE EASEMENT WERE CONVEYED TO THE STATE OF OREGON BY WARRANTY DEED RECORDED IN THE DEED RECORDS OF CLACKAMAS COUNTY AS DOCUMENT NO. 2022-026632. THE CONVEYANCES ARE DEFINED IN TERMS OF STATIONS AND OFFSETS FROM THE "L" LINE AS SURVEYED IN SN 2022-263.

SAID "L" LINE THAT I COMPUTED FOR PARTITION PLAT NO. 2018-116 WAS BASED ON SN 2016-039 BY OREGON DEPT. OF TRANSPORTATION (ODOT). ODOT LATER FILED SN 2022-263, WHICH SURVEY CHANGED THE CENTERLINE ALIGNMENT ("L" LINE) BY REDEFINING STATION 106+90.50, MOVING IT NORTHERLY 0.06 FOOT. IN ORDER TO CONFORM TO THE REDEFINED "L" LINE FOR THIS SURVEY, I FIRST ESTABLISHED SAID "L" LINE PER SN 2022-263. I THEN COMPUTED ODOT POINT NUMBERS 1013 AND 1002 BY THE STATIONS AND OFFSETS LISTED IN THE MONUMENT TABLE ON SHEET 10 OF SAID SURVEY. SAID ODOT POINTS 1013 AND 1002 CORRESPOND TO FOUND MONUMENTS 505 AND 500 RESPECTIVELY ON THIS SURVEY. THE DISTANCE INVERSED BETWEEN THE ODOT POINTS IS ONLY 0.02' LESS THAN THE DISTANCE BETWEEN THE CORRESPONDING POINTS 505 AND 500 PER THIS SURVEY. I THEREFORE ROTATED SAID COMPUTED ODOT OFFSET POINTS (ALONG WITH THE CENTERLINE ALIGNMENT) ONTO THE BEARING BETWEEN POINTS 505 AND 500 AND PLACED THE ERROR OF 0.01 FOOT ON EACH END OF THE LINE BETWEEN SAID POINTS. I NOTE THAT THE ERROR OF 0.02 FOOT IN 633.19 FEET IS OVER 1 IN 30,000 AND WELL WITHIN ACCEPTABLE TOLERANCE.

**BASIS OF BEARING:**

BEARINGS ARE BASED ON PARTITION PLAT NO. 2018-116, CLACKAMAS COUNTY SURVEY RECORDS. THE EAST LINE OF SAID PLAT, HELD BETWEEN THE FOUND MONUMENTS MARKING THE NORTHEAST AND SOUTHEAST CORNERS THEREOF, BEARS S 08°19'25" W.

HORIZONTAL DATUM IS "OCRS (OREGON COORDINATE REFERENCE SYSTEM) SALEM", NAD 1983 (2011) EPOCH 2010.00, INTERNATIONAL FEET. CONTROL WAS ESTABLISHED WITH A TRIMBLE R8 MODEL 3 GPS UNIT AND TRIMBLE VRSNOW OREGON GPS NETWORK.

**PLAT NOTES:**

- 1. SUBJECT TO THE CONDITIONS OF APPROVAL AS STATED IN THE CITY OF MOLALLA OF PLANNING DEPARTMENT FILE NO: MP02-2020.
- 2. SUBJECT TO PUBLIC RIGHTS TO STATE HWY 213.
- 3. SUBJECT TO A STORM DRAINAGE EASEMENT FOR THE BENEFIT OF THE CITY OF MOLALLA, SAID EASEMENT IS APPROXIMATELY 25 FEET FROM TOP OF BANK OF BEAR CREEK. SEE DETAIL "B" SHEET 3 OF 3.
- 4. SUBJECT TO EXISTING SANITARY SEWER EASEMENTS PER DOCUMENT NO. 88-026307, CLACKAMAS COUNTY DEED RECORDS.
- 5. THIS PLAT IS SUBJECT TO THE TERMS AND CONDITIONS OF COMMUNICATIONS LINE RIGHT-OF-WAY AND AGREEMENT RECORDED AS DOCUMENT NO. 2007-070823, CLACKAMAS COUNTY DEED RECORDS. NOT ABLE TO MAP.
- 6. THIS PLAT IS SUBJECT TO EASEMENTS, COVENANTS, CONDITIONS AND RESTRICTIONS AGREEMENT RECORDED IN DOCUMENT NO. 2018-067934, CLACKAMAS COUNTY DEED RECORDS.
- 7. SUBJECT TO RIGHT-OF-WAY DONATION TO THE STATE OF OREGON DEPARTMENT OF TRANSPORTATION AS RECORDED IN DOCUMENT NO. 2018-068819, CLACKAMAS COUNTY DEED RECORDS.
- 8. THIS PLAT IS SUBJECT TO A 10 FOOT PUBLIC UTILITY EASEMENT FOR THE BENEFIT OF THE CITY OF MOLALLA AS RECORDED IN DOCUMENT NO. 2018-069809, CLACKAMAS COUNTY DEED RECORDS.
- 9. THIS PLAT IS SUBJECT TO CITY OF MOLLALA RESOLUTION 2010-01 AS RECORDED IN DOCUMENT NO. 2010-022085, CLACKAMAS COUNTY DEED RECORDS.
- 10. THIS PLAT IS SUBJECT TO COVENANTS, CONDITIONS, AND RESTRICTIONS AS SHOWN ON PARTITION PLAT NO. 2018-116.
- 11. THIS PLAT IS SUBJECT TO DEVELOPMENT AGREEMENT WITH THE CITY OF MOLALLA AS RECORDED IN DOCUMENT NO. 2024-016831, CLACKAMAS COUNTY DEED RECORDS.
- 12. THE 40' ACCESS AND UTILITY EASEMENT BENEFITING PARCEL 1 OF PARTITION PLAT NO. 2018-116 IS TO BENEFIT PARCEL 4 OF THIS PARTITION PLAT.
- 13. THE 20' ACCESS EASEMENT ALONG THE EAST LINE OF PARCEL 3 IS TO BENEFIT PARCEL 4 OF THIS PARTITION PLAT.
- 14. THIS PLAT IS SUBJECT TO ACCESS EASEMENT MAINTENANCE AGREEMENT WITH THE CITY OF MOLALLA AS RECORDED IN DOCUMENT NO. 2025 \_\_\_\_\_, CLACKAMAS COUNTY DEED RECORDS.

**PARTITION PLAT No. 2025-<sup>60</sup>001**  
**A REPLAT OF PARCEL 2 OF PARTITION PLAT NO. 2018-116 IN THE HUGH GORDON D.L.C. NO. 40, IN THE NE 1/4 OF SECTION 7, T. 5 S., R. 2 E., W.M., CITY OF MOLALLA, CLACKAMAS COUNTY, OREGON CITY OF MOLALLA PLANNING FILE NO. MP02-2020**  
**OCTOBER 30, 2023 SHEET 3 OF 3**

**APPROVALS:**

APPROVED THIS 10th DAY OF December, 2025

CITY OF MOLALLA COMMUNITY PLANNER

BY: [Signature]  
PLANNING DEPARTMENT

APPROVED THIS 10th DAY OF December, 2025

CITY OF MOLALLA PUBLIC WORKS

BY: [Signature]  
PUBLIC WORKS DIRECTOR

APPROVED THIS 15 DAY OF January, 2026

[Signature]  
CLACKAMAS COUNTY SURVEYOR

ALL TAXES, FEES, ASSESSMENTS AND OTHER CHARGES AS PROVIDED BY ORS 92.095 HAVE BEEN PAID THROUGH JUNE 30, 2026

APPROVED THIS 15 DAY OF January, 2026  
CLACKAMAS COUNTY ASSESSORS & TAX COLLECTOR.

BY: [Signature]  
DEPUTY

STATE OF OREGON }  
COUNTY OF CLACKAMAS } SS

I DO HEREBY CERTIFY THAT THE ATTACHED PARTITION PLAT WAS RECEIVED FOR RECORD

ON THE 28th DAY OF January, 2026

AT 2:27 O'CLOCK P.M.

AS PARTITION PLAT NO. 2026-001

DOCUMENT NO. 2026-003407

CATHERINE MCMULLEN, CLACKAMAS COUNTY CLERK

BY: [Signature]  
DEPUTY

REGISTERED PROFESSIONAL LAND SURVEYOR

Tony Brooks  
OREGON  
JAN. 16, 1996  
TONY J. BROOKS  
2736

RENEWS: 12/31/25  
SIGNED: 11-6-23

Client: 18-013 CRAIG & KATHLEEN YAW



30532 S McCown Road, Molalla, OR 97038  
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## FINDINGS OF FACT

Staff finds the following provisions of Oregon Revised Statutes, Oregon Administrative Rules, and Molalla Municipal Code to be applicable to a Zoning and Comprehensive Map change:

### MMC 17-4.6.030 Annexation & Zone Change Approval Criteria

*Planning Commission review and recommendation, and City Council approval, of an ordinance amending the Zoning Map, Development Code, or Comprehensive Plan shall be based on all of the following criteria:*

- (A) If the proposal involves an amendment to the Comprehensive Plan, the amendment must be consistent with the Statewide Planning Goals and relevant Oregon Administrative Rules;*

**Findings:** The proposal amends the Comprehensive Plan by amending the Comprehensive Plan Map, and thus, must be consistent with the Statewide Planning Goals and relevant Oregon Administrative Rules.

Applicable Statewide Planning Goals are:

- Goal 1: Citizen Participation; and,
- Goal 5: Natural Resources, Scenic and Historic Areas, and Open Spaces; and,
- Goal 7: Areas Subject to Natural Hazards; and
- Goal 8: Recreational Needs.

Additionally, the proposed zone change will more accurately reflect what the site is used for and will remove the site from the City's Buildable Land calculations. This will better allow staff to understand the full scope of active industrial lands within the City.

### **Findings for Goal 1: Citizen Participation**

Prior to bringing this item to City Council, staff completed the following actions:

1. The proposed ordinance and staff report were made available to the public at least 5 days prior to the July 1<sup>st</sup>, 2026, Planning Commission meeting.
2. Staff held a discussion of the proposed ordinance with the Molalla Planning Commission on July 1<sup>st</sup>, 2026, to gather their recommendation.
3. Staff notified DLCD through their Post-acknowledgement plan amendments (PAPA) portal on June 16<sup>th</sup>, 2026. This meets or exceeds the 35-day notice requirement.
4. The only party who was required to be noticed via Measure 56 was the applicant, The City of Molalla, per ORS 227.186. This will be done within the 20-40 day period prior to the hearing.
5. Notice was mailed to an interested parties list on [DATE], exceeding the 20 day requirement for Type IV decisions per MMC 17-4.1.050 C, 2. *Will be filled in for Council .*
6. Newspaper notice was published on [DATE] exceeding the 10-day requirement for Type IV decisions per MMC 17-4.1.050 C, 4. *Will be filled in for Council .*
7. The proposed ordinance and staff report were made available to the public at least 5 days prior to the [DATE] City Council meeting. *Will be filled in for Council .*

Consistency with Goal 1 is met.

### **Findings for Goal 5: Natural Resources, Scenic and Historic Areas, and Open Spaces**

The subject site is encumbered by Bear Creek and associated riparian buffer and wetlands, as identified in the City's 2001 Local Wetlands Inventory. It currently has industrial zoning and comprehensive plan designations. As the City is now the property owner, these designations are inconsistent with the land's character. The zone change will more accurately reflect the property's intended use.

The adopted 2025 Parks, Recreation, and Trails System Plan includes the creation of a trail along Bear Creek in its lists of projects. Rezoning this site to Public and Semi Public (PSP) better aligns with the plan to use the site for open space and recreational purposes.

Consistency with Goal 5 is met.

### **Findings for Goal 7: Areas Subject to Natural Hazards**

The subject site is encumbered by Bear Creek. While the subject site is not included on the FEMA 100 or 500 year maps, the City's adopted Natural Hazard Mitigation Plan states that "Other portions of Molalla outside of the mapped floodplains may also be at relatively high risk from over bank flooding from streams too small to be mapped by FEMA or from local storm water drainage." During local high water events, Staff observed seasonal streams and wetland areas becoming saturated. The City acquired this property with the goal of implementing the adopted 2025 Parks, Recreation, and Trails System Plan project of creating a trail along Bear Creek. This will reduce the development potential of the site and better situate the City for future high water events. Prioritizing this site as open space will also increase the capacity of the area to absorb water.

Consistency with Goal 7 is met.

### **Findings for Goal 8: Recreational Needs**

The subject site is identified in the adopted 2025 Parks, Recreation, and Trails System Plan for a future trail. While the land use category "Public Parks and Open Space, including Playgrounds, Trails, Nature Preserves, Athletic Fields, Courts, and similar uses" is permitted in all zoning designations, changing the subject parcel to Public and Semi Public (PSP) aligns with the City's standard of zoning publicly owned land PSP. The PSP zoning designation will streamline the development process. Additionally, a site zoned Public and Semi Public is better situated to withstand legal challenge against recreational immunity.

Consistency with Goal 8 is met.

*(B) The proposal must be consistent with the Comprehensive Plan (the Comprehensive Plan may be amended concurrently with proposed changes in zoning);*

### **Findings:**

The adopted 2025 Parks, Recreation, and Trails System Plan is one of the functional plans adopted as part of the Molalla Comprehensive Plan. This plan identifies the subject parcel as the western portion of a future trail that will run adjacent to Bear Creek. This proposal changes the zoning from Light Industrial (M-1) to Public/Semi-Public to help facilitate that project as the City acquires more land for this project. This proposal amends the Comprehensive Plan Map concurrently with zoning map changes.

Molalla Comprehensive Plan Goals 1, 5, 7, and 8 are discussed under Statewide Planning Goal Findings. The proposal is intended to prepare the site to be developed as a public recreation area, and will bring the zoning map and comprehensive plan into conformance with this plan through changes to both.

*(C) The City Council must find the proposal to be in the public interest with regard to community conditions; the proposal either responds to changes in the community, or it corrects a mistake or inconsistency in the subject plan or code;*

**Findings:**

Staff finds that the proposal is in the public interest by responding to changes in community conditions. The City recently acquired this property, as shown in Exhibit B. As the subject property is now publicly owned and planned for public use, zoning for the property ought to be changed accordingly. The proposed ordinance corrects this use/zoning inconsistency.

This criterion is met.

*(D) The amendment must conform to Section 17-4.6.050 Transportation Planning Rule Compliance*

**Findings:**

The current designation for the site is Light Industrial. The Institute of Traffic Engineers (ITE) Trip Generation Manual, 10<sup>th</sup> Edition, states that a General Light Industrial Use has an average rate of 4.96 trips on a weekday. The proposed change would be to change the zoning to Public and Semi Public, to create a trail along Bear Creek. The 10<sup>th</sup> edition of the ITE Manual states that a Public Park, including hiking trails, has an average rate of 0.78 trips on a weekday.

Thus, according to 10<sup>th</sup> edition of the ITE Manual, maximum trip generation from a Public Park is lower than maximum trip generation from uses allowed under the current Light Industrial zoning designation for this parcel. Staff determines that no significant impact on the City's transportation systems will result from the proposed ordinance and that no further analysis is required. This criterion is met.



## CITY OF MOLALLA

### Staff Report

**Agenda Category: GENERAL BUSINESS**

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**Agenda Date: Wednesday, July 1, 2026**

**Submitted by: Christie Teets, City Recorder**

**Approved by: Dan Huff, City Manager**

**SUBJECT:** Planning Commission Start-Time

**RECOMMENDATION/RECOMMENDED MOTION:**

I move to begin Planning Commission meetings at 6:00p.m.

**BACKGROUND:**

In December 2025, Molalla City Council adopted Ordinance No. 2025-12: Amending Molalla Municipal Code, Chapter 2.04.010 City Council Meetings. This Ordinance changed the standard meeting time from a 7:00p.m. to 6:00p.m. start time.

Planning Commission meetings currently begin at 6:30p.m. Staff is seeking consideration from Commission regarding a 6:00p.m. start time.